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**PLANNING COMMISSION
STAFF REPORT
AGENDA ITEM NO. 5.A.
April 19, 2016**

Project Address: No site address - Admiral Court and Commodore Drive
Assessor's Parcel No: 020-013-250, 020-013-260
Application No.: PDP-15-002, UP-15-006, AR-15-002
Zoning District: P-D (Planned Development District)
General Plan Classification: Visitor Services
Specific Plan Area: U.S. Navy Site and Its Environs Specific Plan
Prepared by: Paula Bradley, MCP, AICP, (650) 616-7038

REQUEST

Request for a Planned Development Permit PDP-15-002, Use Permit UP-15-006, Architecture Review Permit AR-15-002 to allow the construction of five-story, 69'-4" high, 152-room hotel, with 3,000 square feet of meeting/banquet space and below grade parking pursuant to Chapters 12.96.190, 12.96.200, 12.84.210, and 12.108.010 of the San Bruno Municipal Code. OTO Development, LLC (Applicant).

RECOMMENDATION

Staff recommends that the Planning Commission adopt Resolution 2016-05 adopting an Addendum to a SEIR, for the U.S. Navy Site And Its Environs Specific Plan EIR and approve a Planned Development Permit PDP-15-002, a Use Permit for alcoholic beverage sales UP-15-006, an Architecture Review Permit AR-15-002 based on Findings 1-16 and Conditions of Approval 1-188 and a Mitigation Monitoring and Reporting Plan for the property located on Admiral Court and Commodore Drive (Assessor's Parcel Numbers 020-013-250; 020-013-260; and a portion of an adjacent parcel 020-013-220).

BACKGROUND

On March 15, 2016, the Planning Commission approved a General Plan Consistency determination for the disposition (sale) of the two parcels that comprise the site of the application for the purpose of developing the proposed 152-room hotel, five-story select service hotel with 3,000 square feet of meeting/banquet space and below grade parking in addition to surface parking. On March 29, the City Council approved the Purchase and Sale Agreement to allow OTO Development, LLC, to purchase the subject property from the City of San Bruno; and authorized the City Manager to execute an Easement Agreement and Certificate of Acceptance with ASN Tanforan Crossing II for a landscape agreement on adjacent Parcel 2.

PROJECT DESCRIPTION

The applicant proposes to construct a 152-room Springhill Suites select service hotel by Marriott on the 1.5-acre site within The Crossing development, located within the northwest quadrant of interchange I-

380 and El Camino Real in San Bruno. The proposed hotel would be five stories, approximately 69'-4" high at the highest point, 99,022 square feet in size and includes a total of 167 parking spaces, 60 of which are at surface and 107 subterranean. The hotel would include 3,000 square feet of meeting space to accommodate up to 300 people for theater style events, banquet style seating for sit-down events for up to 200 people and up to 150 people for sit-down meetings, weddings and parties. The building includes 6,040 square feet of rooftop solar panels as well as other energy efficiency measures.

The project will require the following planning approvals from the City of San Bruno: a Planned Development Permit, an Architectural Review Permit, a Use Permit (for alcoholic beverage sales), a Sign Permit, and a Supplemental Environmental Impact Report (SEIR) Addendum in accordance with California Environmental Quality Act (CEQA) Guidelines Section 15168.

As a "Select Service" hotel, it would provide limited food and beverage service for hotel guests and conference and meeting attendees. Food service will include breakfast in a buffet format and light meals in the evenings in the bar area. The proposed hotel will not offer room service and food will be catered for larger events. Guests would typically dine off-site. A market/pantry will sell sandwiches, snacks, wine and beer and sundries to hotel guests. The applicant will obtain an Alcohol Beverage Control (ABC) license to allow liquor sales for the food service, bar, and conference and/or special events such as weddings. No entertainment will be provided other than televisions and periodic entertainment for special event guests. The number of employees is expected to be approximately 30, 40% full-time, working different shifts throughout the week.

ENVIRONMENTAL REVIEW

The proposed Crossing hotel project has been reviewed according to the standards and requirements of the CEQA and the San Bruno environmental review procedures. The proposed project is located within the U.S. Navy Site and Its Environs Specific Plan (Specific Plan) area. On January 9, 2001, the City of San Bruno adopted the Specific Plan and certified an associated programmatic Environmental Impact Report (EIR). On September 8, 2015, the City amended the Specific Plan to decrease the size of the hotel and certified a Supplemental EIR (SEIR) to the Specific Plan EIR due to changes in the development capacity of the site resulting from construction of earlier phases of The Crossing Development and to ensure consistency with the 2012 Comprehensive Land Use Compatibility Plan for the San Francisco International Airport. The SEIR analyzed development of a 152-room hotel, identified potentially significant environmental impacts, and updated mitigation measures in the program EIR. The analysis determined that the identified mitigation measures would reduce all potentially significant impacts to less than significant level after mitigation.

For this proposed project, an addendum to the certified 2015 SEIR and the 2001 program EIR was prepared in accordance with CEQA Guidelines for projects in which only minor changes or additions are necessary and none of the conditions necessary for preparation of a subsequent EIR or Negative Declaration are present. This proposed project is identical to the hotel analyzed in the SEIR. Because there have been no substantial changes in the circumstances under which the project will be undertaken, and there is no new information that would result in new or substantially more severe impacts of the project or warrant new mitigation measures or alternatives, none of the conditions necessary for preparation of a subsequent EIR or Negative Declaration are present. Based on an

examination of the 2015 SEIR and the 2001 program EIR, the potential environmental impacts

associated with the proposed hotel project have been adequately analyzed and covered in the prior EIRs, and no further review or analysis under CEQA is required.

LEGAL NOTICE

1. Notices of public hearing mailed to owners of property within 300 feet on April 8, 2016.
2. Advertisement published in the San Mateo Daily Journal, Saturday, April 9, 2016.

PUBLIC COMMENT

No comments have been received as of the completion of this staff report.

REVIEWING AGENCIES

Community Development Department
Public Services Department
Community Services Department
Police Department
Fire Department
San Mateo County Airport Land Use Commission (ALUC)

EXISTING CONDITIONS

The subject site is approximately 1.52 acres in area (66,125 square feet), vacant and accessed via Admiral Court within The Crossing development, located across El Camino Real from the Shops at Tanforan in the northern portion of the City. The project site is zoned Planned Development (P-D). The property consists of two vacant lots (Assessor's Parcel Numbers 020-013-250 and 020-013-260) that were once part of a U.S. Naval administrative facility that was demolished in 2002. Additionally, the site has access and landscape easement rights on a portion of an adjoining parcel, (APN 020-013-220), under separate ownership. The two lots are the last remaining developable sites within The Crossing development. A portion of the site has been used by Jack's restaurant as an interim overflow parking lot, and the remainder is secured by a fence. Jack's ownership was allowed to use this property with full knowledge of the limited term nature of the availability of this site for parking and the future plans for a hotel there. Jack's ownership will need to consider valet or other parking solutions to meet their needs.

The west side of the site faces a multifamily apartment building within The Crossing development. The north side faces the El Camino Real Commercial Development project, which includes Jack's restaurant and several small retail uses. The east side faces El Camino Real and the The Shops at Tanforan. The south side faces the entrance ramp to Interstate 380 with the Bayhill Office Park south of I-380.

SURROUNDING LAND USES

North: Commodore Drive – Residential (Multi-Family Residential) and ECR Commercial Overlay (Restaurant and Retail/Office uses)
South: Interstate 380
East: El Camino Real – Regional Retail (The Shops at Tanforan)
West: Commodore Drive – Residential Office/Residential Flex (Multi-Family Residential)

ENTITLEMENT PROCESS

The applicant submitted the planning application on March 16, 2015, which was deemed incomplete. With resubmittals on July 1 and August 13, 2015, the application was deemed complete on September 16, 2015. The proposed project requires the following entitlements:

Planned Development Permit: The applicant is requesting a Planned Development Permit to allow the development of the proposed hotel. The site is currently designated P-D (Planned Development) with a Visitor Services land use classification. A Planned Development Permit is required for any project proposed in a P-D district to assess and determine that the proposal is consistent with the applicable P-D District requirements.

Architectural Review Permit: An Architectural Review Permit is required for any new building which would be visible from the public right-of-way to assess and ensure that the proposed architecture and site improvements meet City standards.

Conditional Use Permit: The project will require a Use Permit for alcoholic beverage sales.

Sign Permit: A proposed Comprehensive Sign Program is an aspect of the proposed project. The approval of a sign program permit to allow actual installation of the signage will be required as a condition of approval.

NEIGHBORHOOD OUTREACH

The applicant has reached out to the surrounding property owners regarding the proposed project. The applicant has had preliminary discussions with property management at Jack's restaurant and The Shops at Tanforan regarding potential shared parking agreements as needed for day-to-day coordination and special events. The applicant met with property management at the adjacent Avalon Bay Apartments regarding design and construction of the hotel as well as negotiated an agreement along with City representation to allow access and landscape easement rights within a narrow strip of land owned by ASN Tanforan Crossing II on the south and east side of the hotel property. In addition, notice was mailed to the surrounding neighborhood about the proposed project during the Specific Plan amendment process, inviting public comment during the 45-day public comment period for the SEIR. The City has received no comments as of the writing of this report.

Staff mailed a notice of the Architectural Review Committee Meeting to owners of properties within 300 feet of the subject site on November 3, 2015 as well as contacted property managers of the neighboring residential properties and The Shops at Tanforan. No comments were received in response to the legal notices on April 8th and April 9th, 2016 for this Planning Commission meeting. Staff did not receive any comments from the public regarding the proposed project as of writing this report.

ARCHITECTURAL REVIEW COMMITTEE

The Architectural Review Committee (Commissioners Chase, Sammut, and Kayal) reviewed this project at its December 10, 2015 meeting. It was previously reviewed at the Committee's November 12, 2015 meeting and most of the recommendations were incorporated into the plans. The Architectural Review Committee forwarded the project to the Planning Commission with a recommendation of approval, with the following comments and recommendations:

- Light standard fixture “Premier II” (Option 3) was selected as depicted in Exhibit I.
- The revised landscape plan as proposed dated 12/02/15 and the rendering of the east elevation is satisfactory.
- West elevation: propose a textured surface, or multiple textured surfaces, with a possible wave design, and consider extending it above the roof line. Provide a rendering for the Planning Commission meeting.
- Roof overhangs - not needed on the west elevation.
- Porte cochere - may have additional columns if structurally necessary, can be approved at staff level as part of the building plan review.
- Provide before and after elevations renderings with landscaping for review at the Planning Commission meeting.

In response to the ARC recommendations, revised plans were submitted dated January 5, 2016 with a new proposed West elevation detail for the Planning Commission consideration. It consists of two prefabricated framed vertical perforated panels from the second floor and extending above the parapet wall. The final pattern is to be determined and the proposed example is shown on Sheet DR-6A of the plans. The Committee’s recommendations were addressed and revised plans submitted and attached as Exhibit M.

Airport Land Use Commission (ALUC):

The project was referred to the City/County Association of Governments (C/CAG) Airport Land Use Commission (ALUC) for a height consistency determination. On February 11, 2016 the ALUC Commission determined that the hotel would be consistent with the applicable airport land use policies and criteria contained in the Comprehensive Airport Land Use Compatibility Plan for Environs of San Francisco International Airport (SFO ALUCP) with two conditions met for sound attenuation and with the execution of an aviation easement granted to the City and County of San Francisco as operator of SFO (Exhibit J). These are both required as conditions of approval.

STAFF’S PROJECT ANALYSIS

Land Use. The site is located within the U.S. Navy Site and Its Environs Specific Plan, adopted in 2001 and amended in 2002, 2005 and 2015. The U. S. Navy Site and Its Environs Specific Plan designates this site for a high quality hotel with 152 rooms with a maximum height consistent with Federal Aviation Administration (FAA) requirements; 80% maximum lot coverage for structures, minimum 15% landscaping, and parking based on the current code requirements (or as reduced by the Planning Commission). A “high quality” hotel is a full service or select service hotel offering guest rooms for overnight stay, meeting facilities, food services and a variety of personal services offered to hotel guests. It is envisioned that a conference space will be developed within the hotel to accommodate: (i) 250-300 people in theatre-style seating for conferences and meetings; (ii) 200 people in banquet-style seating for sit-down events; and (iii) 100-150 people in ballroom-style arrangements for wedding events. The proposed hotel is consistent with the land use plan designation of a large high-quality hotel use as the economic cornerstone of the Specific Plan and as a transit oriented development (Policy LU-1.1). The Specific Plan also includes design guidelines for the site relating to building orientation to streets and pedestrian corridors as well as building design (see U.S. Navy Site and Its Environs Specific Plan Design Guidelines Exhibit F).

Height. The proposed building is five stories with the highest point at 69'-4". The proposed building is within with the Specific Plan requirements of seven stories and a maximum height allowed according the FAA. The Navy Specific Plan Area has unique height restrictions, due to the physical proximity to the San Francisco International Airport (SFO), development is restricted under the 2012 Airport Land Use Compatibility Plan (ALUCP) and Federal Aviation Administration (FAA) height restrictions. The FAA has established safety compatibility zones, airspace protection policies and Federal Aviation Regulation (FAR) Part 77 obstruction criteria. The FAR Part 77 obstruction criteria define the maximum heights of all structures and are intended to prevent buildings and other objects from encroaching into required takeoff and landing airspace. In order to be deemed consistent with ALUCP, the maximum height of a new building must be the lower of (1) the height shown on the SFO critical aeronautical surfaces map, or (2) the maximum height determined not to be a "hazard to air navigation" by the FAA in an aeronautical study prepared pursuant to the filing of FAA Form 7460-1 (see letters attached as Exhibit J and K).

The hotel project is within the allowed height according to the ALUCP regulations and the FAR Part 77 obstruction criteria. The proposed 67-foot height is within the Critical Aeronautical surface zone in the southeast corner. Outside of the One –Engine Operating Line (OEI) the height is allowed to be higher and is proposed height is 69'-4" in the southwest corner of the building. The FAA has provided a "Determination of No Hazard to Air Navigation" letter dated 12/11/15 for the hotel and a letter dated 11/30/15 for the construction (crane) during construction (Exhibit K). Height verification is required as a condition of approval according to the FAA. The project was also referred to the City/County Association of Governments (C/CAG) and it's Airport Land Use Commission (ALUC) for consistency with the ALUCP the San Francisco Airport plan, and the FAA restrictions cited above. The proposed hotel height is consistent with Specific Plan Policy LU1.4.

Sustainability Measures The US Navy Plan and Its Environs Specific Plan designates this site for a high quality, full or select service with meeting facilities 152 room select service hotel at the subject site . The Amended Specific Plan requires the hotel to implement the following conditions of approval to Sustainability Requirements:

- a. Install a minimum of 6,000 square feet of solar photovoltaic panels on the rooftop.
- b. Improve the building envelope energy efficiency 15% over 2013 Title 24 standards;
- c. Incorporate high efficiency lighting (such as LEDs, metal halide post top, metal halide cobrahead or cutoff lights, or high pressure sodium cutoff lights) into public areas, such as parking lots, garages, and other exterior areas to achieve at least 15% lighting energy reduction compared to the use of mercury cobrahead lights;
- d. Install energy efficient appliances that comply with the most recent U.S. EPA Energy Star criteria, including refrigerators, dish washers, fans, and clothes washers;
- e. Incorporate water conservation strategies, including the installation of low flow faucets, toilets, and showers, as well as water efficient irrigation-systems; and
- f. Institute recycling and composting services in order to achieve at least a 10% reduction in waste disposed.

Transportation and Parking. The project is in close proximity to Interstate Highways 380, 280 and State Highway 101 and the San Francisco International Airport. It is also close to BART (approximately 0.5 miles), SamTrans and Caltrain, restaurants, shopping, Bayhill Office Park and many services are within walking distance.

The proposed hotel will include a total of 167 parking spaces (107 spaces in a subterranean garage with two levels and 60 surface spaces). Twenty tandem parking spaces are included which will be utilized when there is valet parking service for special events. When there is no valet parking, ten of the spaces will be cordoned off. Required for the project is one space per room, one for the manager and one space per 100 square feet of meeting room space. The Specific Plan allows a 15% reduction for shared parking between the hotel and the meeting space requirement. Therefore the parking requirement is 183 spaces reduced by 27 spaces) is 156 parking spaces required. The hotel will provide valet parking for larger events, which is estimated to increase parking capacity by 30-40% (46 to 61 spaces). Employee parking will be in the garage, but employees will also be encouraged to use public transit. Since most employees work during the day, no more than 2-3 spaces will be required after 9:00 pm. The hotel will provide five short-term bike parking outside the main building entrance and five long-term in the parking garage. Deliveries will arrive at the main entrance and deliveries for catered events will be at the rear entry (meeting rooms) on the southeast elevation close to the garage.

The U.S. Navy Site and Its Environs Specific Plan Design Guidelines states that parking shall not dominate the experience along any pedestrian route. The proposed surface parking area is primarily located toward the east towards El Camino Real away from the main entrance and The Crossing plaza and pedestrian area. Approximately 64% of the proposed parking is located in an underground parking garage access from the rear of the building.

To further address and consistent with environmental mitigation measures and staff's direction, the applicant submitted a Transportation Demand Management Plan (TDM) to reduce parking demand and vehicle trips generated by the project. Two dedicated natural gas shuttle buses or similar contract services will serve travelers from the San Francisco airport to the hotel and other local destinations. Bicycle facilities include five short-term bicycle spaces (bike racks) and ten long-term bicycle spaces (five bicycle lockers) will be provided along with showers and changing rooms for employees. The hotel will provide a kiosk with travel information including mass transit options to reduce peak hour trips. Staff recommends that additionally, a condition of approval require that the hotel provide a Clipper card to encourage employees to use transit. A condition of approval requires that a final TDM planned be submitted for the staff review and approval prior to occupancy and ongoing monitoring.

With the implementation of a well-managed TDM Plan in addition to the required parking, and the business traveller focus hotel guest, it is expected that on-site parking will be more than sufficient for full occupancy and special events.

To ensure compliance and to evaluate the effectiveness of the proposed TDM measures staff has included a Condition of Approval that would require the tenant to provide annual reports to the Community Development Department for the first five years describing the implementation of the TDM measures selected for the project, to ensure the implementation of the program.

Trash Enclosure: The trash enclosure is proposed along the south side of the building within a CMU enclosed with metal doors and roof to match the hotel building. The design is consistent with the design of the building as required in The Crossing Design Guidelines. The enclosure structure is approximately 15' by 16'-8" wide.

Architectural Analysis

During the pre-submittal plan review phase, City staff worked with the applicant (OTO) and their architect (Architectural Dimensions) to ensure that the proposed hotel design meets the Specific Plan design guidelines. OTO representatives were very cooperative in working collaboratively with staff and Larry Cannon, the City's peer review architect, to improve the building design. The City Council Development Subcommittee and City Department heads also reviewed the design plans and provided their input. Between August 2014 and March 2015, City staff met frequently with OTO and Architectural Dimensions representatives to work out many site and design plan issues. Early direction provided to the applicant included the following:

Overall Design. The consulting architect and staff agreed the hotel should be more contemporary in design than the nearby residential buildings, but it should incorporate some of the residential elements of those structures to blend contextually. The Crossing Design Guidelines states "Building design features are intended to create a sense of mass and scale compatible with a pedestrian friendly transit-oriented development." The proposed contemporary design includes a readily distinguishable entrance on the northeast elevation oriented towards The Crossing fountain and pedestrian oriented area on Admiral Court. Varied color and materials, recessed windows and architectural details are proposed to break up the mass of the five-story building and provide visual interest for pedestrian/street level as viewed from the roadways consistent with Specific Plan Policy CD-1.2 and CD-1.3.

Site Plan and Building Orientation. The US Navy Plan and Its Environs Specific Plan states that the hotel building orientation shall be designed to give the appearance of fronting on El Camino Real. However, due to the physical site constraints including noise and the view of I-380 above the project to the south, desire to provide a visually attractive sense of arrival to the hotel from Admiral Court, and to relate the hotel to The Crossing (fountain and plaza, Jack's restaurant) an entry towards both Admiral Court and El Camino is proposed. With the proposed main entry and porte-cochere facing Admiral Court with The Crossing pedestrian friendly areas and to maintain an "urban village" concept, this orientation better achieves the goals of the plan. The v-shaped building provides visual, noise (from I-380 and the airport) and wind protection for the open courtyard and pool area. Staff required additional landscaping area and hardscape/pavers to integrate and connect with those of The Crossing at the fountain plaza area. Different pavers will distinguish pedestrian vs. vehicle travel areas.

Hotel guests will have views of the San Francisco Bay and San Bruno Mountain and to the northwest. Views of the hotel from I-380, El Camino Real and from the north in will be attractive, consistent with the Crossing Design Guidelines and the US Navy Plan and Its Environs Specific Plan concerning building design and views.

Building Massing/Scale. The Crossing Design Guidelines states that building mass and scale should be human scale at the pedestrian level by the use of building articulation breaking up mass larger building elements as viewed from a distance. Consistent with The Crossing Design Guidelines the building has a readily distinguishable entrance with a pedestrian friendly first floor

with doors, windows, variations in color and materials, punched openings and recessed windows, articulation with horizontal and vertical variation in plane, to break up the mass and scale of the building. Quality materials (French limestone is used on the base and middle of the building with a canopy above.

The U.S. Navy Site and Its Environs Specific Plan Design Guidelines (Policy DS-1.1) states that the hotel building shall be designed to minimize shadows and encourage light and air exposure. The hotel is consistent as it minimizes shadows with the v-shape, is oriented away from the main entry and towards the parking lot to the east.

Elevations. The design guidelines of both the Navy Site Specific Plan and the Transit Corridors Specific Plan strongly encourages a strong base, middle and top for buildings. This was a strong element in the designs for the existing residential buildings in The Crossing. At staff's request, the applicant revised the elevation multiple times to achieve a design consistent with the design guidelines and plans which includes vertical and horizontal planes, recessed windows the curved porte-cochere canopy and expanded use of the limestone on the front northeast façade.. Finishes include both metal wall panels on the towers, and stucco panels, French limestone veneer, metal roof overhangs. The proposed west elevation was lacking in visual interest and the ARC members recommended articulation such as alternating stucco finishes or color panels and a wavy pattern or design to be presented at the Planning Commission meeting.

Exterior Colors and Materials. Staff direction was to apply greater color contrast to accentuate articulation of different planes and bring limestone from the base up to the second floor on the north façade above the porte-cochere. An expanded French limestone treatment at the base and on all three elevations was requested by staff and incorporated. Consistent with the U.S. Navy Site and Its Environs Specific Plan Design Guidelines the proposed materials (French limestone base) for the building will convey a sense of durability and permanence.

Windows. Staff recommended that the windows be more deeply inset with prominent sills. The long horizontal sills and vertical lines between the pairs of windows on the north side should be eliminated. Staff suggested adding two windows to center of north façade to fill blank space and create more interest. The Crossing Design Guidelines discourages large expanses of windows, and calls for the use clear glazing on the ground level as well as the upper levels. Windows should be recessed to provide shadow lines and trim to provide visual interest. The proposed project is consistent with these design standards with the incorporation of staff's recommendations to further recess the windows and add projecting balconies and trim on the north facade. There are small-scale windows at the base of the building providing a pedestrian visual interest with clear glazing along with varied materials and colors to decrease mass and scale at the ground and upper levels.

Roof. Staff recommended that the roof lines should emphasize variation in height with cap treatments on all roof sections. The proposed roof lines are now consistent with the directives. The roof includes a parapet wall that will screen the mechanical equipment and will be required as

a condition of approval.

Landscaping. Staff and the Architectural Review Committee suggested that the entire parcel edge along the east and south sides will need careful and abundant landscaping to minimize views of parked cars and the trash area. Proposed landscaping is 15.4%, not including ground cover and turf planted on the adjacent Parcel 2. Trees, shrubs and ground cover are proposed including tree and shrub species suggested in The Crossing landscape design and planting guidelines. Bio-retention areas required for stormwater retention on site include water filtration shrubs and grasses. Trees are proposed around the perimeter and within the parking lot. Landscaping was increased with wider planters as required by staff. In response the applicant added landscaping to Parcel 2, to screen the parking area along El Camino Real. A slatted six-foot chain link fence visually separates the parking area from the commercial center (Jack's restaurant). Along El Camino Real and the I-380 west on ramp is a six-foot slatted chain link fence owned by Caltrans.

In terms of hardscape treatments, a variety of gray and beige color pavers are proposed for the entry driveway, parking area of the plaza fountain area, which match the existing pavers in The Crossing on Admiral Court. Contrasting pavers are used to delineate the vehicle vs. pedestrian traffic areas in the entry and for interior walkways, the courtyard and pool area.

Lighting. Exterior lighting is proposed and specifications for light fixtures and parking lot standards for review. The Crossing Design Guidelines state that "Buildings and their environs should be lighted for safety, accent, and way finding purposes." Concerning exterior building lighting recommendations include that "The aesthetic quality of exterior lighting should be considered in relationship to adjacent streets, gathering areas, and open spaces." Proposed are lighting designs appropriate to the modern style of architecture. The Crossing Design Guidelines state that "Locations of Building name signage and Building address shall be illuminated to facilitate way finding." A photometric plan was submitted with lighting specifications for review. Wall sconces are proposed near the doorways and step lights in the pool area. Light poles are proposed in the parking areas and pool areas throughout the site. Staff and the Architectural Review Committee recommended that the applicant provide light standard option to fit in with the context of The Crossing. The light standards proposed are modern style in character with the hotel design but very different than the lighting style in the Crossing.

Signage. A proposed comprehensive sign program for the project has been submitted for review and the approval of a sign permit is required as a condition of approval. The plan was reviewed by the ARC on November 12, 2015. The Crossing Design Guidelines specifies monument signage, placed within the landscape areas, and lighted by up lighting. Internally lit signs are not allowed. Signage is allowed near the roof line, provided that they are approved by the Architectural Review Committee. Signage includes internally illuminated vinyl channel letters/branding on the three primary building elevations. A five-foot high monument sign is proposed for the main entry. Materials include a violet color acrylic and aluminum frame box with white and red brand letters. A French limestone base to match the building will used. Proposed

directional signage is illuminated by up-lights. Signage is proposed for the subgrade parking garage with white channel letters with a black perforated vinyl above the entry (conceptual). Sign lighting is proposed with up-lights. Smaller monument type signage is proposed for way finding signs to parking areas.

The Crossing Design Guidelines concerning exterior building signage/graphics calls for monument signage placed in landscape areas at prominent entrances, corners and other points of visual focus within the land plan without obstructing views and creating hazards. Furthermore, "Signs shall be not taller than six feet above grade." And "No pole signs will be allowed." Signage will be lighted by up lighting or other similar focused lighting. Signs which are exclusively lit internally will not be permitted although internal lighting for the purpose of accent lettering or logos is permissible." Signs related exclusively to hotel(s) may be located on the exterior of the building(s) near the roof line, provided such signs are reviewed and approved by the San Bruno Architecture Review Committee." The proposed project signage is consistent with the guidelines as described above.

Use Permit

The applicant will apply for an ABC license to allow alcoholic beverage sales for hotel and conference and/or special events uses guests, and for the bar and market/pantry retail uses. This use is consistent with and permitted by both the Specific Plan and the Zoning Code designations of P-D Planned Development District; however, the sale of alcohol requires Planning Commission approval. Staff does not anticipate any negative effects from alcohol sales in conjunction with the proposed use. The Police Department has also reviewed the application and has recommended conditions of approval for the project. If any operational issues arise in the future related to the use permit, the Community Development Director has the authority to call the item back to the Planning Commission for review.

Staff finds that the proposed restaurant with alcohol sales will comply with required performance standards of the Municipal Code. The required performance standards come from Section 12.84.210 of the San Bruno Municipal Code. The performance standards are listed in bold followed by staff's analysis:

RECOMMENDATION

Staff finds that the proposed of five-story, 69'-4"-foot high, 152-room hotel, with 3,000 square feet of meeting/banquet space to be consistent with the design and scale of the surrounding Crossing development and conforms with the US Navy Site and Its Environs Specific Plan and The Crossing Design Guidelines as further described in the required Findings, subject to the recommended conditions of approval.

The project would complete the build out of The Crossing development and fulfill the vision for a mixed-use transit-oriented development with a high quality hotel and meeting and conference facility in the Specific Plan area. The building would provide an attractive visual anchor and gateway into the City along El Camino Real. The project would also fulfill a key economic development objective of the Specific Plan by providing a high quality Springhill Suites hotel with meeting and banquet space and generating long-term tax revenues to the City. Located at the prominent intersection of El Camino Real and I-380, the project would play a key role as a catalyst for economic development and revitalization on

El Camino Real and the nearby Transit Corridors Plan area. In addition, the project is located within a short distance of SamTrans bus routes, the new Caltrain station, and BART, increasing transportation options for hotel guests and workers, and reducing dependency on automobiles.

In conclusion, staff recommends the Planning Commission approve the project with the following staff recommendation related to architectural design:

1. West elevation: Propose a decorative metal panel with a design reflecting the local context. Alternatively, propose a second option with a pattern incorporated into the panel surface to provide visual relief and interest.

FINDINGS

The required findings are in bold followed by staff's analysis of the merits of the project and how the findings can be made.

With respect to the SEIR, Addendum was prepared for the project, the Planning Commission finds:

1. Based upon the analysis, findings, and conclusions of the 2015 SEIR, as well as those of the 2001 EIR and the Addendum prepared for the project and comments and testimony received, the Planning Commission hereby finds that the Crossing Hotel Project is identical to the hotel assumed in the SEIR analysis; there are no substantial changes to the project. Furthermore, there have been no substantial changes in the circumstances under which the project will be undertaken. Additionally, there is no new information that would result in new or substantially more severe impacts of the hotel project, or that new mitigation measures or alternatives are warranted. The Crossing Hotel Project therefore meets the requirements for an Addendum and that the Addendum reflects the Planning Commission's independent judgment and analysis.

The Planning Commission does hereby find this action to be consistent with the project evaluated in the Supplemental EIR and that i) there are no substantial changes to the project which would result in the need for major revisions to that Supplemental EIR, ii) there are no substantial changes with regard to the circumstances surrounding the proposed action which would require major revisions to that Supplemental EIR, and iii) no substantial new information exists which was not previously known which would show that the project has new significant environmental impacts, that the project's identified impacts are substantially more severe than previously disclosed, or that alternatives or mitigation measures previously found to be infeasible are in fact feasible and/or would reduce significant environmental impacts more than previously disclosed, which would require that a new Subsequent or Supplemental EIR be prepared under section 15162 of the CEQA Guidelines.

Overall, based on an examination of the analysis, findings, and conclusions of the 2015 SEIR, as well as those of the 2001 EIR— the potential environmental impacts associated with the Crossing Hotel Project have been adequately analyzed and covered in prior Program EIRs. Each of the above findings provides a separate and independent basis for CEQA compliance. Therefore, no further review or analysis under CEQA is required.

With respect to the **Planned Development Permit**, the Planning Commission finds:

Per Chapter 12.96.190 of the San Bruno Zoning Ordinance a Planned Development Permit is required for any development within the Planned Development zoning district. In addition, Chapter 12.108 of the San Bruno Zoning Ordinance requires an Architectural Review Permit for a new building, and Chapter 12.84.210 requires a Use Permit to allow alcoholic beverage sales.

2. The proposed planned development permit is consistent with the previously approved development plan.

The subject site is included within the US Navy and Its Environs Site Specific Plan and is zoned P-D Planned Development District. According to Chapter 12.96.200 of the Zoning Code the Zoning Regulations and Development Standards are as set for in the Specific Plan. The Specific Plan designates this site for a high quality hotel with 152 rooms with a maximum height of seven stories and height in compliance with the Federal Aviation Administration (FAA) regulations, 80% maximum lot coverage for structures, minimum 15% landscaping, up to 20,000 square feet of gross leasable space and parking based on the current code requirements (or as reduced by the Planning Commission). The Specific Plan also includes design guidelines for the site relating to building orientation to streets and pedestrian corridors as well as building.

The proposed building is 99,022 gross square feet in size with five floors, resulting in 1.5 FAR and 25% lot coverage. The proposed building is five stories with the highest point at 69'-4". The proposed building is within with the Specific Plan requirements of seven stories and a maximum height allowed according to the FAA regulations. The project was also referred to the City/County Association of Governments (C/CAG) and it's Airport Land Use Committee (ALUC) for a consistency determination with the ALUCP the San Francisco Airport plan, and the FAA restrictions cited above. The proposed hotel height is consistent with Specific Plan Policy LU1.4 with the implementation of two conditions, for noise attenuation and execution of an Avigation Easement for the benefit of SFO.

The Crossing Design Guidelines states "Building design features are intended to create a sense of mass and scale compatible with a pedestrian friendly transit-oriented development." The proposed contemporary design includes a readily distinguishable entrance on the northeast elevation oriented towards The Crossing fountain and pedestrian oriented area on Admiral Court. Varied color and materials, recessed windows and architectural details are proposed to break up the mass of the five-story building and provide visual interest for pedestrian/street level as viewed from the roadways consistent with Specific Plan Policy CD-1.2 and CD-1.3.

Landscaping proposed is 15.4%, not including ground cover and turf planted on the adjacent Parcel 2. Trees, shrubs and ground cover are proposed including tree and shrub species suggested in The Crossing landscape planting and Design Guidelines. The proposed hotel size, site, height and building design and landscaping is consistent with the policies and Design Guidelines in the Specific Plan Development Standards for the site. The proposed hotel parking includes a total of 167 parking spaces where one per room is required (107 spaces in a subterranean garage with two levels and 60 surface spaces). Additionally twenty tandem parking spaces and 45 valet parking for larger events. A condition of approval requires roof screening for

mechanical equipment to match the exterior of the building.

With respect to the **Architecture Review Permit**, the Planning Commission finds:

- 3. That the location, size and intensity of the proposed operation will not create a hazardous or inconvenient vehicular or pedestrian traffic pattern, taking into account the proposed use as compared with the general character and intensity of the neighborhood.**

Vehicular and pedestrian access to the site is provided from both Admiral Court and Commodore Drive (no direct vehicular access from El Camino Real). The main vehicle and pedestrian entry is enhanced with landscaping and trees and light bollards and decorative pavers. The development will include two 24-foot wide entrance and exit driveways, and will provide the appropriate level of service for the amount of on-site circulation. This development fits in well with the general character and intensity of The Crossing Specific Plan development considered a Transit-Oriented Development, by providing a high-quality hotel along El Camino Real in close proximity to major transportation hubs including SFO, two Interstates (I-380 and I-280) and State Highway 82 and public transit (BART, Caltrain, and SamTrans).

The U.S. Navy Site and Its Environs Specific Plan Design Guidelines states that parking shall not dominate the experience along any pedestrian route, but should be located to the rear of the main building near I-380 and should be shared with other uses to the extent possible. Below grade parking is a preferable design option. The proposed hotel includes a total of 167 parking spaces (107 spaces in a subterranean garage with two levels and 60 surface spaces) including 20 tandem parking spaces. When there is no valet parking and valet parking for larger events. The hotel will provide five short-term bike parking outside the main building entrance and five long-term (lockers) in the parking garage. The proposed project meets the parking requirements of the Specific Plan.

- 4. That the accessibility of the off-street parking areas and the relation of parking areas with respect to traffic on adjacent streets will not create a hazardous or inconvenient condition to adjacent or surrounding uses.**

Vehicular access to the site is from two 24-foot wide driveways, the main entry from Admiral Court and secondary entry-egress from Commodore Drive near the garage. Vehicular access is not provided to the site from El Camino Real. Parking on-site is provided for 167 vehicles. Admiral Court and Commodore Drive also provide vehicular access to the adjacent multi-family housing developments. The accessibility of off-street parking is appropriate for the area. Adequate site visibility will be maintained from the driveways with the required sight-distance clearance.

- 5. That sufficient landscape areas have been reserved for the purposes of separating or screening service and storage areas from the street and adjoining building sites, breaking up large expanses of paved areas, and separating or screening parking areas from the street and adjoining building areas from paved areas to provide access from buildings to open areas. In addition, that adequate guarantees are made, such as the filing of a performance bond, to**

insure maintenance of landscaped areas.

The plans call for landscaping along El Camino Real and Admiral Court, as well as along the side property lines and within the interior of the site. The plan proposes a 5' wide turf along Admiral Court planted with trees and shrubs. Proposed landscaping is 15.4% (15% is required by the Specific Plan), not including ground cover and turf planted on the adjacent Parcel 2. Trees, shrubs and ground cover are proposed including tree and shrub species suggested in The Crossing landscape design and planting guidelines. Conditions of Approval require that the landscaping and irrigation shall be maintained for the life of the project. The trash enclosure is proposed along the south side of the building within a CMU enclosed with metal doors and roof to match the hotel building. The design is consistent with the design of the building as required in the Crossing Design Guidelines.

- 6. The proposed development, as set forth on the plans, will not unreasonably restrict or interfere with light and air on the property and on other property in the neighborhood, will not hinder or discourage the appropriate development and use of land and buildings in the neighborhood, or impair the value thereof; and is consistent with the design and scale of the neighborhood.**

The proposed building is five stories with the highest point at 69'-4". The majority of the building is proposed at a height of 67 feet height within the critical surface area. The proposed building is within the specific plan requirements of seven stories and a maximum height allowed according the FAA as specified in the Specific Plan. The proposed building has some variation in building height. The U.S. Navy Site and Its Environs Specific Plan Design Guidelines (Policy DS-1.1) states that the hotel building shall be designed to minimize shadows and encourage light and air exposure. The hotel is consistent as it minimizes shadows with the v-shape, is oriented away from the main entry and towards the parking lot to the east. The proposed hotel size, site, height and building design and landscaping is consistent with the policies and Design Guidelines in the Specific Plan Development Standards for the site. The site is currently vacant and the proposed building will complete the build out of The Crossing development.

- 7. That the improvement of any commercial or industrial structure, as shown on the elevations as submitted, is not detrimental to the character or value of an adjacent residential district.**

The US Navy Site and Its Environs Specific Plan includes a mix of residential and commercial uses. The U. S Navy and Its Environs Specific Plan designates this site for a high quality hotel with 152 rooms with a maximum height of seven stories and height per the Federal Aviation Administration (FAA). A "high quality" hotel is a full service or select service hotel offering guest rooms for overnight stay, meeting facilities, food services and a variety of personal services offered to hotel guests. The proposed hotel is consistent with the land use plan designation of a large high-quality hotel use as the economic cornerstone of the Specific Plan and transit oriented development (Policy LU-1.1). The proposed building will not be detrimental to the adjacent residential district with the inclusion of sound attenuation required for hotel windows and a design that includes minimal windows facing the residential use, a 14-foot high green screen and visual interest on the west façade. The hotel use was planned for the site as part of the U. S. Navy Site and Its Environs Specific Plan and analyzed in the 2001 Specific Plan EIR and the 2015 SEIR,

which analyzed a reduced scale and size of the hotel from 500 rooms to 152 rooms.

8. That the proposed development will not excessively damage or destroy natural features, including trees, shrubs, creeks and rocks, scenic corridors, and the natural grade of the site.

The subject site is currently vacant and was previously developed with buildings and surface parking for use by the US Navy's Western Division Engineering offices. The site does not contain any trees, creeks or scenic corridors. Four existing pine trees on Parcel 2 will be preserved. The proposed development therefore will not damage or destroy any natural features.

9. That the general appearance of the proposed building, structure, or grounds will be in keeping with the character of the neighborhood, will not be detrimental to the orderly and harmonious development of the city, and will not impair the desirability of investment or occupation in the neighborhood.

The site is located within the U.S. Navy Site and Its Environs Specific Plan, adopted in 2001 and amended in 2002, 2005 and 2015. The U. S. Navy Site and Its Environs Specific Plan designates this site for a high quality hotel with 152 rooms with a maximum height of seven stories and height per the Federal Aviation Administration (FAA). The proposed exterior materials, landscaping, and overall building design is high quality and will enhance the overall appearance of the Crossing and the El Camino Real commercial corridor. The project would support the Specific Plan by completing a missing component of the plan that has been envisioned for the past 15 years.

10. That the proposed development is consistent with the San Bruno General Plan

The property is located within the US Navy Site and Its Environs Specific Plan area and is zoned Planned Development. The proposed development is consistent with the Specific Plan and the Development Standards of that plan according to Chapter 12.96.200 of the Zoning Code. The project is consistent with the Visitor Serving land use designation as a commercial hotel and with the Transit-Oriented Development and the FAR as well as that of the Specific Plan.

Land use POLICY LU-1-1: Establish land uses which are conducive to Transit Oriented Development.

The development of a hotel on The Crossing Hotel Site has long been a high priority of the City Council and the community, as it is an integral part of the vision for The Crossing as a mixed-use transit-oriented development.

Land Use POLICY LU 1-2: Establish land uses at an intensity that sustains an active Pedestrian and public transit supportive environment.

The hotel project is in close proximity to Interstates 380, 280 and State Highway 101 and the San Francisco International Airport. It is also close to BART (approximately 0.5 miles), SamTrans and Caltrain, restaurants, shopping, Bayhill Office Park and many services are within walking distance. A Traffic Demand Management program is required to reduce vehicle trips and to encourage use of public transit. Natural gas shuttles will provide transportation to transit local

shopping and eating venues and transit.

Land Use POLICY LU 1-4 Establish building heights and land use regulations to assure compliance with FAA standards and compatibility with the noise environment.

The project was referred to the City/County Association of Governments (C/CAG) Airport Land Use Commission (ALUC) for a height consistency determination. On February 11, 2016 the ALUC Commission determined that the hotel would be consistent with the applicable airport land use policies and criteria contained in the Comprehensive Airport Land Use Compatibility Plan for Environs of San Francisco International Airport (SFO ALUCP) with two conditions met for sound attenuation and with the execution of an aviation easement granted to the City and County of San Francisco as operator of SFO.

Land Use POLICY LU-2.1 Encourage a compatible mixtures of land uses and service facilities which will assure long-term revenue generation, create jobs, and offer a range of housing types, densities and affordability.

If entitlements are approved by the Planning Commission, the hotel would create more than 100 construction jobs in addition to 30-40 ongoing hotel service jobs with what OTO has described as competitive wages and benefits for full-time employees. The property is designated for a hotel use by the Specific Plan and the hotel would generate an estimated one million dollars per year in transient occupancy tax and provide 3,000 square feet of community meeting space. The proposed hotel is consistent with the land use plan designation of a large high-quality hotel use as the economic cornerstone of the Specific Plan and transit-oriented development.

Therefore the project is consistent with the US Navy Site and Its Environs Specific Plan and the San Bruno General Plan.

Community Design Policy CD-1.2 Implement Building form Policies which assure that views from pedestrian land facilities are attractive, varied and stimulating.

And

Community Design Policy CD-1.3 Implement Building Design Standards to create a sense of mass and scale compatible with a pedestrian-friendly transit oriented development.

The proposed contemporary design includes a readily distinguishable entrance on the northeast elevation oriented towards The Crossing fountain and pedestrian oriented area on Admiral Court. Varied color and materials, recessed windows and architectural details are proposed to break up the mass of the five-story building and provide visual interest for pedestrian/street level as viewed from the roadways consistent with Specific Plan Policy CD-1.2 and CD-1.3.

With respect to the **Use Permit**, the Planning Commission finds that the proposed restaurant with alcohol sales will comply with all the required performance standards of the Municipal Code, under Section 12.84.210. The performance standards are listed in bold followed by staff's analysis:

11. The activity or use does not jeopardize, endanger or result in adverse effects to the health, peace or safety of persons residing or working in the surrounding area. (SBMC 12.84.210.B.1)

The use of the property as a hotel with eating and conference facilities along with a bar serving alcoholic beverages, and the market/pantry retail use selling wine and beer to hotel guests is compatible with surrounding uses, which include other restaurants, retail stores, and other similar uses.

12. The activity or use does not result in repeated nuisance activities or police interventions within the premises or in close proximity of the premises, including but not limited to criminal activities, disturbance of the peace, illegal drug activity, public drunkenness, drinking in public, harassment of passersby, gambling, prostitution, sale of stolen goods, public urination, theft, assaults, batteries, acts of vandalism, excessive littering, loitering, graffiti, illegal parking, excessive loud noises, especially in the late night or early morning hours, traffic violations, curfew violations, lewd conduct, or police detentions and arrests. (SBMC 12.84.210.B.2)

The sale of alcoholic beverages will be conducted in conjunction with the hotel. The bar hours of operation will be 5:00 p.m. until 9 to 10 p.m., seven days a week. The hotel will provide food and alcoholic beverage services for hotel guests and conference and meeting attendees. The hotel will be operated 24 hours a day, but food service will only be available for breakfast (offered in a buffet format) and limited food service will be provided in the bar area in the evening for guests and during catering and banquet events. No entertainment will be provided in the bar/lounge other than televisions and periodic entertainment as provided by catering or banquet guests. No room service will be provided.

The hotel market/pantry will sell snacks, sandwiches, wine and beer, etc. and sundries to hotel guests. Guests are expected to eat lunch off-site, but some may eat a light meal in the bar area in the evening. The bar is expected to have seating which includes stools and site down tables. Guests will also be able to eat and drink in the outside patio areas near the pool. Bar hours will be approximately 5:00 p.m. until 9:00 to 10 p.m.. These hours are not anticipated to pose any operational concerns. The Community Development Director has the ability to call this use permit back to the Planning Commission for review, if he/she finds that the use is negatively impacting the surrounding neighborhood. Therefore, if any unforeseen impacts occur, the use can be further reviewed.

13. The activity or use does not result in violations to any applicable provision of any other city, state, or federal regulation, ordinance or statute. (SBMC 12.84.210.B.3)

By obtaining this Use Permit, and subsequently obtaining approval from the State Department of Alcohol and Beverage Control, the applicant will be in compliance with local and state regulations. As required by the local and state law, the City will issue a "need and necessity" letter to the Department of Alcohol and Beverage Control upon approval of this Use Permit.

14. The upkeep and operating characteristics of the activity or use are compatible with and will not adversely affect the livability or appropriate development of abutting properties and the surrounding neighborhood. (SBMC 12.84.210.B.4)

As stated above, the use of the property as a hotel with eating and conference facilities along with a

bar serving alcoholic beverages, and the market/pantry retail use selling wine and beer to hotel guests is compatible with surrounding uses, which include other restaurants, retail stores, and other similar uses. The closest residential neighborhood are the multifamily residential uses adjacent to the hotel, adjacent to and east of the existing restaurant. The hotel will provide limited food (no restaurant) and serve and sell alcoholic beverages to hotel guests and conference and meeting attendees. The hotel will be operated 24 hours a day, but alcoholic beverages will be provided in the bar area in the evening for guests and during catering and banquet events. No entertainment will be provided in the bar/lounge other than televisions and periodic entertainment as provided by catering or banquet guests. Bar hours will be approximately 5 p.m. to 9 p.m. or 10 p.m. The proposed market/pantry will sell snacks, sandwiches, wine and beer, and sundries to hotel guests. Guests are expected to dine off-site, but some may eat a light meal in the bar area in the evening. Guests will also be able to eat and drink in the outside patio areas near the pool.

15. The applicant for a liquor license receives a Letter of Public Convenience or Necessity issued by the City of San Bruno for an application which would tend to create a law enforcement problem, or if issuance would result in or add to an “Undue Concentration” of licenses, required due to either of the following conditions:

- a. **The applicant premises are located in a crime reporting district that has a twenty percent greater number of reported crimes in a geographical area within the boundaries of the city than the average number of reported crimes as determined from all crime reporting districts within the jurisdiction of the San Bruno Police Department that are identified by the department in the compilation and maintenance of statistical information on reported crimes and arrests. (SBMC 12.84.210.5.a)**
- b. **The applicant premises are located in an area of Undue Concentration, which is defined to exist when an original application or premises-to-premises application is made for a retail on-sale license in a census tract where the ratio of existing on-sale retail licenses to population in the census tract exceeds or will exceed the ratio of retail on-sale licenses to population in San Mateo County. (SBMC 12.84.210.5.b)**

Although the project site is located in a crime-reporting district that has a 20% greater number of reported crimes in a geographical area within the boundaries of the City than the average number of reported crimes in the City, the restaurant is located in an area designated for a 152-room hotel use according to the US Navy Site and its Environs Specific Plan. It is adjacent to a land use designation of ECR Commercial Overlay (Restaurant and Retail/Office uses) and to the east across El Camino Real, a regional commercial/shopping center (The Shops at Tanforan) which typically have a higher crime rate than other districts. A large percentage of the crimes reported are not related to alcohol use but are related to the commercial uses typical of a shopping center. Staff acknowledges that this hotel with meeting rooms for events such as conferences and weddings, with a bar with limited food service and no entertainment other than TVs, a small retail shop selling wine, beer, snacks and sundries to hotels guest, no on-site restaurant, and serving alcohol associated with on-site conferences and weddings, has reduced concerns. This use although it may result in more calls related to alcohol use is not expected to result in repeated calls and should nuisance concerns result from this activity, the Community Development Director has the ability to call this use permit back to the Planning Commission for review. Therefore, should unforeseen negative impacts occur, the permit may be revoked. The application project site is not located in an area of “Undue

Concentration” of on-sale retail licenses.

EXHIBITS

- A:** Location
- B:** Photographs
- C:** Resolution 2016-XX Recommending Approval of a CEQA Addendum, Planned Development Permit, Architectural Review Permit and Use Permit (alcoholic beverage sales). Attached Conditions of Approval and Mitigation Monitoring Reporting Plan
- D:** Architectural Review Committee Minutes (December 10, 2015)
- E:** CEQA US Navy Plan and Its Environs Specific Plan Amendment 2015 SEIR Addendum
- F:** U.S. Navy Site and Its Environs Specific Plan Design Guidelines
- G:** Operations Statement
- H:** Green Building Design Techniques and Green Building Design Techniques During Construction
- I:** Site Lighting recommended by the ARC 12/15/15
- J:** C/CAG ALUC letter dated March 3, 2016, Height Consistency Determination
- K:** FAA “Determination of No Hazard to Air Navigation” letter dated 12/11/15 for the hotel, and letter dated 11/30/15, for construction (crane)
- L:** Transportation Demand Management Plan (TDM) dated February 14, 2016
- M:** Project Plans

Location Map - Admiral Court
APNs: 020-013-250, 020-013-260

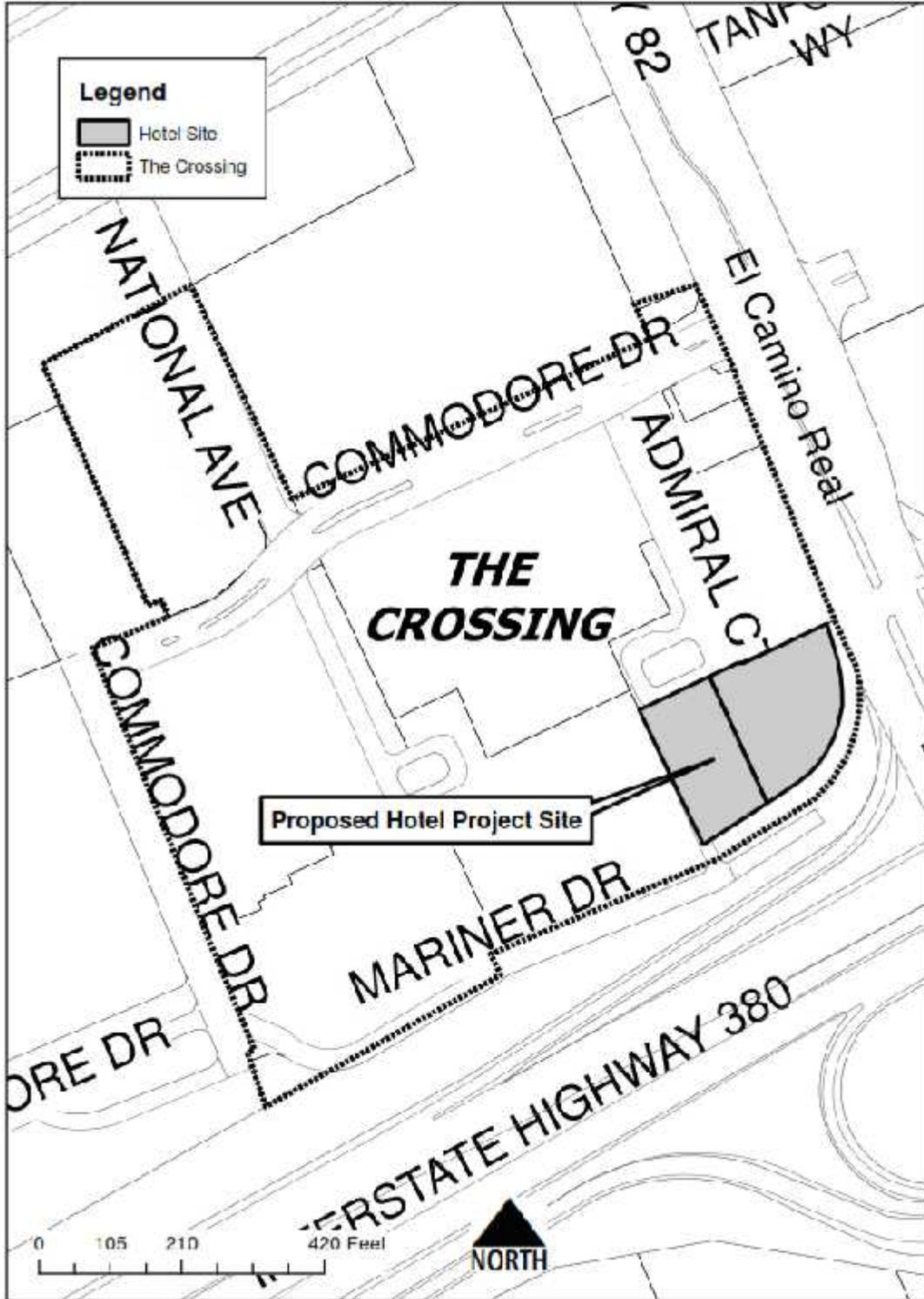


Exhibit A- Site Location

Exhibit B – Photographs



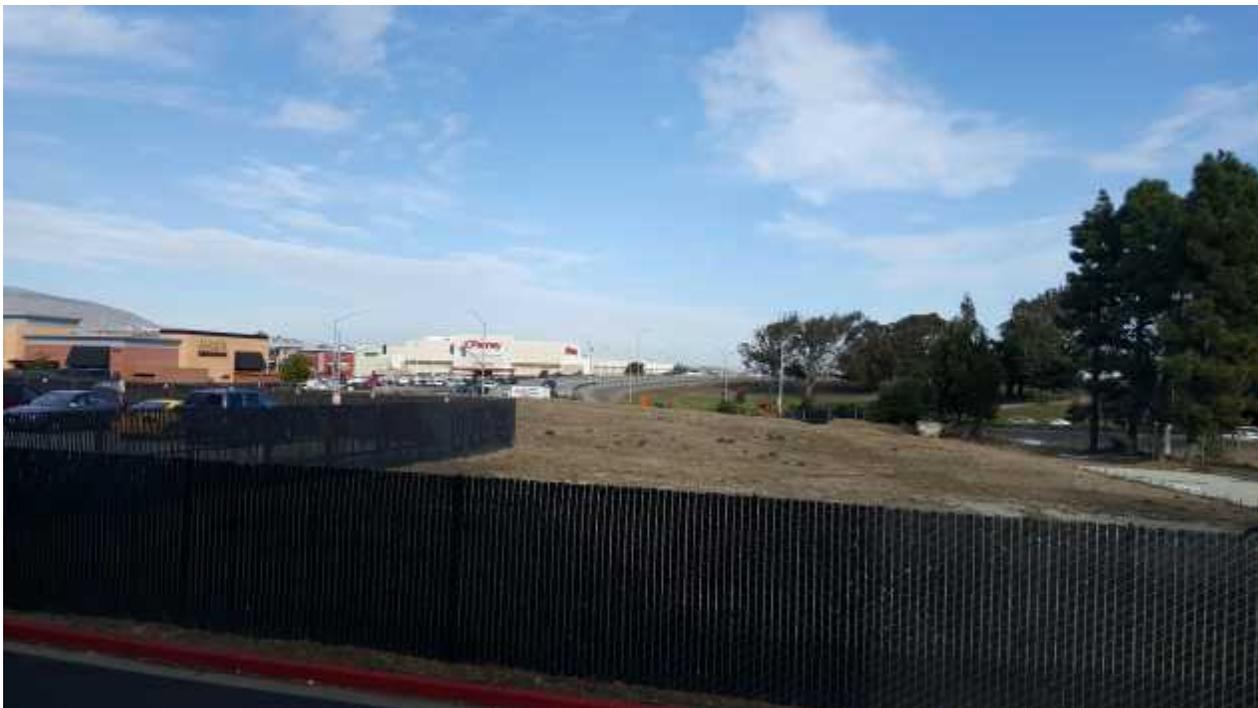
View to southwest towards I-380, temporary parking area



View to South within the perimeter fence



View to east toward The Shops at Tanforan, El Camino Real and I-380



View to East El Camino Real and on ramp to I-380 west



View to west of site from El Camino Real



View to north – The Crossing fountain and plaza to the left and the restaurant

RESOLUTION NO. 2016-__

RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF SAN BRUNO ADOPTING THE ADDENDUM TO THE SEIR FOR THE U.S. NAVY SITE AND ITS ENVIRONS SPECIFIC PLAN EIR AND APPROVING A PLANNED DEVELOPMENT PERMIT AN ARCHITECTURAL REVIEW PERMIT, AND USE PERMIT FOR ALCOHOL SALES, FOR THE PROPOSED CROSSING HOTEL LOCATED ON ADMIRAL COURT AND COMMODORE DRIVE (ASSESSOR'S PARCEL NUMBERS 020-013-250; APN 020-013-260; AND A PORTION OF AN ADJACENT PARCEL 2, APN 020-013-220)

WHEREAS, OTO Development, LLC ("Applicant") submitted an application to develop a five-story, 152-room hotel, that is approximately 69'-4" in height, containing 99,022 square feet with 167 parking spaces (60 surface and 107 subterranean spaces) and 3,000 square feet of meeting space ("Project") for the certain 1.52 acre site located on Admiral Court and Commodore Drive within The Crossing Development in the City of San Bruno and more particularly described as Assessor's Parcel Numbers: 020-013-250 (parcel 7), APN 020-013-260 (parcel 8); and a portion of the adjacent parcel 2, APN 020-013-220 (under separate ownership), ("Property"); and

WHEREAS, in order to develop the Project, the Applicant has submitted an application to the City of San Bruno for approval of the following: a Planned Development Permit an Architectural Review Permit, and a Use Permit for alcohol sales, pursuant to San Bruno Municipal Code (S.B.M.C.) Sections 12.96.190, 12.96.200, 12.84.210, and 12.108.010; and

WHEREAS, the City adopted the U. S. Navy Site and Its Environs Specific Plan and certified an associated program Environmental Impact Report (EIR) for the project site on January 9, 2001; and

WHEREAS, the City amended the Specific Plan on September 8, 2015, to decrease the size of the hotel and certified a Supplemental EIR (SEIR) to the Specific Plan EIR due to changes in the development capacity of the site resulting from construction of earlier phases of The Crossing Development and to ensure consistency with the 2012 Comprehensive Land Use Compatibility Plan for the San Francisco International Airport. The SEIR analyzed development of a 152-room hotel, identified potentially significant environmental impacts, and updated mitigation measures in the program EIR. The analysis determined that the identified mitigation measures would reduce all potentially significant impacts to less than significant level after mitigation; and

WHEREAS, an Addendum to the certified 2015 SEIR and the 2001 program EIR was prepared in accordance with Public Resources Code 21166 for the proposed project under CEQA Guidelines (sections 15162 and 15164) for projects in which only minor changes or additions are necessary and none of the conditions necessary for preparation of a subsequent EIR or Negative Declaration are present. Because the proposed project is identical to the hotel analyzed in the SEIR and there have been no substantial changes in the circumstances under which the project will be undertaken, and there is no new information that would result in new or substantially more severe impacts of the project or warrant new mitigation measures or alternatives, none of the conditions necessary for preparation of a subsequent EIR or Negative Declaration are present. Based on an examination of the 2015 SEIR and the 2001 program EIR, the potential environmental impacts associated with the proposed hotel project have been adequately analyzed and covered in the prior EIRs, and no further review or analysis under CEQA is required; and

WHEREAS, on November 12, 2015 and December 10, 2015, the Architectural Review Committee reviewed the application and provided a favorable recommendation of the Project with comments to be forwarded to the Planning Commission; and

WHEREAS, the project was referred to the City/County Association of Governments (C/CAG) Airport Land Use Commission (ALUC) for a height consistency determination. On February 11, 2016 the ALUC Commission determined that the hotel would be consistent with the applicable airport land use policies and criteria contained in the Comprehensive Airport Land Use Compatibility Plan for Environs of San Francisco International Airport (SFO ALUCP) with two conditions for sound attenuation and the execution of an aviation easement granted to the City and County of San Francisco as operator of SFO; and

WHEREAS, a Notice of Public Hearing was mailed on April 8, 2016, and duly posted in the San Mateo Times on Saturday, April 9, 2016, for consideration of the project; and

WHEREAS, on April 19, 2016, the Planning Commission of the City of San Bruno held a duly-public hearing pursuant to Section 65353 of the California Government Code to make a CEQA determination and consider the Planned Development Permit and Architectural Review Permit, and Use Permit for alcohol sales San Bruno Municipal Code.

NOW, THEREFORE BE IT RESOLVED by the Planning Commission of the City of San Bruno, based on the facts in the staff reports, written and oral testimony, and exhibits presented:

1. Based upon the analysis, findings, and conclusions of the 2015 SEIR, as well as those of the 2001 EIR and the Addendum prepared for the project and comments and testimony received, the Planning Commission hereby finds that the Crossing Hotel Project is identical to the hotel assumed in the SEIR analysis; there are no substantial changes to the project. Furthermore, there have been no substantial changes in the circumstances under which the project will be undertaken. Additionally, there is no new information that would result in new or substantially more severe impacts of the hotel project, or that new mitigation measures or alternatives are warranted. The Crossing Hotel Project therefore meets the requirements for an Addendum and that the Addendum reflects the Planning Commission's independent judgment and analysis.
2. With respect to the Planned Development Permit the Planning Commission hereby finds that the proposed project is consistent with the requirement of the applicable Planned Development District Regulations and Standards (i.e. Development Plan) for the U. S. Navy Site and Its Environs Specific Plan.
3. With respect to the Architectural Review Permit, the Planning Commission hereby finds:
 - a. That the location, size and intensity of the proposed operation will not create a hazardous or inconvenient vehicular or pedestrian traffic pattern, taking into account the proposed use as compared with the general character and intensity of the neighborhood; and
 - b. That the accessibility of off-street parking areas and the relation of parking areas with respect to traffic on adjacent streets will not create a hazardous or inconvenient condition to adjacent or surrounding uses; and
 - c. That sufficient landscape areas have been reserved for the purposes of separating or screening service and storage areas from the street and adjoining building sites, breaking up large expanses of paved areas, and separating or screening parking areas from the street and adjoining building areas from paved areas and to provide access from buildings to open areas. In addition, that adequate guarantees are made, such as the filing of a performance bond, to insure maintenance of landscaped areas; and

- d. That the proposed development, as set forth on the plans, will not unreasonably restrict or interfere with light and air on the property and on other property in the neighborhood, will not hinder or discourage the appropriate development and use of land and buildings in the neighborhood, or impair the value thereof; and is consistent with the design and scale of the neighborhood; and
 - e. That the improvement of any commercial structure, as shown on the elevations as submitted, is not detrimental to the character or value of an adjacent residential district;
 - f. That the proposed development will not excessively damage or destroy natural features, including trees, shrubs, creeks and rocks, scenic corridors, and the natural grade of the site; and
 - g. That the general appearance of the proposed building, structure, or grounds will be in keeping with the character of the neighborhood, will not be detrimental to the orderly and harmonious development of the City, and will not impair the desirability of investment or occupation in the neighborhood; and
 - h. That the proposed development is consistent with the general plan.
4. With respect to the Use Permit for alcohol sales, the Planning Commission hereby finds:
- a. The activity or use does not jeopardize, endanger or result in adverse effects to the health, peace or safety of persons residing or working in the surrounding area.
 - b. The activity or use does not result in repeated nuisance activities or police interventions within the premises or in close proximity of the premises.
 - c. The activity or use does not result in violations to any applicable provision of any other city, state, or federal regulation, ordinance or statute.
 - d. The upkeep and operating characteristics of the activity or use are compatible with and will not adversely affect the livability or appropriate development of the neighborhood.
 - e. The premises are not located in an area of Undue Concentration.
5. The Planning Commission hereby approves the SEIR Addendum, Planned Development Permit, Architectural Review Permit, and Use Permit for alcohol sales, subject to the conditions of approval attached hereto as Attachment 1.

Dated: _____

 Planning Commission Chair

ATTEST:

APPROVED AS TO FORM:

 Planning Commission Secretary
 David Woltering

 City Attorney
 Marc Zafferano

I, David Woltering, Planning Commission Secretary, do hereby certify that the foregoing Resolution was duly and regularly passed and adopted by the Planning Commission of the City of San Bruno on this 19th day of April, 2016, by the following vote:

AYES: Commissioners: _____

NOES: Commissioners: _____

ABSENT: Commissioners: _____

Attachment 1

CONDITIONS OF APPROVAL AND MITIGATION MONITORING AND REPORTING PLAN

OTO Development, LLC The Crossing - Springhill Suites Hotel

I. General Conditions

Community Development Department

1. All conditions of approval herein shall apply to the project in its entirety, regardless of the individual department under which the condition is listed. These conditions of approval and any other conditions associated with any further approvals of the OTO Development, LLC, The Crossing Springhill Suites hotel project, shall run with the land, and any and all successors in interest of the property shall comply with all conditions of said approval.
2. After the close of escrow, the developer shall indemnify, defend, and hold harmless the city, its officers, employees and agents, from any and all claims and lawsuits from third party(s) involving or related to the city's consideration and/or approval of the developer's application for development.
3. The project shall be built substantially in compliance with the plans reviewed by the Planning Commission on April 19, 2016, labeled "Springhill Suites", except as required to be modified by these Conditions of Approval. Any modification to the approved plans shall require review and approval by the Community Development Director.
4. Applicant shall obtain a City of San Bruno building permit before construction can proceed.
5. Prior to Certificate of Occupancy, all pertinent Conditions of Approval, Mitigation and Monitoring measures and all improvements shall be completed to the reasonable satisfaction of the City of San Bruno. A temporary Certificate of Occupancy (TCO) will be considered for stocking, placement of furnishings and training.
6. Applicant/developer shall provide evidence of an ingress/egress easement for the privately owned streets within The Crossing (Admiral Court and Commodore).
7. Applicant/developer shall provide evidence of a maintenance and ingress/egress agreement for separately owned Assessor's Parcel Number (APN): 020-013-220.
8. The project shall comply with all aspects of the 2013 California Building Code.
9. The applicant shall pre-wire conduits to allow for adaptation for solar.
10. Sustainability Requirements:
 - a. Install a minimum of 6,000 square feet of solar photovoltaic panels on the rooftop or an alternative photovoltaic system that meets the requirements of the environmental document (SEIR adopted September 8, 2015).
 - b. Improve the building envelope energy efficiency 15% over 2013 Title 24 standards understanding that the "envelope" is defined as the exterior walls, roof and fenestration;
 - c. Incorporate high efficiency lighting (such as LEDs, metal halide post top, metal halide cobrahead or cutoff lights, or high pressure sodium cutoff lights) into public areas, such as parking lots, garages, and other exterior areas to achieve at least 15% lighting energy reduction compared to the use of mercury cobrahead lights;

- d. Install energy efficient appliances that comply with the most recent U.S. EPA Energy; Star criteria, including refrigerators, dish washers, fans, and clothes washers;
 - e. Incorporate water conservation strategies, including the installation of low flow faucets, toilets, and showers, as well as water efficient irrigation-systems;
 - f. Institute recycling and composting services which is expected to achieve at least a 10% reduction in waste disposed.
11. The applicant/owner shall enter into a shared parking agreement with the adjacent restaurant located in the El Camino Real Commercial Center (Jack's Restaurant). Although the hotel meets the parking requirements for on-site hotel guest parking and for the meeting space, off-site parking could be negotiated to meet the needs of event parking between the two properties. An executed copy of the agreement shall be provided to the Community Development Director for review and approval prior to occupancy.

The shared parking agreement shall be structured in a manner that only extra capacity parking between the users would be offered for shared use. It is understood that the individual uses must satisfy their respective parking requirements in accordance with City standards and approvals first, and only during periods when parking demand is less and extra parking capacity is available, would shared use of parking be considered to the satisfaction of the Community Development Director.

Public Services

12. New driveway approaches shall be installed in accordance with the City Standard Details.
13. San Bruno Water Division will operate and maintain water facilities up to the water meters. The Applicant shall design and construct water facilities according to San Bruno Standard Details and Specifications. During construction, only San Bruno Water Division water personnel will be allowed to operate existing water facilities including water valves to facilitate any shut-downs required for construction.
14. All private utilities (storm, sanitary, water, electric, gas, etc.) and private street facilities within the development shall be maintained and repaired by the Applicant and its successors and shall be memorialized in maintenance and operations agreement.
15. No fences, retaining walls, permanent structures or trees/landscaping with deep root structures shall be placed or constructed within public easements or public right of way.
16. Water efficient irrigation systems shall be used throughout all landscaped areas in accordance with the State of California Model Water Efficient Landscape Ordinance.
17. Project shall incorporate landscaping that minimizes irrigation and runoff, promotes surface infiltration, minimizes the use of pesticides and fertilizers, and incorporates other appropriate sustainable landscaping practices such as Bay-Friendly Landscaping.
18. The project geotechnical report shall confirm existing soils types and the Ksat rate, if applicable. It shall also include seasonal high groundwater levels.
19. The Geotechnical Engineer who prepared the geotechnical report shall review all improvement plans prior to submittal of plans to the City and conduct any

- inspections, testing or other actions during construction that are called for the geotechnical report.
20. If the geotechnical report reveals significant future settlement will occur, all surface drainage systems shall be designed to provide positive drainage after settlement, and shall be satisfactory to the City Engineer.

II. Prior to Building Permit Issuance

Community Development

21. Applicant shall file a declaration of acceptance of the following conditions by submitting a signed copy of the Summary of Hearing to the Community Development Department within thirty (30) days of Planning Commission approval. Until such time as the Summary is filed, Planned Development Permit (PDP15-002), Conditional Use Permit, (UP15-006) and Architectural Review Permit (AR15-002) shall not be valid for any purpose. The entitlements will become effective simultaneously with close of escrow. Construction documents shall be submitted, reviewed and a Building Permit ready for issuance at the close of escrow. The Building Permit must be issued and construction work commenced within 30 days of the close of escrow.
22. The Planned Development Permit, Conditional Use Permit, and Architectural Review Permit shall expire if the Purchase and Sale Agreement (PSA) is terminated or if there is a default on the PSA.
23. The signed copy of the Conditions of Approval shall be photocopied and included as a full size page in the Building Division set of drawings.
24. Prior to securing a building permit, the applicant, owner, and general contractor shall meet with Planning, Building, and Public Services staff to ensure compliance with the conditions of approval during the construction process.
25. Provide exit analysis including computation, calculating size, travel distance and location of requires exits on all floors. Means of egress shall comply with 2013 California Building Code, Chapter 10. Remove all accessory use areas and remove and relocate access doors to the commercial trash room in exit passageway.
26. Provide Civil drawings clearly showing locations of existing and proposed utilities.
27. A detailed recycling and garbage plan shall be approved by the Community Development Director, the Building Department, the Fire Marshal, and Recology. (Also see MMRP Utilities and Service Systems Mitigation Measure 17.c.)
28. All proposed business identification signs shall require prior city approval and the approved Master Sign Program in accordance with Chapter 12.104 of the Municipal Code.
29. The applicant shall file the required materials for the review and approval of a Lot Line Adjustment to merge the two parcels (APN: 020-013-250 and 020-013-260) according to SBMC Chapter 12.52. The Lot Line Adjustment is required to be completed and recorded prior to the issuance of building permit or close of escrow, whichever occurs first.
30. All roof mechanical equipment shall not be higher in elevation that the closest top of parapet measured perpendicularly from the equipment or be screened from public view in accordance with SBMC Chapter 12.84.180.1 and shall be submitted for review and approval by the Community Development Department Director.
31. Applicant shall submit a final landscaping and irrigation plan for staff's review and approval. The landscape shall include three inches of mulch to retain soil moisture, and be maintained weed free condition and plants not in a healthy thriving condition

- or dead shall be replaced.
32. The use of irrigation for landscape improvements shall contain either drip irrigation or low-flow overhead sprinklers. All trees shall be irrigated by bubblers. Trees shall be irrigated on a separate valve than other plant material. All irrigation valves shall contain a gate valve prior to the valve to allow for isolation and valve maintenance.
 33. The applicant shall submit a detailed construction and staging plan including methods and means for providing pedestrian protection and security during construction that shall be reviewed and approved by the City Engineer and Chief Building Official.
 34. A plan showing the location of any temporary contractor's storage yard or construction trailer on the property, including security fencing, lighting and worker vehicles parking, shall be submitted to the Community Development Director for approval prior to installation and prior to building permit issuance.
 35. Off-site construction staging requires the submittal of an application for a Temporary Use Permit to be reviewed and approved by the Planning Commission.
 36. Prepare a detailed construction plan identifying the schedule for major noise-generating construction activities. The construction plan shall identify a contract person and a procedure for coordination with the occupants of nearby noise-sensitive facilities so that construction activities can be scheduled to minimize noise disturbance. The plan shall stipulate the measure that result in compliance with the noise ordinance. (Also see MMRP Noise Mitigation Measure 12.a.)
 37. Submit a complete Geotechnical analysis/report at the time of building permit submittal for the review and approval of the Building Official. Applicant shall implement the recommendations of a geotechnical report by a registered Geotechnical Engineer. The geotechnical investigation shall provide data to evaluate the geotechnical conditions of the site and provide seismic, landslide and mudslide evaluation and recommendations and recommendations for appropriate soil engineering to reduce seismic hazards.
 38. A Soils Report shall be submitted to the Building Division for review and approval.
 39. A Grading and Drainage Plan shall be submitted to the Building Division for review and approval.
 40. A Shoring Plan and a dewatering plan shall be submitted to the Building Division for review and approval.
 41. Subterranean parking requires a hydrology report with recommendations for means / methods of addressing ground and storm water collection in parking areas.
 42. Sound attenuation to be provided to reduce interior noise levels from exterior sources to CNEL 45 db or lower. (Also see MMRP Hazards and Hazardous Materials Mitigation Measure 8.e.)
 43. Avigation easement to be granted to the City and County of San Francisco as operator of SFO. The avigation easement to be used in fulfilling this condition is provided in Appendix G of the SFO ALUCP. (MMRP Hazards and Hazardous Materials Mitigation Measure 8.e.)

Public Services

44. Prior to the issuance of any permits, certificates of insurance shall be provided to the City verifying that both the Applicant and any contractors have general liability insurance. The amount and type of insurance shall be reviewed by the City and shall

- be sufficient to cover damages that may result from construction and operations. The insurance limits shall be as required by the City Attorney. Combined single limit coverage and the policy shall be subject to review and approval of the City Attorney.
45. Applicant shall hold a preconstruction conference with City staff. The Applicant shall arrange for the attendance of the construction managers, contractor, and all subcontractors who are responsible for installing public improvements, grading and erosion control measures.
 46. The Applicant shall pay all required utility fees and post all applicable bonds for infrastructure improvements to be dedicated to the City.
 47. At the time that any building permit is issued for any new building, the Applicant shall pay for all current, on-site service connection fees (including but not limited to Water and Wastewater).
 48. The Applicant shall dedicate on all pertinent maps any and all public utility easements required for all public utilities on on the parcels.
 49. The applicant shall submit a Traffic Control Plan for the construction period to the satisfaction of the City Engineer prior to construction related activities.
 50. Existing on-street parking shall not be negatively impacted by project construction without permission from the Public Services Department.
 51. The Applicant shall provide the name and 24-hour emergency phone number of the contact person in charge of construction.
 52. The Applicant shall apply for an Encroachment Permit from the Public Services Department for work, including any traffic control, in the public right-of-way, City easements or property in which the City holds interest.
 53. The Applicant shall install traffic signage and striping throughout the project. A STOP sign or a Yield sign with appropriate pavement legends shall be installed at the project exits using thermoplastic materials and to the satisfaction of the City Engineer.
 54. Property Owner shall enter into a Maintenance Agreement with the City to ensure long-term maintenance and servicing by the Property Owner of stormwater site design and treatment control measures according to the approved Maintenance Plan(s). The Maintenance Agreement shall be recorded against the property.
 55. A Maintenance Plan for every stormwater treatment control measure or applicable site design measure, inclusive of maintenance and inspection checklists and Maintenance Inspection Report Forms, shall be submitted to the City for review and approval prior to issuance of a grading permit. A copy of the final, approved Maintenance Plan(s) shall be made a part of the Maintenance Agreement. A copy of the final, approved Maintenance Plan(s) shall also be on file with the Engineering Division.
 56. Haul routes within San Bruno for construction shall be reviewed and approved by the City Engineer, via the City's transportation permit.
 57. The Applicant shall apply for and obtain a City Grading Permit. Rough and Finished Grading Plans shall be prepared by a California licensed Civil Engineer.
 58. Prior to the issuance of grading permit, the applicant shall provide Public Services Department with a plan indicating the amount of soil to be removed, disposal sites, the estimated number of truck trips required and the proposed haul routes. Final haul route within San Bruno shall be approved by the City Engineer. (Also see MMRP Air Quality Mitigation Measure 4.A-2 and 4.A.3.)
 59. The grading plans shall minimize the need for off haul from the Project Site. Design shall incorporate all elements of the applicable geotechnical report(s) and include a construction stockpile plan in compliance with all Best Management Practices

- (BMPs). The grading plans shall be considered and signed by the Geotechnical Engineer indicating that plans are in compliance with the geotechnical report and be subject to review and approval of the City Engineer.
60. The Applicant shall receive approval of the stormwater management plan from a third-party reviewer appointed by the City prior to building permit issuance. The Applicant shall pay the entire cost of the third-party review.
 61. Applicant shall prepare a Stormwater Management Plan (SWMP) that illustrates full compliance with Section C.3 of Order R-2-2009-0074 (NPDES Permit No. CAS612008) and compliance with the San Mateo Countywide Pollution Prevention Program (SMCWPPP). (MMRP Hydrology and Water Quality Mitigation Measure 9a and 9c.)
 62. Grading plans with appropriate erosion control measures shall be required for the development. Grading plans shall show all adjacent properties sufficiently to assure that the proposed grading does not negatively impact adjacent lands and shall incorporate drainage features necessary to assure continued drainage without erosion and drainage entering from adjacent properties. (Also see MMRP Air Quality Mitigation Measure 4.A-2 and 4.A-3.)
 63. A Storm Water Pollution Prevention Plan (SWPPP) shall be submitted to the City of San Bruno and the Regional Water Quality Board subsequent to the approval and issuance of grading and building permits, and prior to the approval of the Improvement Plans. (Also see MMRP Hydrology and Water Quality Mitigation Measure 9a and 9c.)
 64. Prior to the issuance of a grading permit, Applicant shall obtain from the California State Water Resources Control Board a General Construction Activity Storm Water Permit under the National Pollutant Discharge Elimination System (NPDES), if applicable. Applicant shall comply with all requirements of the permit to minimize pollution of storm water discharges during construction activities. The permit shall include any work by public and/or private utilities performing work on behalf of Applicant, if applicable. (Also see MMRP Hydrology and Water Quality Mitigation Measure 9a and 9c.)
 65. Temporary erosion control structures shall remain in place until the site is completely developed. A Maintenance Plan shall be submitted to the City Engineer indicating contractor responsibility for complying with the erosion control plan for the duration of the construction project. The Maintenance Plan shall include dust control, but is not limited to BMP's as outlined in the Storm Water Pollution Prevention Plan (SWPPP), and shall be to the satisfaction of the City of San Bruno and meet all Regional Water Quality Control Board (RWQCB) requirements. (Also see MMRP Hydrology and Water Quality Mitigation Measure 9a and 9c.)
 66. The erosion control plan sheets shall be included as separate, numbered sheets in the grading plan of the improvement plans. The Applicant shall pay for implementing the erosion control measures depicted on the plan.
 67. All drainage improvements shall be to the satisfaction of the City Engineer.
 68. Backflow protection on water services shall be required. The backflow preventer shall be above grade, and shall be located on private property, accessible to Public Services staff from the outside for testing and subject to the City Engineer's approval.
 69. Regarding grading, area drain grates in landscaped or dirt areas shall be cast iron, and shall be a minimum of 0.75 square foot in area.
 70. All sidewalks, curb & gutter shall be monolithic, and all transverse grades shall be 2%. Gutters shall have grades that create positive flow into the City's stormwater

- system. If applicant/developer creates any ponding within the public right-of-way, the applicant/developer is fully responsible for the repair; if the applicant/developer contributes, the applicant/developer pays a proportionate share of costs for repair.
71. Roof drains shall drain away from the building and be directed to landscaping or a stormwater treatment measure.
 72. All measures checked "yes" in Worksheets listed in the C.3 and C.6 Development Review Checklist shall be strictly adhered to. Measures shall be clearly labeled and dimensioned on the Improvement Plans.
 73. Roof leaders and downspouts to the treatment measures shall be clearly shown on the Improvement Plans.
 74. Appropriate sizing calculations for biotreatment measures shall be shown on the improvement plans.
 75. Any street lighting or utilities located within biotreatment areas shall be verified and approved by City.
 76. On-site storm drain inlets shall be clearly marked with the words "No Dumping! Flows to Bay," or equivalent using thermoplastic material or a plaque.
 77. Trash storage areas, wash areas, loading docks, repair/maintenance bays, and equipment or material storage areas and fueling areas shall be completely covered and asphalt bermed to ensure that no stormwater enters the covered area. Covered areas shall be sloped to drain to area drains connected to the sanitary sewer system.
 78. Interior level parking garage floor drains, and any other interior floor drains, shall be connected to the sanitary sewer system, subject to the local sanitary sewer agency's authority and standards.
 79. Swimming pools, hot tubs, spas and fountains shall have a connection to the sanitary sewer, subject to the local sanitary sewer agency's authority and standards. This connection could be a drain in the pool to the sanitary sewer or a cleanout located close enough to the pool so that a hose can readily direct the pool discharge into the sanitary sewer cleanout.

Fire Department

80. Safety plan for construction process to be approved by the Fire Marshal prior to building permit issuance.
81. FDC/double-detector check valves and fire hydrant locations to be approved by Fire Marshal.
82. Building water flow requirements for fire suppression to comply with CFC Appendix B.
83. Provide NFPA 13 Fire Sprinkler system to be installed throughout building and garages. System under separate fire permit.
84. Fire Sprinkler system to include Class I combination standpipe connections with 21/2 inch Fire Department threading at each stairwell and floor landing, and to include roof access points. In addition, combination standpipe connections to be provided throughout building in protective cabinets (stairwell and roof level standpipes may be exposed). Where the most remote portion of a sprinklered floor or story located in excess of 200 ft (61m) of travel distance from a required exit containing or adjacent to a hose connection, additional hose connections shall be provided, in approved locations.
85. Provide signage to identify location of garage standpipes.
86. Standpipe devices to each provide min. 250 gallons per minute at 20 PSI.

87. Building fire sprinkler system Fire Department Connections (FDC's) shall be located on the address side of the building in an approved locations. Separate double detector check valves (DDCV's) with incorporated FDC's for the building shall be provided.
88. In lieu of a fire sprinkler bell, an exterior rated horn/strobe shall be mounted eight (8) feet above grade immediately adjacent to the building's FDC.
89. Fire hydrant locations and distribution to comply with CFC Appendix C.
90. Addition of new hydrants and/or modification to existing hydrants to be performed under separate fire permit.
91. Hydrants to be a Clow 960 model. City hydrants to be yellow. Private hydrants to be white.
92. Elevators: Shunts to be removed. Sprinklers omitted from elevator shafts, with the exception of the bottom of the shafts. Sprinklers to also be omitted from electrical rooms. Provide smoke detectors on top of shafts and equipment rooms. Sprinklers to be provided at bottom of elevator shafts in accordance with SBMC section 607.6 and 903.3.1.1.1.
93. Stairwells shall be numbered to coincide with elevator locations as feasible.
94. Include comments in elevator and fire sprinkler notes on respective plans.
95. Provide Fire Alarm system throughout building and parking garage. System to be submitted under separate fire permit.
96. The fire sprinkler system shall be monitored by an approved fire alarm system which reports to a UL listed central station.
97. The fire alarm system shall be a UL Certified installation.
98. Fire alarm system to utilize horn strobe units in place of bells.
99. A master graphic annunciator panel shall be provided in the FACP room or in a clearly visible location in the lobby showing the building in alarm and type of alarm.
100. An automatic smoke detection system that activates the occupant notification system shall be installed throughout all interior corridors serving sleeping units.
101. Sleeping unit percentage designated by numbers in accordance to CFC 2013 Table 907.5.2.3.3 shall supply the proper number of visible alarm notification appliances, to be activated by both the in-room smoke alarm and building fire alarm system.
102. FACP Room to contain, or be immediately adjacent to, an exterior door.
103. Provide approved unit numbering.
104. Smoke control system (if applicable) for lower garage floors to allow for fire department override and control.
105. Unit building address numbering system will require approval of both the Fire Marshal and Building Official.
106. All rooms to be identified by use. Use min. 3 inch high lettering.
107. Garage exit signs to be visible and not obstructed.
108. Provide Fire Lane marking and signage plan.
109. Knox Boxes shall be provided at each main stairwell entrance, and at the pool gate along Commodore Drive. Two sets of keys to be provided for each Knox Box.
110. Fire extinguishers shall be mounted in cabinets and shall not be obstructed or obscured from view.
111. Manually operated flush bolts or surface bolts not permitted except storage room and /or utility rooms.

112. The unlatching of any door in exit paths shall not require more than one operation.
113. In the event of power failure, an emergency electrical system shall automatically illuminate the means of egress.
114. Exit and exit access doors shall be marked by approved exit signs readily visible from any direction of egress travel.
115. Exit signs shall be internally or externally illuminated at all times: signs shall be connected to an emergency power system that provides illumination for not less than 90 minutes in case of primary power loss.
116. FACP and other utility rooms to be identified on entry door faces.
117. Main meeting room to have occupancy posted.
118. Electrical service equipment must have a 36 inch working space at all times.
119. All drapes, hangings, curtains, upholstered fabric furniture, and other decorative material that would tend to increase the fire and panic hazard shall be made from a non-flammable material or shall be treated and maintained in a flame-retardant condition with a flame-retardant rating approved by the State Fire Marshal. Insure that ratings meet California standards.
120. Commercial cooking equipment that produces grease laden vapors will be provided with an automatic fire extinguishing system rated and labeled for its intended use.
121. Any roof access hatches shall be shall be code compliant.
122. Stairwell emergency exit doors to be labeled to not block on the exterior door surfaces.
123. Provide details that proper isolation for exit pathway shall be maintained past kitchen prep area on ground floor.
124. Provide nine sets of 8 ½ x 11 inch laminated, color-coded, floor plans and site plans as required by Fire Marshal. To be supplied with electronic file back-up.
125. Emergency Vehicle Access (EVA) paths to be rated for 60,000 lb. vehicle load.
126. Path of travel to be adequate to reach the interior West courtyard (by pool) from street level with a rescue ladder measuring 20 feet, 2 inches (bedded length of a 35 foot extension ladder) in length without obstruction, other than required gates needed to secure pool.

Police Department

127. Addressing:
Address numbers for the business are to be on a contrasting background, easily visible from the street. The address numbers also must be visible at night.
128. Lighting:
 - a. Parking lots and associated garages, driveways, circulation areas, aisles, passageways, recesses, and grounds contiguous to buildings shall be provided with lighting of sufficient wattage to provide adequate illumination to make clearly visible the presence of any person on or about the premises during the hours of darkness.
 - b. All exterior doors shall have their own light source which will adequately illuminate entry/exit areas at all hours in order to:
 - i. Make any person on the premises clearly visible.
 - ii. Provide adequate illumination for persons entering and exiting the building.

129. Landscaping:
 - a. Landscaping shall be of the type and situated in locations to maximize observation while providing the desired degree of aesthetics. Security planting materials are encouraged along fence and property lines and under vulnerable windows. Landscaping shall not conceal doors or windows from view, obstruct visibility of the parking lot from the street or business buildings, nor provide access to the roof.
130. Line of sight/natural surveillance:
 - a. Stairwell landings shall allow for a sixty-inch turning radius for use by the police and fire departments or accordance with applicable Fire and Building Codes.
 - b. Single and double binned trash enclosures should be located at the perimeter of the parking lot or contiguous to exterior building doors.
131. Parking structure:
 - a. Metal halide, or other bright white light source, should be utilized. No foot candles less than the Building Code should exist inside the structure.
 - b. Alcoves and other visual obstructions that might constitute a hiding place should be eliminated whenever structurally possible. Pillars, columns and other open construction should be utilized over a solid wall design.
 - c. Bars or grating should be utilized to impede pedestrian access to the structure from ground-level openings. Landscaping contiguous to this grating should be the type that does not block natural light fenestration into the garage.
 - d. CCTV surveillance should be utilized throughout the structure, including the garage.
132. Signage/parking lot:
 - a. All entrances to parking areas shall be posted with appropriate signs per 22658(a) CVC, to assist in removal of vehicles at the property owners/managers request.
 - b. All handicap parking stalls shall be appropriately painted and marked as per the California Vehicle Code.
 - c. Designated fire lanes shall be properly painted and signage that reflects the red zone is a fire lane, for proper enforcement purposes.
 - d. Compact-parking spaces shall be clearly marked on the pavement.
133. Permits and licenses:
 - a. The applicant must obtain an A.B.C. license to sell alcohol on the premises and comply with all their rules and regulations.
134. Miscellaneous:
 - a. It is highly recommended that the applicant consider installing a video surveillance system in the public areas and the garage that is capable of recording and saving any crimes that are committed on the premises.
 - b. The applicant is responsible to submit emergency contact information to the police department for after hour's emergency contact.
 - c. All new buildings shall have approved radio coverage for emergency responders within the building based upon existing coverage levels of the public safety communications systems of the jurisdiction at the

exterior of the building. This section shall not require improvement of the existing public safety communications systems. According to Section 510.1 of the California Fire Code, Emergency Responder Radio Coverage.

- d. Include a keyless entry for FOB option for first responders to the building and parking garage on a 24 hour basis.

San Bruno Cable

135. If a trench for underground utilities is otherwise needed, the developer / contractor shall be responsible for labor and construction to provide two SCH40 2-inch conduits connecting the building's MPOE to one of the San Bruno Cable's existing utility boxes located on Commodore Drive. San Bruno Cable would be responsible for pulling the fiber cables to the MPOE.
136. Should the developer choose to use San Bruno Cable's service in the future any infrastructure beyond the MPOE would need to be provided by the developer.

Improvement Plans

Public Services

137. An improvement plan for public improvements shall be submitted by the Applicant to the City for review, comment and approval. The improvement plans shall include public streets, sidewalks, sewers, storm drains, water, electrical, streetlights, other utilities, and landscaping.
138. Improvement Plans must be prepared pursuant to the City of San Bruno Municipal Code (Muni Code), State Law and Regulations and Standard Engineering Practice, where the Muni Code shall govern in the event of a conflict.
139. Applicant shall provide copies of written permission to construct within existing private property and/or private easements and to connect to existing private utility systems.
140. Applicant shall pay a deposit to the City in the amount of \$10,000 at the time Improvement Plans are submitted to the City for review. Actual costs for staff time shall be deducted from this deposit. From time to time the City may require payment of additional deposit amounts to ensure that there are adequate funds available to pay for City services. At the end of the project, any remaining deposit amount will be refunded.
141. Storm drainage improvements including any storm drain pump station shall be privately owned and maintained. Storm drain facilities and laterals, including those within existing private storm drain easements shall be privately owned and maintained. Applicant shall provide to City evidence or copies of written permission from owner of existing private storm drain facilities allowing connections proposed by Applicant.
142. The street storm drain system shall be designed to withstand a 25-year storm. The 100 year storm shall be contained within the right-of-way.
143. Hydraulic calculations for design of the storm drain system prepared by a registered civil engineer shall be submitted for City review and approval at the time that the improvement plans are submitted. The Applicant's design professional shall evaluate the project's impact to the City's storm drainage system and shall

- substantiate their conclusions with drainage calculations to the satisfaction of the City Engineer.
144. All project stormwater runoff shall be treated to meet C.3 requirements of the Municipal Regional Permit.
 145. Interceptors or other storm pollution control systems per NPDES requirements shall be installed for storm water from roadways that are not filtered by vegetated swale or other biological pretreatment facilities.
 146. A final hydrology and hydraulic report prepared by a qualified California Registered Civil Engineer shall be submitted to the City for review and approval to demonstrate full compliance with drainage system design requirement. Post-construction runoff into the storm drain shall not exceed pre-construction runoff levels.
 147. In conjunction with submittal of Grading Plans, the Applicant shall file a Notice of Intent for storm water discharge with the Regional Water Quality Control Board. A copy of the filing shall be submitted to the City Engineer as part of the required Improvement Plans for the site.
 148. Joint trenches under sidewalks may include telephone, City of San Bruno (CSB) Cable TV, electrical, communication, and gas lines. The trench width and depth shall be to the standards of the utility companies and to the satisfaction of the City Engineer. Utility clearances between utility mains, San Bruno Cable TV, sewers, structures or other objects shall be to the satisfaction of the City Engineer and comply with San Bruno Standard Specification 33 10 50 of the City Standard Specifications.
 149. Prior to approval of the improvement plans, the Applicant shall provide written approval of the development improvement plans from all affected utility companies, including, but not limited to, Pacific Gas and Electric, CSB Cable TV and Telephone demonstrating their review and approval of the proposed improvements.
 150. Applicant shall submit any project phasing (if applicable) with the Improvement Plans.
 151. Applicant shall indicate the number of sewer laterals required for the Project. Laterals are considered at the discretion of the City Engineer.
 152. Any plans must be prepared to standard engineering practice and the City of San Bruno Municipal Code (Muni Code). In the event of conflict, the Muni Code shall govern unless approved by the City Engineer.
 153. All Improvement Plans shall be submitted on 22"x34" standard plan sheets. Scale shall be sufficiently large for clarity and review. Street Improvement Plans and Profiles shall have a minimum of 1"=20' scale. The Site Plan and Grading Plans shall have a minimum scale of 1"=40'.
 154. The Applicant shall submit engineered Improvement Plans (including specifications & engineers cost estimates) for approval by the City Engineer, showing the infrastructure necessary to serve the Development. The Improvement Plans shall include, but are not limited to, all engineering calculations necessary to substantiate the design, proposed roadways, drainage improvements, utilities including City Cable TV service, traffic control devices, retaining and/or sound walls, waterlines, sanitary sewers, and storm drains, street lighting, common area landscaping and other project improvements.

Construction Process

Community Development

155. Any removal or demolition which may impact any amount of ACM shall be performed by a registered CalOSHA asbestos abatement contractor in compliance with CalOSHA and Bay Area Air Quality Management District (BAAQMD) standards.
156. Construction hours for the OTO Development Springhill Suites hotel project shall be limited to the hours between 7:00 a.m. and 6:00 p.m. for interior work and between 8:00 a.m. and 6:00 p.m. for exterior construction and vibration generating activity or more restrictive hours as determined through the approval process. Interior construction work shall be allowed on Saturdays 9:00 a.m. to 6:00 p.m. Exterior work on Saturdays between 9:00 a.m. and 6:00 p.m. and interior construction work on Sundays between 9:00 a.m. to 6:00 p.m. could be allowed (no Sunday exterior work) with written permission from the Community Development Director, requested one week in advance. Otherwise, construction and work shall be prohibited on weekends and holidays.
157. Height shall be verified and e-filed according to the FAA requirements within 5 days after the construction reaches its greatest height (FAA Form 7460-2 Part 2) and evidence of FAA approval provided to the Community Development Department Director. The FAA "Determination of No Hazard to Air Navigation" letter dated 12/11/15 for the hotel and letter dated 11/30/15 for the construction (crane) during construction. The hotel determination expires on 6/11/17 and the crane determination on 5/30/17, unless construction is commenced. (Also see MMRP Hazards and Hazardous Materials Mitigation Measure 8.e.)
158. "A height survey shall be required prior to the roof framing inspection and prior to building permit final. The height survey shall be conducted by a State Licensed Surveyor."

Public Services Department

159. Traffic control, regulatory, warning, guide signs and markings (including fire hydrant pavement markers) shall be installed in conformance with the California Manual of Uniform Traffic Control Devices, and as directed and approved by the City Engineer.
160. City streets shall not be closed permanently during construction of the project and a process shall be put in place so sufficient notification is given to the neighborhood.
161. Applicant shall coordinate installation of stormwater treatment measures with the City and shall arrange to have a municipal Special Inspector or designated third party inspector present at the time of installation. Applicant shall be responsible for all fees associated with special stormwater inspections during construction.
162. Applicant shall arrange and pay for final inspection of installed treatment measure by City's Special Inspector within 45 days of installation or project construction completion, whichever comes first.
163. Periodic site inspection shall be provided by a Geotechnical Engineer at the Applicant's expense during trenching and backfill operations. The Geotechnical

Engineer shall take compaction tests, and shall submit test results to the Public Services Department.

Fire Department

164. Fire Department access shall be maintained throughout construction, with a minimum of 20 feet of width at all locations along route.

III. Prior to Occupancy

Community Development

165. All pertinent conditions of approval mitigation monitoring measures, and all improvements shall be completed to the satisfaction of the City of San Bruno.
166. The applicant shall meet with staff and Recology regarding regularly scheduled trash, recycling, and compost collection.
167. Applicant shall submit documentation to the Building Department that the materials have been recycled in accordance with the approved Recycling and Garbage Plan. (Also see MMRP Utilities and Services Systems Mitigation Measure 17.c.)
168. The Avigation easement granted to the City and County of San Francisco as operator of SFO shall be executed and recorded and a copy of the stamped recorded document provided to the Community Development Department. (MMRP Hazards and Hazardous Materials Mitigation Measure 8.e.)
169. A height survey shall be required prior to the roof framing inspection and prior to building permit final. The height survey shall be conducted by a State Licensed Surveyor.
170. The hotel shall submit a draft Traffic Demand Management Plan (TDM) with measures as described in the San Mateo County Congestion Management Plan to reduce peak hour trips resulting from the project for the review and approval of the Community Development Department Director. The project already includes TDM measures such as providing two dedicated natural gas shuttles (or other equivalent powered shuttle system with the same level of service such as shuttles with other local hotels) to serve travelers from the San Francisco airport to the hotel and other local destinations; bicycle facilities including five short-term bicycle spaces (bike racks) and five long-term bicycle spaces (bicycle lockers) and showers and changing rooms for employees. Additional measures shall include but not be limited to:
 - a. Transit Subsidy for Employees – Each employee would be provided with a Clipper card. This will familiarize employees with available public transportation options.
 - b. Distribute Transportation Information – Each employee would be provided an informational package regarding alternate means of transportation in the immediate area.
 - c. On-site Ride Share Program – Each employee will be provided information on how to coordinate with other employees to share rides and carpool. Additionally, an information board will be installed in the break room where ride share and carpool information can be posted.
 - d. The owner/operator/tenant(s) shall provide annual reports to the Community Development Department for the first five years, and every other year

thereafter, describing the on-going implementation of the TDM measures selected for the project. If after five years the program is working effectively achieves the TDM goals and with the Community Development Director's permission, the annual reports requirement can be terminated.

The TDM Plan measures shall offset the 91 peak hour trips generated in the SEIR traffic analysis (Table 4.C-3, US Navy Plan and its Environs Specific Plan SEIR, dated May 2015).

Public Services

171. Within six months of Certificate of Occupancy, the Applicant shall retain a Civil Engineer to prepare "as-built" or "record" construction drawings, and the drawings shall be submitted in AutoCAD and PDF formats.
172. Within six months of Certificate of Occupancy, the Applicant shall prepare "Maintenance and Responsibility" drawings and shall be submitted in AutoCAD and PDF formats.
173. For all work to be dedicated to the City, Applicant shall provide six printed copies and one digital copy of any operations and maintenance manuals with all certifications, warranties, guarantees, and any permits to outside agencies e.g. the State Department of Water Resources Department (DWR).
174. Upon completion of construction, the Applicant shall repair any public or private improvements damaged by construction operations to the conditions existing prior to project construction, but not prior to Temporary Occupancy Permit (TCO) and to the satisfaction of the City Engineer.

Police Department

175. Include a keyless entry for FOB option for first responders to the building and parking garage on a 24 hour basis.
176. As required per Section 510.1 of the California Fire Code, applicant/developer shall demonstrate radio coverage throughout the building exists at the same level of coverage outside of the building.

On-Going

Community Development

177. All trash, recycling, and composting bins shall remain inside of the trash room located within the building until the time of collection. At no time shall trash, recycling, or composting bins be stored within the public right-of-way, or be visible from the public right-of-way. Regular pick up service shall be scheduled.
178. Applicant shall comply with all requirements of San Bruno Municipal Code Chapter 5.16 regarding procedures for regulating and abating graffiti. Applicant shall install measures reasonably related to the control of graffiti and/or the mitigation of the impacts of graffiti, including, without limitation, the following:
 - a. Applicant or owner shall immediately remove any graffiti at the property;
 - b. Right of access by City forces to remove graffiti;
 - c. The applicant/developer/operator shall apply paints or materials to match the existing paint color and sheen to cover or remove graffiti.;

- d. Installation and use of landscaping so as to screen or to provide a barrier to the surface or structure;
 - e. Installation and use of additional lighting;
 - f. Applicant or permittee shall store on site with sufficient matching paint and/or anti-graffiti material on demand for use in the abatement of graffiti.
179. The applicant shall implement the Parking and Transportation Demand Management plan approved by the Community Development Director. On an annual basis the Owner/Operator/Tenant shall submit a report to the Community Development Department for the first five years, and every other year thereafter, describing the on-going implementation of the Parking and Transportation Demand Management measures selected for the project. Any changes to the Parking and Transportation Demand Management plan shall require approval in writing from the Community Development Director.
180. The landscaping shall be maintained in a health growing condition and any plants that are dead or not thriving shall be replaced.
181. The landscape shall be maintained in a weed free healthy growing condition, and plants not thriving or dead shall be replaced. After one year of occupancy the landscaping shall be inspected by a qualified landscape architect who shall verify the landscape condition and make recommendations in a memo report to be submitted to the Community Development Director.
182. Traffic Demand Management Plan: the owner/operator/tenant(s) shall provide annual reports to the Community Development Department for the first five years, and every other year thereafter, describing the on-going implementation of the TDM measures selected for the project.

Public Services

183. Property Owner shall be responsible for conducting all servicing and maintenance as described and required by the stormwater treatment measure Maintenance Plan(s). Maintenance of all site design and treatment control measures shall be the owner's responsibility.
184. Approved Maintenance Plans shall be kept on-site and made readily available to maintenance crews. Maintenance plans shall be strictly adhered to.
185. By April 1 each year, Maintenance Inspection and Servicing Reports for the stormwater treatment systems shall be submitted to the City for the previous calendar year (January 1 through December 31).
186. Site access shall be granted to representatives of the City, the San Mateo County Mosquito and Vector Control District, and the Water Board, at any time, for the sole purpose of performing operation and maintenance inspections of the installed stormwater treatment systems. A statement to that effect shall be made a part of the Maintenance Agreement recorded for the property.
187. Property Owner shall be required to pay for all municipal inspections of installed stormwater treatment systems as required by the Regional Water Quality Control Board or the City.

Fire Department

188. Required means of egress shall be continuously maintained free of obstructions.

THE CROSSING SPRINGHILL SUITES HOTEL MITIGATION MONITORING AND REPORTING PROGRAM

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
Mitigation Measures					
Air Quality					
<p>Mitigation Measure 4.A-2 (H.1.a): The City shall condition approval of individual development proposals under the Specific Plan on implementation of an appropriate dust abatement program, patterned after the BAAQMD approach described herein. The following will be required for all construction activities within the project area. These measures will reduce fugitive dust emissions primarily during soil movement and grading activities, but also during vehicle and equipment movement on unpaved project sites:</p> <ol style="list-style-type: none"> 1. All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day. 2. All haul trucks transporting soil, sand, or other loose material off-site shall be covered. 3. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited. 4. All vehicle speeds on unpaved roads shall be limited to 15 mph. 5. All streets, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used. 6. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of CCR). Clear signage shall be provided for construction workers at all access points. 7. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. 8. A publicly visible sign shall be posted with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations. 	<ol style="list-style-type: none"> 1. City incorporates measure as a condition of approval. 2. Project sponsor includes dust abatement program in contractor specifications. 3. Contractor implements measures during construction. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. City reviews construction specifications to verify inclusion. 3. Project sponsor requires construction contractor to submit documentation of compliance following completion of project. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 3. Project sponsor and construction contractor 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. During construction 3. Prior to Issuance of Certificate of Occupancy 	
<p>Mitigation Measure 4.A-3: The project sponsor shall ensure that construction contract specifications include a requirement that all off-road diesel-powered construction equipment used for project improvements be equipped with engines that meet or exceed either</p>	<ol style="list-style-type: none"> 1. City incorporates measure as a condition of approval. 	<ol style="list-style-type: none"> 1. City adopts condition of 	<ol style="list-style-type: none"> 1. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to project approval. 	

OTO Development - The Crossing Springhill Suites Hotel
 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>U.S. Environmental Protection Agency (USEPA) or California Air Resources Board (CARB) Tier 2 off-road emission standards, and are fitted with Level 3 Verified Diesel Emissions Control (VDEC), which would reduce diesel particulate emissions by at least 85 percent. (Engines meeting Tier 4 [Interim or Final] emission standards automatically meet the Level 3 VDEC requirement and no additional emissions control is required.)</p>	<p>2. Project sponsor includes dust abatement program in contractor specifications.</p> <p>3. Contractor implements measures during construction.</p>	<p>approval with project.</p> <p>2. City reviews construction specifications to verify inclusion.</p> <p>3. Project sponsor</p>	<p>2. City of San Bruno</p> <p>3. Project sponsor and construction contractor City reviews construction specifications to verify inclusion.</p>	<p>2. Prior to issuance of grading permit.</p> <p>3. Prior to Issuance of Certificate of Occupancy</p>	
Biological Resources					
<p>Mitigation Measure 4.a (G.1.b): In the event that tree removal is required, a tree removal permit would be obtained from the City of San Bruno. The City would assure that the conditions contained within the appropriate tree removal permit would be followed.</p> <p>The following measures are also applicable for removed trees.</p> <ul style="list-style-type: none"> • Tree removal will not occur between February 1 and August 31 without a bird survey conducted by a qualified biologist to determine that the tree is unused during the breeding season by avian species that are protected under California Fish and Game Codes 3503, 3503.5, and 3511. If bird species are detected or active nests are observed, the District will obtain the necessary permits from California Fish and Game, and will comply with permit conditions for protecting these species, which will likely involve seasonal avoidance or construction “exclusion zones” around nest sites. Buffer zones will be avoided during construction activities until young have fledged or the nest is otherwise abandoned. • A qualified biologist shall conduct bat surveys to determine whether any mature trees that would be removed during project construction provides hibernacula or nursery colony roosting habitat. Exclusion should be conducted at specific times of the year. Winter roosts are generally occupied between October 15 and February 28, and maternity colonies are generally occupied between April 15 and August 31. Therefore, exclusion, if required, should be conducted generally between August 15 and October 15 and between March 1 and April 15. If bats are using trees that need to be removed, the roosting season of the colony should be determined and the removal of the tree conducted when the colony is using an alternate roost. 	<p>1. City incorporates measure as a condition of approval.</p> <p>2. Project sponsor presents final site plan to City that delineates extent of tree removal.</p> <p>3. If tree removal is proposed, project sponsor contracts with a qualified biologist to conduct preconstruction bird and bat surveys</p> <p>4. If tree removal is proposed, project sponsor submits plan for replacement plantings.</p>	<p>1. City adopts condition of approval with project.</p> <p>2. City reviews site plan.</p> <p>3. Conduct bird and bat surveys.</p> <p>4. City reviews landscaping plan.</p>	<p>1. City of San Bruno</p> <p>2. City of San Bruno</p> <p>3. Project sponsor or contractor</p> <p>4. City of San Bruno</p>	<p>1. Prior to project approval.</p> <p>2. Prior to issuance of grading or demolition permit.</p> <p>3. Prior to construction activities.</p> <p>4. Prior to issuance of grading or demolition permit.</p>	

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 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<ul style="list-style-type: none"> The City of San Bruno will require replacement of all removed street trees with native trees and will require that the replaced trees be incorporated into a landscape plan for site development that is submitted to the City for review and approval. 					
<p>Mitigation Measure 4.e (G.1.a): The City of San Bruno, to the extent feasible, will avoid removal of, or damage to all street trees, as designated by the City of San Bruno Tree and Planting Ordinance. The following presents limitations for construction within and around trees to be preserved:</p> <ul style="list-style-type: none"> A certified arborist shall be contracted to perform a tree survey of the site to confirm the presence or absence of heritage trees within the project site prior to construction. The survey will also confirm the presence of trees that are to remain onsite according to the Tree Disposition Plan (2001). Should heritage trees be present within the project site, a certified arborist shall determine appropriate protective measures to be implemented during construction and which may include but is not limited to the following: <ul style="list-style-type: none"> A certified arborist shall be consulted prior to construction to accurately locate root protection zones and identify other specific measures that would limit potential indirect impacts on trees that may be encroached upon. For all development that would encroach into the feeder root zone (drip-line) or a 12-foot radius from the trunk, whichever is greater, of a preserved tree, special construction techniques to allow roots to breath and obtain water will be required, as determined by the City of San Bruno (e.g., use hand equipment for trenching, protect natural resources with highly visible protective fencing, allow only one pass through an area with preserved trees). The existing ground surface within the drip-line of any tree will not be cut, filled, or compacted unless otherwise approved by the City of San Bruno. Excavation adjacent to any trees, when permitted, will be in such a manner that will cause only minimal root damage. Permission and inspection will be required prior to back-filling. Construction staging areas shall be designated on plans and prohibit parking, loading, digging (especially trenching), and grading during all construction activities within root protection zones of all trees. There will be no parking or storing of vehicles, equipment, machinery, construction materials, or construction trailers, and no mechanical 	<ol style="list-style-type: none"> City of San Bruno incorporates measure as a condition of approval. Applicant contracts with a certified arborist to perform a tree survey on the project site. 	<ol style="list-style-type: none"> Verify and approve the incorporation of the measure into the construction plan. Conduct tree survey and present results to City. 	<ol style="list-style-type: none"> City of San Bruno Applicant and its contractor(s); City of San Bruno 	<ol style="list-style-type: none"> Prior to project approval. Prior to construction activities. 	

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 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>excavation, construction of buildings or dumping of oils or chemicals within the drip-lines of any trees.</p> <p>Prior to the start of any clearing, stockpiling, trenching, grading, compaction, paving, or change in ground elevation on a site with saved protected trees, fencing will be installed at the drip-line. Prior to grading or issuance of any permits, the fences may be inspected and the location thereof approved by appropriate county staff. The county requires the installation of a 6-foot-high chain-link fence around the drip-line of preserved trees during construction and demolition phases. A 4-inch layer of chipped bark mulch should be placed over the soil surface within the fenced dripline prior to installing temporary fencing. Suitable mulch must contain bark "fines." Maintain this layer of mulch throughout construction.</p> <ul style="list-style-type: none"> - Pruning shall be overseen by a certified arborist for all protected trees, and should be done to clean and raise canopy per International Society of Arboriculture pruning standards. - A drainage plan shall be designed that will avoid heritage trees. - Construction drawings shall accurately locate areas to be avoided such as tree trunks and root protection zones. - A pre-construction meeting conference shall be held with contractors to review BMPs and require bonding and fines to ensure the replacement of any inadvertently damaged trees. - Whenever possible, existing grade shall be maintained within the fenced portion of the dripline. 					
Hazards and Hazardous Materials					
<p>Mitigation Measure 8.a (L.1.d): The City of San Bruno will require all proposed project sponsors to submit a Phase II report, based on the September 17, 2013 Phase I findings, and subsequent reports as may be required by the City of San Bruno, completed no more than 18 months prior to approval of a proposed project by City Council to assure no additional contamination is present from overlooked USTs or other unknown sources. The City of San Bruno will require that any project sponsor incorporate the recommendations of the Phase II report into the design of the proposed project.</p>	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor and its contractor(s) shall retain a qualified environmental professional to prepare a Phase II report. 3. Project sponsor and its contractor(s) shall incorporate the recommendations of the Phase II 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. Review construction plans for inclusion of Phase II results. 3. City reviews results of Phase II and any 	<ol style="list-style-type: none"> 1. City of San Bruno 2. Project sponsor and its contractor 3. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to Project Approval. 2. Prior to ground-disturbing activities. 3. Prior to ground disturbing activities. 	

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 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
	report into the design of the proposed project.	applicable project revisions.			
<p>Mitigation Measure 8.e</p> <p>C.1a: The City of San Bruno will require that all building heights and associated roof structures proposed under the Specific Plan be consistent with the height limitations defined by FAR Part 77. Prior to issuance of any demolition or construction permits, the City of San Bruno shall require the project sponsors for any project within the Specific Plan area to provide appropriate notification to the FAA via FAA Form 7460-1.</p> <p>C.1b: The City of San Bruno shall prohibit the following uses within the Specific Plan area:</p> <ul style="list-style-type: none"> • Any use that would direct a steady or flashing light of white, red, green, or amber color toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing, other than FAA-approved navigational lights. • Any use that would cause sunlight to be reflected toward an aircraft engaged in an initial straight climb following takeoff or engaged in straight final approach toward a landing. • Any use that would generate smoke or rising columns of air. • Any use that would attract large concentrations of birds within approach-climbout areas. • Any use that would generate electrical interference that may interfere with aircraft communications or aircraft instrumentation. <p>C.1c: The City shall require all sponsors of new dwelling units and/or buildings for human occupation to record a notice of fair disclosure, regarding the proximity of the proposed development to San Francisco International Airport and of the potential impacts of aircraft operation, per the recommendations contained in the Final Report of the 1998 San Mateo County Civil Grand Jury, as implemented by the City of San Bruno.</p>	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor to provide notification to the FAA via FAA Form 7460-1. 3. Project sponsor to submit disclosure notice to City for approval. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. City to review FAA response to Form 7460-1. 3. City review disclosure notice. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 3. City of San Bruno. 	<ol style="list-style-type: none"> 1. Prior to project approval 2. Prior to issuance of building permit. 3. Prior to issuance of building permit. 	

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 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
Cultural References					
<p>Mitigation Measure 5.b (N.1b): If cultural resources or human remains, prehistoric or historic-period archaeological resources are encountered during construction of a project, all construction activities within 100 feet will halt and the resources and their context shall not be further disturbed until a qualified cultural resource consultant has evaluated the situation. The City of San Bruno shall assure that identified cultural resources are recorded on proper historical properties forms.</p> <p>A Secretary of the Interior-qualified archaeologist will inspect the findings within 24 hours of discovery. If it is determined that the project could damage a historical resource or a unique archaeological resource (as defined pursuant to the CEQA Guidelines), mitigation will be implemented in accordance with PRC Section 21083.2 and Section 15126.4 of the CEQA Guidelines, with a preference for preservation in place. Consistent with Section 15126.4(b)(3), this may be accomplished through planning construction to avoid the resource; incorporating the resource within open space; capping and covering the resource; or deeding the site into a permanent conservation easement. If avoidance is not feasible, a qualified archaeologist will prepare and implement a detailed treatment plan in consultation with the City of San Bruno. Treatment of unique archaeological resources shall follow the applicable requirements of PRC Section 21083.2. Treatment for most resources would consist of (but would not be not limited to) sample excavation, artifact collection, site documentation, and historical research, with the aim to target the recovery of important scientific data contained in the portion(s) of the significant resource to be impacted by the project. The treatment plan will include provisions for analysis of data in a regional context, reporting of results within a timely manner, curation of artifacts and data at an approved facility, and dissemination of reports to local and state repositories, libraries, and interested professionals.</p>	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. If cultural/archeological resources are encountered, a Secretary of the Interior-qualified archaeologist shall inspect the findings within 24 hours of discovery and report to City. 3. If cultural/archeological resources are encountered, Archaeologist shall conduct independent review and prepare treatment plan, if necessary. 4. If cultural/archeological resources are encountered, Project sponsor or its contractor(s) shall implement treatment plan. 5. If cultural/archeological resources are encountered, City of San Bruno shall assure that identified cultural resources are recorded on proper historical properties forms. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. If resources are encountered, verify work is suspended. 3. If resources are encountered, review and approve treatment plan. 4. If resources are discovered, implement treatment plan. 5. Review historic properties forms. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. Project Sponsor and Archaeologist; City of San Bruno 3. Archaeologist; City of San Bruno. 4. Project sponsor and archaeologist. 5. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to project approval 2. Upon cultural resources discovery. 3. Upon cultural resources discovery. 4. Upon cultural resources discovery. 5. Prior to issuance of Certificate of Occupancy. 	
Hydrology and Water Quality					
<p>Mitigation Measure 9a(G.3a): The City shall require all contractors to develop and implement a SWPPP, as required by the State Water Resources Control Board NPDES General Construction Permit, for areas to be disturbed by construction activities. At a minimum, the SWPPP shall include the following:</p> <ul style="list-style-type: none"> • A construction schedule that restricts excavation and grading activities to the dry season (generally April 15 to October 15) to reduce erosion associated intense rainfall and surface runoff. 	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor and its contractor(s) shall prepare a SWPPP that adheres to all specifications of this measure. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. Review and approve SWPPP. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno; RWQCB 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. Prior to issuance of construction and grading permit(s). 	

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 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>The construction schedule shall indicate a timeline for earthmoving activities, hydroseeding, and stabilization of soils;</p> <ul style="list-style-type: none"> • Soil stabilization techniques such as hydroseeding and short-term biodegradable erosion control blankets; • Silt fences, hay bales, or some kind of inlet protection at downstream storm drain inlets; and • The post-construction inspection of all drainage facilities and clearing of drainage structures of debris and sediment. 	3. Inspect construction site for adherence to SWPPP.	3. Include any findings in monitoring report	3. City of San Bruno; RWQCB	3. One inspection to occur during each phase of construction.	
<p>Mitigation Measure 9c</p> <p>G.2a: The City of San Bruno shall require, for incorporation into all redevelopment designs, permanent stormwater controls such as vegetated swales, retention ponds, landscape areas, etc., in accordance with MS4 NPDES and San Mateo Countywide Water Pollution Prevention Program (SMCWPPP) requirements, for the stormwater collected from new parking lots and other impervious surfaces.</p> <p>G.2b: To help minimize the amount of runoff containing urban pollutants, streets, and parking areas in the redevelopment subareas shall be frequently cleaned by the City of San Bruno using street-sweeping equipment, and the collected material properly disposed.</p>	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor and its contractor(s) shall submit incorporate all specifications of this measure in the construction plans. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. Review project construction plans; verify inclusion of SMCWPPP requirements. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. Prior to project approval. 	
Noise					
<p>Mitigation Measure 12.a</p> <p>I.3.b: All development under the proposed Specific Plan shall be constructed to comply with the relevant noise insulation standards contained in Title 24 of the California Code of Regulations (Part 2, Appendix Chapter 12A).</p> <p>I.1: The project applicant will obtain a permit to construct from the Director of Public Works prior to the start of construction activities, since construction would exceed the specified noise levels in the City Municipal Code. Noise-generating construction activities would be limited to reasonable daytime hours, such as between the hours of 7:00 a.m. and 7:00 p.m. on weekdays. No construction activities will be allowed on weekends or national holidays. Information concerning construction-related activities and construction hours should be distributed throughout the affected areas and incorporated as part of the Specific Plan, heavy construction activities would be prohibited on Saturdays and Sundays.</p>	<ol style="list-style-type: none"> 1. Project sponsor shall submit building plans documenting compliance with Title 24 noise standards. 2. Project sponsor shall obtain construction permit. 	<ol style="list-style-type: none"> 1. Review plans for compliance. 2. Review plans for compliance. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to Issuance of building permit. 2. Prior to issuance of building or grading permit(s) 	
Public Services					
<p>Mitigation Measure 14.a.i (F.1): The City of San Bruno shall install signals that can be pre-empted by fire protection or emergency</p>	1. City of San Bruno incorporates measure as a condition of approval.	1. City adopts condition of	1. City of San Bruno	1. Prior to project approval.	

OTO Development - The Crossing Springhill Suites Hotel
 Conditions of Approval and Mitigation Monitoring and Reporting Plan

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>medical response vehicles. Developers shall contribute a “fair share” portion of the costs of these pre-emptive signals as determined by the City of San Bruno.</p>	<p>2. Project sponsor to pay fair share fee for traffic signal installation.</p>	<p>approval with project. 2. Verification of fee payment</p>	<p>2. City of San Bruno</p>	<p>2. Prior to issuance of Certificate of Occupancy.</p>	
<p>Mitigation Measure 14.a.iii (F.4): The City of San Bruno shall coordinate payment to the appropriate school districts of the school impact fee required by California Senate Bill 50.</p>	<p>1. City of San Bruno to provide assistance as needed to project sponsor to coordinate payment of required school impact fee.</p>	<p>1. Verification of fee payment</p>	<p>1. City of San Bruno</p>	<p>1. Prior to issuance of Certificate of Occupancy.</p>	
Utilities and Service Systems					
<p>Mitigation Measure 17.c E.6: Prior to the issuance of building permits, the City of San Bruno shall require that all designs for residential and commercial development within the Specific Plan area include adequate storage space for projected recyclable and compostable materials. The City shall require adequate storage space on each floor of each building and in its enclosed garbage areas, as well as adequate loading space, to accommodate the City of San Bruno’s recycling program. E.7: As a condition of project approval and before demolition and construction, the City of San Bruno shall require the demolition and construction contractors to maximize diversion of materials remaining from the demolition of structures and the byproducts of construction. The City shall require that project sponsors work with the City of San Bruno’s Public Works Department and submit a recovery plan for maximizing diversion of construction and demolition materials associated with construction of any project in the Specific Plan area, so that at least 50 percent of the demolition debris is, if feasible, recycled or can be used as alternative landfill cover.</p>	<p>1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor shall submit building plans documenting compliance. 3. Project sponsor and construction contractor(s) to submit construction/ demolition waste diversion plan to City for approval.</p>	<p>1. City adopts condition of approval with project. 2. Review plans for compliance. 3. Review diversion plan.</p>	<p>1. City of San Bruno 2. City of San Bruno 3. City of San Bruno.</p>	<p>1. Prior to project approval. 2. Prior to issuance of building or grading permit. 3. Prior to issuance of building or grading permit.</p>	



Mary Lou Johnson, *Chair*
Kevin Chase, *Vice Chair*
Rick Biasotti
Marie Kayal
Sujendra Mishra
Perry Petersen
Joe Sammut

MINUTES
ARCHITECTURAL REVIEW COMMITTEE
December 10, 2015
6:00 p.m.

Meeting location: City Hall, Conference Room 101, 567 El Camino Real, San Bruno

The Architectural Review Committee met Thursday, November 12, 2015 at 6:00 P.M. at City Hall, Conference Room 101. Staff present: Paula Bradley (PB), Contract Associate Planner; Matt Neuebaumer (MN), Associate Planner; Michael Smith (MES), Senior Planner, and David Woltering (DW), Community Development Director

ROLL CALL –Chase, Kayal, Sammut (KC, MK, JS)

A. 460 Hazel Avenue (APN: 020-271-250)

Zoning: R-1 (Single-Family Residential)

Recommended Environmental Determination: Categorical Exemption

Request for a Use Permit to allow the construction of a 111 sq. ft., two-story addition at the north side of the building, resulting in a gross floor area of 2,935 sq. ft. where 2,750 sq. ft. is allowed and providing only one (1) parking space for the expansion of a residence that is greater than 1,825 sq. ft. per Sections 12.200.030.B.2. and 12.200.080.A.2. of the San Bruno Municipal Code. Ellen Hartog (Applicant) Kevin and Silvia Martinez (Owners) UP-15-025.

MES presented the staff report. Staff's recommendations were incorporated into the plans by the applicant; therefore, the staff recommendation did not include any conditions of approval.

KC asked where the deck is located and how far it is set back from the side property line?

MES responded by indicating the location of the deck on the site plan and clarifying that the adjacent property does not have a 10' side setback; therefore, the deck is not subject to use permit authorization.

MK asked if the current owners owned the property when the interior stairs were removed.

Kevin Martinez, the property owner, responded that his family owned the property when the stairs were removed to accommodate a kitchen remodel.

JS commented that the ARC wants to ensure that additions are compatible in terms of style, form, and materials and that the proposed addition fits that criteria.

JS Motioned to approve UP15-021, based on Findings of Fact 1-6, with no recommendations.

Motion Passed 3-0.

EXHIBIT D

B. 21 Bayshore Circle (APN: 014-283-070)

Zoning: None (right-of-way), R-1 is the nearest Zoning District

Recommended Environmental Determination: Categorical Exemption

Request for a Conditional Use Permit to install a wireless telecommunications facility within the right-of-way on an existing utility pole, per Sections 12.220 and 12.84.160 of the San Bruno Municipal Code. The facility would consist of one (1) antenna located in a radome mounted to the existing utility pole and two (2) associated equipment cabinets containing three (3) Remote Radio Units (RRUs) located on a concrete pad on the ground on the west side of the pedestrian pathway. Verizon Wireless (Applicant) PG&E (Owners) UP-15-010.

MES presented the staff report. Staff's recommendations were incorporated into the plans by the applicant; therefore, the staff recommendation did not include any conditions of approval.

KC commented that the radome around the antenna makes it appear much larger and more visible.

MK agreed with KC's comment and added that the radome makes the antennas "pop" when they would otherwise be discrete.

JS agreed with both KC and MK and added that maybe the cabinets should be painted brown to match the pole and asked whether the cabinets would be accessible to the public.

The project sponsor, represented by Ashley Woods of Core Development Services, responded that the cabinets would be accessible to the public and that an enclosure could be added around the units.

MES pointed out the location of the cabinets at ground level near the pedestrian pathway and asked the ARC members whether they still wanted the cabinets to be painted brown to match the pole given their location within the landscape.

JS confirmed the location near the bushes and concluded that the cabinets did not need to be painted brown.

JS Motioned to approve with the recommendation to remove the radome stealthing from the antennas and add an enclosure around the cabinets at grade level.

Motion Passed 3-0.

C. The Crossing Hotel Project (APN: 020-013-250 and 020-013-260)

Zoning: P-D (Planned Development)

Recommended Environmental Determination: A draft Environmental Impact Report (EIR) addendum to the Navy Site Specific Plan Supplemental EIR previously adopted for a hotel at this location.

Request for an Architectural Review Permit to allow the construction of five-story, 74 +/- foot high, 152-room hotel, with 3,000 square feet of meeting/banquet space and below grade parking pursuant to Chapter 12.96.190 of the San Bruno Municipal Code. OTO Development, LLC (Applicant) AR15-002.

(A companion request for a Planned Development Permit, Conditional Use Permit, and Lot Line Adjustment will be analyzed as the project moves forward to the Planning Commission and City Council).

(This project was continued from the November 12, 2015 meeting)

The applicant was represented by Jeffrey MacAdam (JM), and Jim Heilbronner (JH), Architects from Architectural Dimensions, and Steve Pieters (SP), of OTO Development, LLC.

PB presented the staff report memo, including staff recommendations.

JM presented the project and revised plans on boards.

KC asked how many feet in height were lost to satisfy the airport requirements?

JM responded none to the floors of the hotel, only a few feet off the northeast tower elevation. As depicted in the revised drawings. A height study was added to the plans (Sheet D-9). An entirely new northeast elevation was presented with a curtain wall for the windows on floors two to five, a more simplified desirable design. The front entry canopy in front is curved. A new west elevation with a green wall at the courtyard and a medallions at the intersections of the stucco wall panels, with widened lines in between to add architectural interest. There are no additional recesses or roof overhangs as recommended at the November 12th ARC meeting.

JM presented three different light standard options.

JM stated that landscape shrubs and trees were added to the east elevation to screen the vehicles in the parking lot and renderings were presented with three phases of growth as the plants mature over time.

No public comments were submitted or made.

KC motioned to recommend approval with the following recommendations:

- Light standard fixture "Premier II" (Option 3).
- The landscape treatment as proposed on the rendering of the east elevation.
- On the west elevation: for the Planning Commission meeting, propose a textured surface, possibly multiple textured surfaces, with a wave design, and possibly extend it above the roof line. Bring a rendering is to be presented at the Planning Commission meeting.
- Roof overhangs not needed on the west elevation.
- Porte cochere may have additional columns if structurally necessary.
- For the Planning Commission meeting, provide renderings with before and after elevations so that the full Commission has an understanding of the changes.
- Recommendations from the November 12, 2015 meeting were for the most part, incorporated into the plans:
- Present a rendering with proposed landscaping on the east elevation at the Planning Commission meeting.

Motion Passed 3-0

The meeting adjourned at 9:30 p.m.



David Woltering, Secretary to the Planning Commission
City of San Bruno

The Crossing Hotel Project

CEQA Analysis

Pursuant to California Resources Code Sections 21083.3, and 21166
and CEQA Guidelines Sections 15162, 15164, and 15183.

Date: December 8, 2015
Project Address: Admiral Court, San Bruno, California
Zoning: Planned Development (P-D)
General Plan: Visitor Services
APNs: 020-013-250, 020-013-260 and a portion of 020-013-220
Lot Size: 1.5 acres
Applicant: OTO Development
100 Dunbar Street, Suite 402
Spartanburg, South Carolina 29306

Staff Contact: Mark Sullivan
msullivan@sanbruno.ca.gov
650-616-7053

Background

In January 2001, the City of San Bruno (City) certified the *U.S. Navy Site and Its Environs Specific Plan EIR* (Specific Plan EIR). The Specific Plan was subsequently amended in 2002 and 2005 to increase residential densities on the site and to provide for the El Camino Real (ECR) Commercial Overlay. Addendums to the 2001 EIR were prepared in both cases. In September, 2015, the City certified the *U.S. Navy Site and Its Environs Specific Plan Amendment (the Crossing Hotel Site) Supplemental Environmental Impact Report* (SEIR) and amended the Specific Plan to reduce the scale of the hotel.

The Specific Plan Amendment analyzed in the 2015 SEIR modified the hotel approved in the original Specific Plan from a 500-room hotel of up to seven stories (90'-0" maximum height) on a 5.5-acre site to a 152-room, approximately 67-foot, five-story hotel on a 1.5-acre site.¹ In addition, the amendment revised the hotel from a full-service hotel to a select-service hotel providing food service facilities rather than a restaurant and a conference space with theater-style seating that would accommodate approximately 300 people, banquet-style seating for sit-down events to accommodate approximately 200 people, and ballroom-style arrangements for wedding events to accommodate up to 150 people.

¹ Although the Specific Plan Amendment and the 2015 SEIR assumed that the hotel would be approximately 73 feet tall, the Specific Plan Amendment stated that the hotel's maximum height is also governed by the Airport Land Use Compatibility Plan for San Francisco International Airport and FAA regulations related to protection of airspace in the vicinity of SFO. The developer will be required to notify the FAA of a proposed development project by filing Form 7460-1. The FAA will determine whether the project would constitute an obstruction or hazard to safe air navigation." Analysis completed by the FAA and the developer since certification of the SEIR has resulted in the hotel height being reduced to 67 feet to comply with FAA regulations.

The proposed project is the construction and operation of the Crossing Hotel as described in the SEIR.

Purpose and Summary of this Document

The purpose of this document is to demonstrate CEQA compliance of the proposed project. An EIR addendum is considered suitable for the currently proposed project, as demonstrated by the analysis presented herein. For comprehensive review and public information, the project would also qualify for a CEQA exemption based on consistency with an adopted community plan, as explained below, which separately and independently provides a basis for CEQA compliances.

1. **Addendum.** Public Resources Code Section 21166 and CEQA Guidelines Sections 15162 and 15164 (Subsequent EIRs, Supplements and Addenda to an EIR or Negative Declaration), state that an addendum to a certified EIR is allowed when minor changes or additions are necessary, and none of the conditions for preparation of a subsequent EIR or Negative Declaration per Sections 15162 and 15164 are satisfied.
2. **Community Plan Exemption.** Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183 (Projects Consistent with a Community Plan or Zoning) allow streamlined environmental review for projects that are “consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site.” Section 15183(c) specifies that “if an impact is not peculiar to the parcel or to the proposed project, has been addressed as a significant effect in the prior EIR, or can be substantially mitigated by the imposition of uniformly applied development policies or standards..., then an EIR need not be prepared for the project solely on the basis of that impact.”

Previous Mitigation Measures

The analysis in this document evaluates the potential project-specific environmental effects of the proposed project, and evaluates whether such impacts were adequately covered by the 2001 EIR and the 2015 SEIR to allow the above-listed provisions of CEQA to apply. The analysis conducted incorporates by reference the information contained in each of the previous EIRs. The proposed project is legally required to incorporate and/or comply with the applicable requirements of the mitigation measures identified in the 2015 SEIR, including those carried over from the 2001 EIR, some of which were modified in the SEIR.

Project CEQA Compliance

The proposed project satisfies each of the CEQA provisions, as summarized below.

- **Addendum.** The analysis conducted in this document indicates that an addendum to the 2001 SEIR applies; therefore, this CEQA Analysis is considered to be the addendum. As discussed under Project Characteristics below, the Crossing Hotel Project is identical to the hotel assumed in the SEIR analysis; there are no substantial changes to the project. There

have been no substantial changes in the circumstances under which the project will be undertaken. In addition, there is no new information that would result in new or substantially more severe impacts of the hotel project, or that new mitigation measures or alternatives are warranted. The Crossing Hotel Project therefore meets the requirements for an addendum.

- **Community Plan Exemption.** Based on the analysis conducted in this document, the Crossing Hotel Project also qualifies for a community plan exemption. It is permitted in the zoning district where the project site is located, and is consistent with the bulk, density, and land uses envisioned for the site. The analysis herein considers the analysis in the 2001 EIR and 2015 SEIR for the evaluation of the Crossing Hotel Project. This CEQA analysis concludes that the proposed project would not result in significant impacts that (1) are peculiar to the project or project site; (2) were not identified as significant project-level, cumulative, or offsite effects in the 2001 EIR or 2015 SEIR; or (3) were previously identified as significant effects, but are determined to have a more severe adverse impact than discussed in the EIRs.

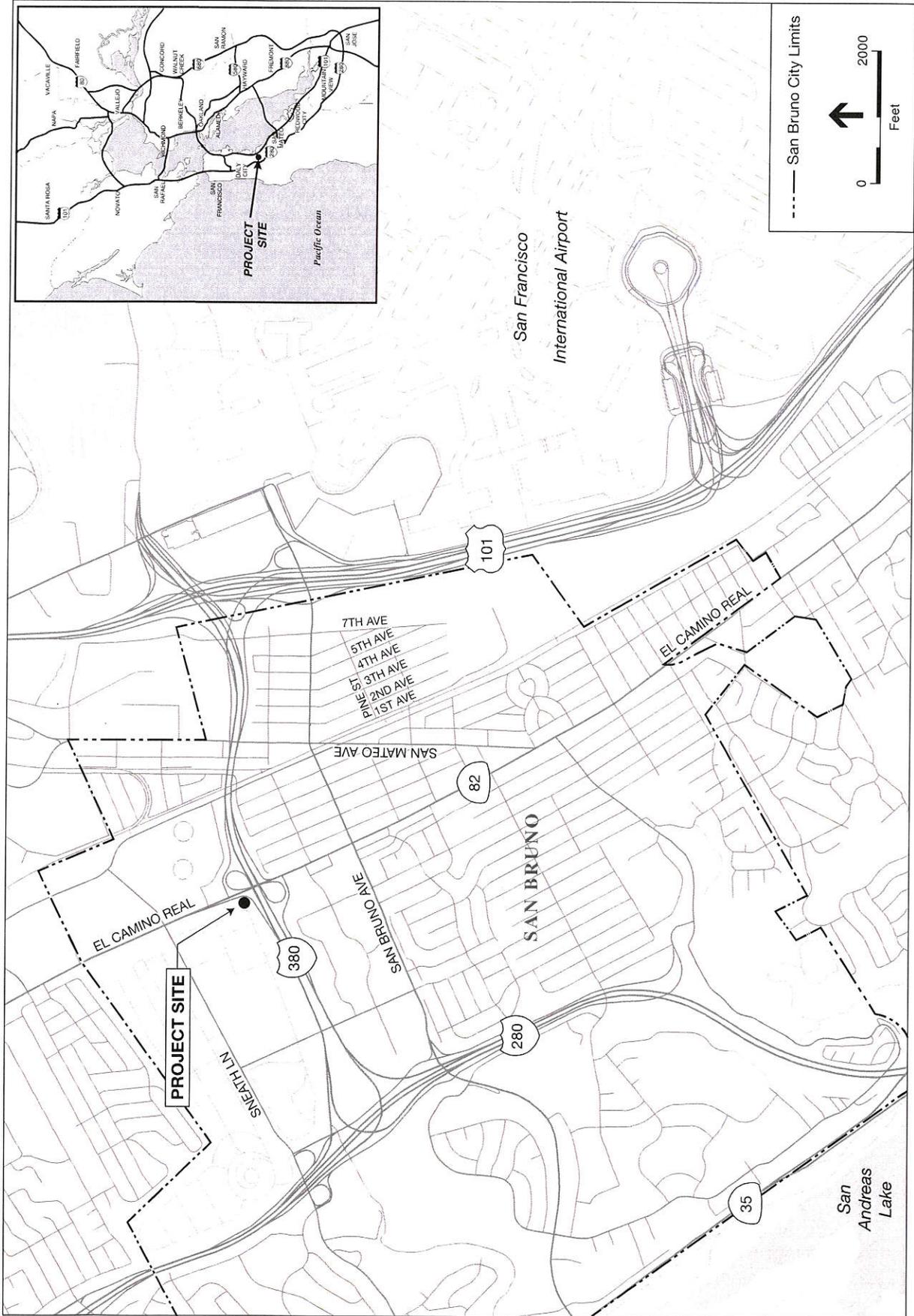
Overall, based on an examination of the analysis, findings, and conclusions of the 2015 SEIR, as well as those of the 2001 EIR— all of which are as summarized in the CEQA Analysis in Section V of this document – the potential environmental impacts associated with the Crossing Hotel Project have been adequately analyzed and covered in prior Program EIRs. Therefore, no further review or analysis under CEQA is required.

Project Description

Project Location and Existing Site Conditions

The project site is located in the northeastern portion of the City of San Bruno, in the vicinity of San Francisco International Airport, Interstate 380 (I-380), and U.S. Highway 101 (**Figure 1**). The parcel is bounded by El Camino Real/State Route 82 (SR 82) to the east, I-380 to the south, and The Crossing San Bruno, a multifamily housing development to the north and east. The site is also within walking distance of two commuter-rail transit stations (BART and Caltrain), The Shops at Tanforan, and the Bayhill office park (**Figure 2**). The 1.5-acre parcel is currently a vacant lot and enclosed by a chain-link slatted fence. It is adjacent to Jack's restaurant and buildings representing The Crossing development.

The project site resides within the U.S. Navy Site Specific Plan planning area; a 56-acre area that includes a 20-acre decommissioned U.S. Navy Facility (**Figure 3**). The hotel site represents the last remaining vacant parcel within The Crossing. The Crossing includes 1,063 multifamily units in five buildings (including 325 affordable units and 228 senior units) and a 12,500-square-foot fully occupied commercial center, all built within the past ten years. The San Francisco International Airport is three miles southeast of the project site. Other notable land uses in the vicinity include the Golden Gate National Cemetery, one-half mile to the north of the site and a one million-square-foot regional mall (the Shops at Tanforan) approximately 500 feet to the east.

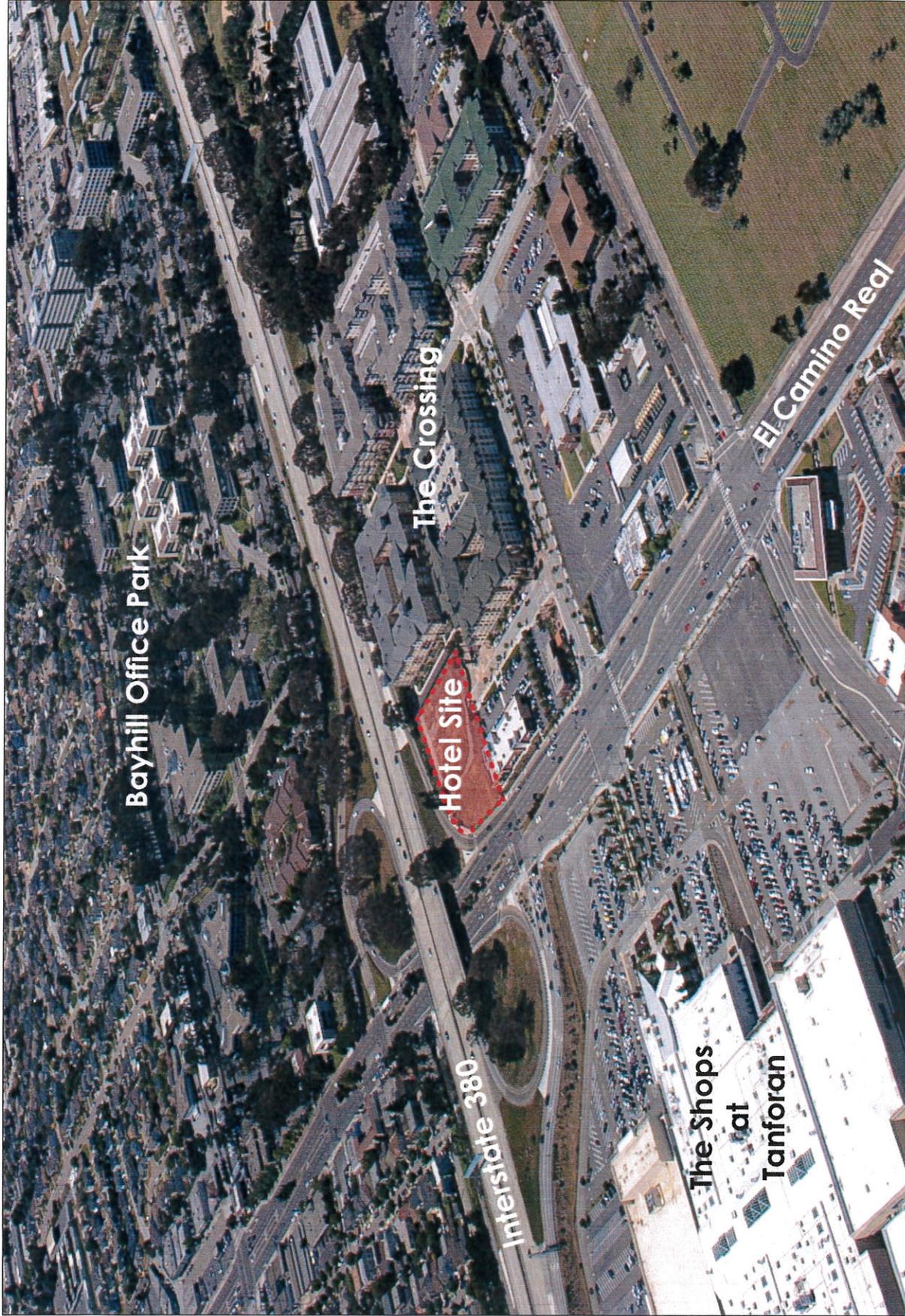


San Bruno City Limits



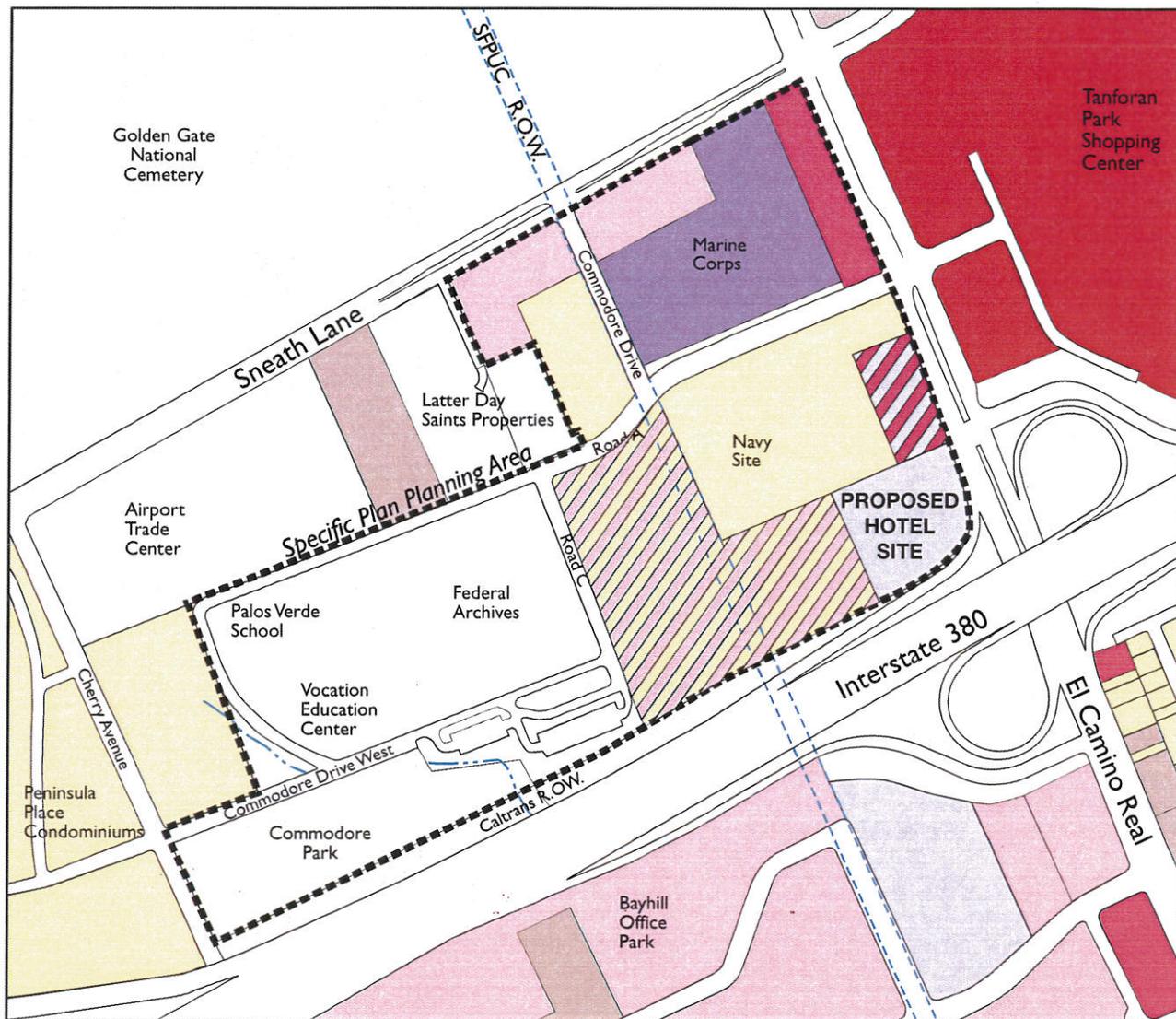
SOURCE: ESA

The Crossing Hotel Site . 130117
Figure 1
 Project Location Map



The Crossing Hotel Site . 130117
Figure 2
Hotel Site Overview

SOURCE: ESA



- | | | | |
|---|--|---|---------------------|
|  | Residential |  | Visitor Services |
|  | Neighborhood Retail |  | Services |
|  | Regional Retail |  | Industrial |
|  | Regional Office |  | Parks/Open Space |
|  | Office/Residential Flex |  | Public/Quasi-Public |
|  | ECR Commercial Overlay | | |
|  | Marines Reserve Facility
(Future Mixed Use) | | |

SOURCE: Dyett & Bhatia

The Crossing Hotel Site . 130117
Figure 3
 U.S. Navy Site Specific Plan
 Land Use Diagram

Project Characteristics

The proposed project would construct a 152-room hotel on 1.5 acres. The select service hotel would provide limited food service and include a conference space with theatre-style seating to accommodate approximately 300 people, banquet-style seating for sit-down events to accommodate approximately 200 people, or ballroom-style arrangements for wedding events to accommodate up to 150 people, depending on configuration for a particular event (**Figure 4**).

The maximum height at The Crossing Hotel site would be approximately 67 feet (five stories). Building elevations along are shown on **Figure 5** and **6**. The maximum building height would not penetrate critical aeronautical surfaces, as determined by the Federal Aviation Administration (FAA) in an aeronautical study prepared for a specific project and the Comprehensive Airport Land Use Compatibility Plan for the San Francisco International Airport with the approval of the San Mateo County Airport Land Use Commission.

The exterior architecture would have a contemporary design with a distinctive illuminated SpringHill Suites Marriott logo sign distinguishing the front entrance (on the site's northern side) and identical illuminated signs mounted on the northwest, south, and northeast sides. The building would be finished with stucco and decorative plastering and simulated brick/stone at lower level (**Figure 7**).

In accordance with the recently amended Specific Plan, the operator of the Crossing Hotel Project would be required to reduce long-term operational greenhouse gas emissions to the extent feasible through the implementation of the following sustainability requirements:

- Install a minimum of 6,000 square feet of solar photovoltaic panels on the rooftop.
- Improve the building envelope energy efficiency 15% over 2013 Title 24 standards;
- Incorporate high efficiency lighting (such as LEDs, metal halide post top, metal halide cobrahead or cutoff lights, or high pressure sodium cutoff lights) into public areas, such as parking lots, garages, and other exterior areas to achieve at least 15% lighting energy reduction compared to the use of mercury cobrahead lights;
- Install energy efficient appliances that comply with the most recent U.S. EPA Energy Star criteria, including refrigerators, dish washers, fans, and clothes washers;
- Incorporate water conservation strategies, including the installation of low flow faucets, toilets, and showers, as well as water efficient irrigation-systems;
- Institute recycling and composting services in order to achieve at least a 10% reduction in waste disposed.

PROJECT SUMMARY

SPRINGHILL SUITES, 5 STORY.
 150,000 S.F. (150,000 S.F.)
 SITE AREA (Parcel 1 & 2) - 12,865 S.F. (1,518 ACRES)
 Parcel of Parcel 2 - 78,689 S.F. (1,815 ACRES)
 TOTAL - 91,554 S.F.
 BUILDING AREA:
 Ground Level: 15,386 S.F.
 1st Floor: 20,740 S.F.
 2nd Floor: 20,832 S.F.
 3rd Floor: 20,832 S.F.
 4th Floor: 20,832 S.F.
 5th Floor: 20,832 S.F.
 Subtotal: 99,822 S.F.
 F.A.R. = 1.56 (Underground Garage is not including)

Underground Garage Area: 25,385 S.F.
 Level 1 Parking: 25,385 S.F.
 Subtotal: 50,770 S.F.
 Total Building Area: 149,752 S.F.

ROOM SUMMARY

Floor	King	O.G.	Total
1st	-	-	0
2nd	22	16	38
3rd	22	16	38
4th	27	11	38
5th	27	11	38
Total	103	49	152

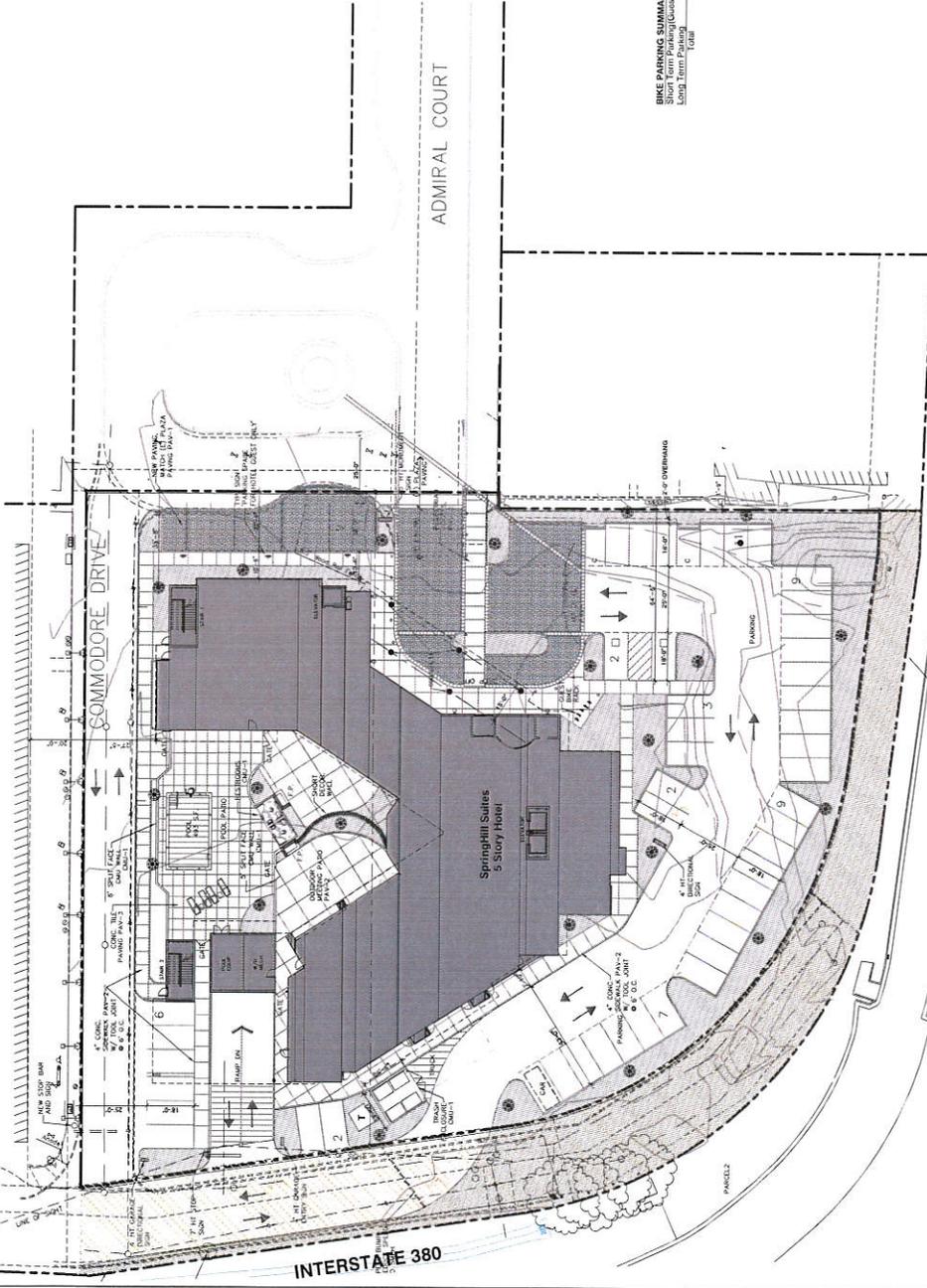
PARKING SUMMARY
 Required 1 Space per Room + Mgr = 153 SPACES
 Meeting Room - not req'd = 15 SPACES
 Provision: 157 SPACES
 Garage Standard: -147 SPACES
 SUBTOTAL: -20 SPACES
 Garage Tandem: -20 SPACES
 TOTAL: 167 SPACES

Total Parking Provided: 167 SPACES
 (including 116 Accommodate Spaces)
 Valid Parking: 17 SPACES
 Garage Level P1: 14 SPACES
 Garage Level P2: 14 SPACES
 TOTAL: 45 SPACES

SITE LEGEND

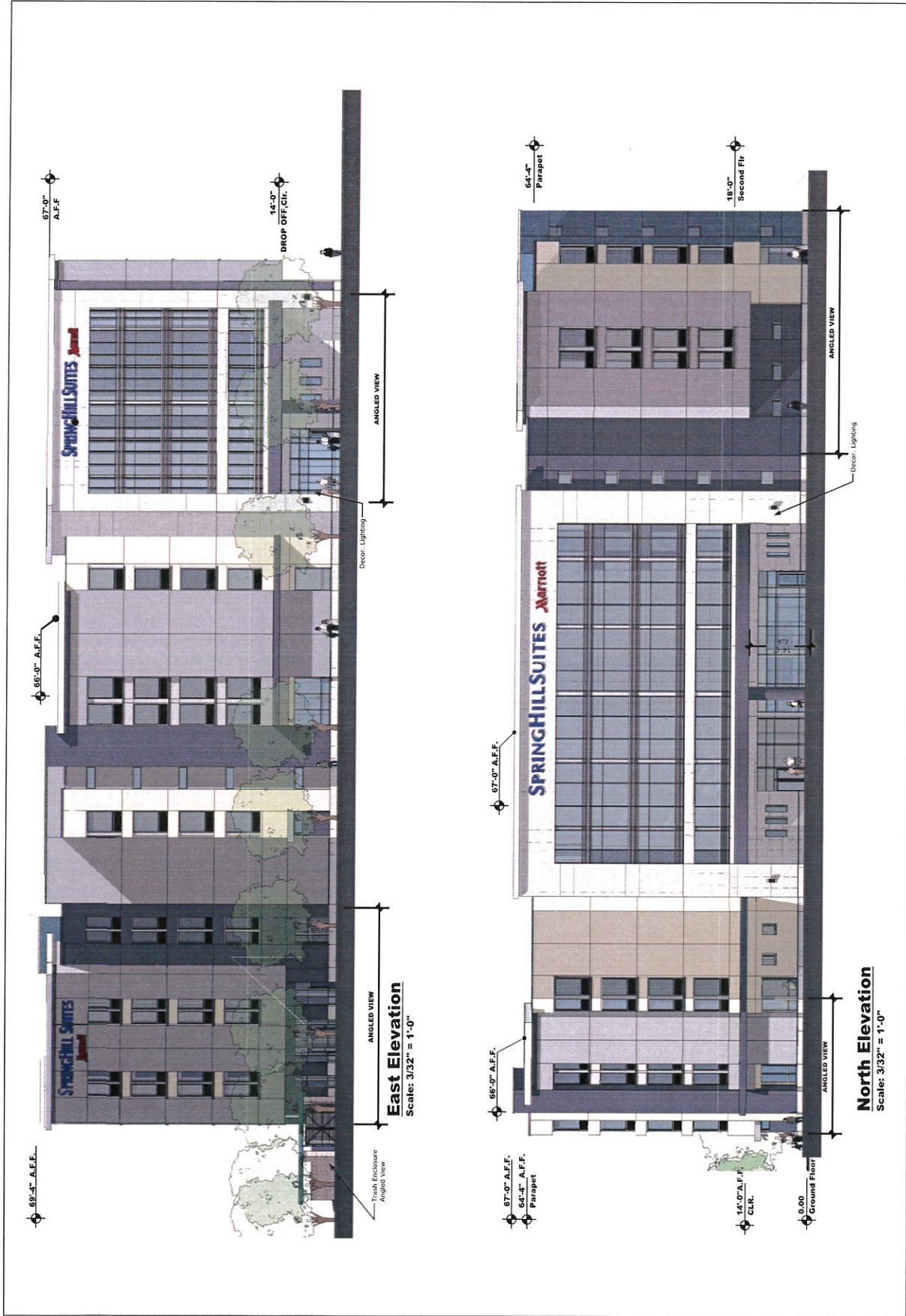
- PAVING AND FENCE MATERIALS SEE SHEET DR#8
- NEW PAVING MATCH (E) PAVING PAV-1
 - NEW CONCRETE PAVING PAV-2
 - NEW CONCRETE PAVING PAV-3
 - NEW LANDSCAPED AREA
 - ADJACENT SITE PORTION OF PARCEL 2

BIKE PARKING SUMMARY:
 Long Term Parking: 5 Stalls
 Guest Parking: 5 Stalls
 Total: 10 Stalls



SOURCE: Oto Development

The Crossing Hotel Site - 130117
Figure 4
 Site Plan



The Crossing Hotel Site . 130117
Figure 5
 Elevations: East and North

SOURCE: Oto Development



View from El Camino Real
Scale: NTS



View from Admiral Ct.
Scale: NTS



View from Fwy 380 Ramp
Scale: NTS



Porte Cochere
Scale: NTS

Parking and Circulation

The proposed hotel development would meet the requirements for hotel guest parking onsite rather than in a shared parking garage with the adjacent office complex. However, shared or offsite parking could be negotiated to meet the needs of events in the hotel meeting space. The hotel would also enter into a shared parking agreement with the restaurant located in the adjacent commercial center.

The proposed project would require one parking space per room with a potential 15 percent reduction for transit-oriented development and shared parking, plus one space for each 100 square feet of meeting space, and envisions a parking structure integrated into the main building. Two natural gas-fueled shuttles would be required as part of a transportation demand management (TDM) program to transport hotel patrons to local businesses and to pick up and drop them off at the San Francisco International Airport, the San Bruno Bay Area Rapid Transit (BART) station, and Caltrain Station.

Project Schedule and Construction

Construction for an approximately 67-foot (5-story) hotel and a parking garage with a depth of 24 feet would require approximately 19,900 cubic yards of soil to be exported from the site in 10-cubic-yard trucks. This would require approximately 1,990 truckloads, or 3,980 truck round trips.

The proposed project would be constructed over an 18-month period. Construction-related activities could include disruptions to the circulation system in and around the project site and surroundings, which may include temporary lane closures and sidewalk closures along adjacent streets. Heavy vehicles (i.e., haul [tractor-trailer] trucks, machinery) would access the project site and surroundings, and equipment and materials would need to be staged for construction. Construction equipment to be used include drill rigs, haul trucks, bulldozers, backhoe, concrete truck, concrete pumper, crane, compressors and employee trucks and automobiles.

Vehicular trips would be generated by an estimated 15 to 20 workers with a maximum of 30 to 40 during framing. Parking for construction workers would be located onsite; there would be no staging of vehicles or equipment on or along existing roadways.

The work day for construction workers would typically commence at 7:00 a.m. and would end at or prior to 7:00 p.m.; therefore, construction employee-generated trips would not have a significant effect on the traffic operations on the roadway during typical peak commute hours (7:00 a.m. to 9:00 a.m., and 4:00 p.m. to 6:00 p.m.). There would likely be multiple destinations for off-haul materials and origins for on-haul materials. Construction workers would also be arriving from different directions. Travel routes for workers, spoils export, and material import would be determined in consultation with the City and scheduled to avoid peak traffic periods.

Project Operation

The project would include secure enclosed structures to house recycling and trash containers. The project site would be regularly monitored by hotel landscape/maintenance staff to ensure that trash would not collect outside the refuse structures. During construction and operation, trash and

other waste would be regularly collected and properly disposed or recycled by a certified waste management company. During hotel operations, hotel management would contract with a local waste management company to provide collection services.

Water supply from the City of San Bruno is available to the project using existing waterlines. The project would connect to an existing sewer main on site. Stormwater would flow through these filtration systems before it is channeled to the site drainage system.

The hotel would employ approximately 30 workers, 40 percent of which would be full time.

Approvals Required

The project site is located within the City of San Bruno's land jurisdiction; the City of San Bruno (City) is the Lead Agency responsible for review of the proposed project under the California Environmental Quality Act (CEQA). This EIR Addendum has been prepared in accordance with CEQA and the State CEQA Guidelines.

The project would be required to comply with the City of San Bruno's General Plan. Project approvals that may be required for development of the hotel include: Planned Development Permit, Architectural Review Permit, Conditional Use Permit, and Development Agreement.

Additionally, the development of a hotel project may require approvals from the following:

- City/County Association of Governments (C/CAG)
- C/CAG Airport Land Use Committee (ALUC)
- Federal Aviation Administration (FAA)
- San Mateo County Department of Environmental Health
- Regional Water Quality Control Board (RWQCB)

Summary of Findings

An evaluation of the proposed project is provided below. This evaluation concludes that the proposed project qualifies for an addendum as well as an exemption from additional environmental review. It is consistent with the development density and land use characteristics established by the San Bruno General Plan, and any potential environmental impacts associated with its development were adequately analyzed and covered by the analysis in the 2001 EIR and the 2015 SEIR.

The proposed project would be required to comply with the applicable mitigation measures identified in the 2015 SEIR. With implementation of the applicable mitigation measures, the proposed project would not result in a substantial increase in the severity of previously identified significant impacts in the 2015 SEIR.

In accordance with California Public Resources Code Sections 21083.3, and 21166; and CEQA Guidelines Sections 15162, 15164, 15183, and as set forth below, the proposed project qualifies for an addendum and one or more exemptions because the following findings can be made:

- **Addendum.** The analyses conducted and the conclusions reached in the 2015 SEIR certified by the City Council on September 8, 2015, remain valid. The proposed project would not cause new significant impacts not previously identified in the previously certified Program EIRs, or result in a substantial increase in the severity of previously identified significant impacts. No new mitigation measures would be necessary to reduce significant impacts. No changes have occurred with respect to circumstances surrounding the Original Project that would cause significant environmental impacts to which the proposed project would contribute considerably, and no new information has been put forward that shows that the proposed project would cause significant environmental impacts. Therefore, no supplemental environmental review is required in accordance with Public Resources Code Section 21166, and CEQA Guidelines Sections 15162, 15164, as well as 15168 and 15180.
- **Community Plan Exemption.** The proposed project would not result in significant impacts that (1) are peculiar to the project or project site; (2) were not previously identified as significant project-level, cumulative, or offsite effects in the 2001 EIR or 2015 SEIR; or (3) were previously identified as significant effects, but which would increase in severity beyond that described in those EIRs. Therefore, the proposed project would meet the criteria to be exempt from further environmental review in accordance with Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183.

Each of the above findings provides a separate and independent basis for CEQA compliance.

CEQA Analysis

The following CEQA analysis provides a summary of the potential environmental impacts that may result from adoption and implementation of the Crossing Hotel Project, as evaluated in the 2015 SEIR (including the Initial Study) for the Specific Plan Amendment, and evaluates whether the currently proposed project would have any new or substantially more severe impacts than those disclosed in the SEIR. As described above, the proposed project consists of the construction and operation of the Crossing Hotel, which was analyzed at the project-level in the SEIR. The SEIR analyzed the impacts to Transportation, Air Quality, and Greenhouse Gas Emissions, while the Initial Study (located in Appendix C of the SEIR) analyzed the remaining checklist resource areas. Beneath each CEQA Checklist item below is a determination of whether the proposed hotel project would result in:

- Equal or Less Severity of Impact Previously Identified in the SEIR;
- Substantial Increase in Severity of Previously Identified Significant Impact in the SEIR; or
- New Significant Impact.

Mitigation Measures cited below have been incorporated into the Mitigation Monitoring and Reporting Program (MMRP), which is provided in Attachment A.

Transportation

The Specific Plan EIR evaluated the environmental impacts of the buildout pursuant to the Specific Plan and found that no significant transportation impacts would result at local intersections due to the implementation of the proposed Specific Plan. However, four intersections would continue to operate at unacceptable levels of service, and potential improvements were outlined to be considered to improve local traffic conditions. To reduce the impact of the redevelopment of the U.S. Navy Site, the Specific Plan EIR required **Mitigation Measure D.3** to coordinate shared parking across the land uses and **Mitigation Measure D.4**, which required a project specific traffic study for each “substantial” development in the Specific Plan area to ensure impacts from unique site characteristics are less than significant or could be mitigated to a less-than-significant level. As revised in 2015, the Specific Plan states, “The hotel development must meet the requirements for hotel guest parking and meeting space on site, although shared or offsite parking could be negotiated to meet the needs of events in the hotel meeting space. The hotel must also enter into a shared parking agreement with the restaurant located in the adjacent commercial center.” This fulfills the requirements of Mitigation Measure D.3 from the Specific Plan EIR. The SEIR completed the traffic analysis of the Specific Plan Amendment required to fulfill Mitigation Measure D.4 from the Specific Plan EIR.

The SEIR concluded that the Specific Plan Amendment would result in less-than-significant traffic impacts, which was consistent with the findings of the Specific Plan EIR, and would not result in any new significant transportation effects that were not identified in the Specific Plan EIR or a substantial increase in the severity of any previously identified significant transportation effects.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the SEIR, the trip generation would be the same and therefore, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to transportation.

Air Quality

The Specific Plan EIR concluded that the Specific Plan would result in potentially significant air quality impacts due to the construction and operation proposed under the Specific Plan. Construction-generated dust would result in a significant impact; however, with implementation of **Mitigation Measure H.1.a** (dust abatement) and **Mitigation Measure H.1.b** (control of asbestos in building demolition), this impact could be reduced to a less-than-significant level. The Specific Plan EIR also concluded that project operations would result in a significant and

unavoidable impact to regional air quality, specifically with respect to ozone and respirable particulate matter (PM₁₀, which comprises particles 10 microns and less in diameter) concentrations, pollutants for which the Bay Area was designated “nonattainment” under federal and/or State air quality standards. In addition, **Mitigation Measure H.2** addressed operational emissions by requiring that the City ensure the implementation, to the extent feasible, of Transportation Control Measures identified in the ‘97 *Clean Air Plan* in future development to reduce vehicular emissions.

The SEIR concluded that in addition to the mitigation measures required under the Specific Plan, a new **Mitigation Measure 4.A-3** would be required in order to ensure that diesel particulate matter (DPM) emissions would be reduced to the extent feasible and that potential construction-related health risk to the relatively close off-site residential uses would be less than significant. With implementation of mitigation measures from the Specific Plan EIR and new **Mitigation Measure 4.A-3**, the SEIR concluded that the Specific Plan Amendment would have less-than-significant impacts to the region’s air quality, which was consistent with the findings of the Specific Plan EIR. Therefore, the Specific Plan Amendment would not result in any new significant air quality effects that could not be mitigated to a less-than-significant level with implementation of mitigation measures identified in the Specific Plan EIR and SEIR.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the SEIR, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to air quality.

Greenhouse Gas Emissions

Effects due to emissions of greenhouse gases (GHGs) were not analyzed in the Specific Plan EIR and were not commonly analyzed in CEQA documents at the time the Specific Plan EIR was prepared and certified. The SEIR evaluated the construction and operation of the hotel permitted under the Specific Plan Amendment related to GHGs. The results of the analysis found that GHG emissions from the proposed hotel would not exceed the recommended thresholds for GHGs, with implementation of Sustainability Requirements included in the Specific Plan, as amended. These Sustainability Requirements are as follows:

The City shall require that the hotel operator be committed to the reduction of long-term operational greenhouse gas emissions to the extent feasible through the implementation of strict control measures. Control measures shall include, but are not limited to:

- Install a minimum of 6,000 square feet of solar photovoltaic panels on the rooftop.
- Improve the building envelope energy efficiency 15% over 2013 Title 24 standards;

- Incorporate high efficiency lighting (such as LEDs, metal halide post top, metal halide cobrahead or cutoff lights, or high pressure sodium cutoff lights) into public areas, such as parking lots, garages, and other exterior areas to achieve at least 15% lighting energy reduction compared to the use of mercury cobrahead lights;
- Install energy efficient appliances that comply with the most recent U.S. EPA Energy Star criteria, including refrigerators, dish washers, fans, and clothes washers;
- Incorporate water conservation strategies, including the installation of low flow faucets, toilets, and showers, as well as water efficient irrigation-systems;
- Institute recycling and composting services in order to achieve at least a 10% reduction in waste disposed.

The SEIR concluded that the City would require the future hotel operator to adhere to the Specific Plan amendment provisions that would result in a reduction in GHG emissions as part of project implementation.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the SEIR, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to greenhouse gas emissions.

Aesthetics

The Specific Plan EIR concluded that the *U.S. Navy Site Specific Plan* would result in less-than-significant visual resources impacts. The analysis within the Specific Plan EIR acknowledged that a future hotel project would introduce landscaping, a consistent architectural style, and a building scale that are compatible with the surrounding neighborhood. The Specific Plan EIR found that the effects on the scenic corridor of Sneath Lane would only be peripheral. No mitigation measures related to visual impacts were identified in the Specific Plan EIR.

The Initial Study prepared in connection with the SEIR found less-than-significant impacts to the aesthetic resources, consistent with the findings of the Specific Plan EIR. The Initial Study did not find any new potentially significant aesthetic resources effects that were not identified in the Specific EIR or a substantial increase in the severity of any previously identified significant aesthetic resources effects.

This EIR Addendum finds that the building scale and architectural style that was evaluated in the Initial Study is similar to the currently proposed Crossing Hotel Project and therefore the impacts would be similar. The Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any

new significant environmental effects or substantially increase the severity of previously identified significant effects related to aesthetics.

Agricultural and Forest Resources

Agricultural resources were analyzed in the Initial Study prepared in connection with the Specific Plan EIR (forest resources were added to the CEQA *Guidelines* Initial Study checklist subsequent to 1999). The Crossing Hotel site is in an urbanized area and these lands are not present in the project area. The Initial Study prepared in connection with the SEIR did not find in any significant effects related to agriculture or forestry resources.

Because agricultural and forest lands are not present in the project area, the currently proposed Crossing Hotel Project would have no impact to these resources, similar to the finding in the Initial Study. Therefore, this EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to agricultural and forest resources.

Biological Resources

The Specific Plan EIR determined that implementation of the projects within the Specific Plan area would result in potentially significant impacts to street trees and the birds or bats that nest in those trees during nesting or roosting season. With implementation of **Mitigation Measure G.1.a** requiring avoidance of street tree removal and damage, particularly to tree groves, as designated by the City of San Bruno Tree and Planting Ordinance and **Mitigation Measure G.1.b** requiring a tree removal permit obtained from the City of San Bruno, impacts were reduced to less than significant.

The Initial Study prepared in connection with the SEIR also concluded less-than-significant impacts to biological resources with implementation of **Mitigation Measures G.1.a** and **G.1.b**, consistent with the findings of the Specific Plan EIR. The Initial Study did not find any new potentially significant biological resources effects that were not identified in the Specific Plan EIR or a substantial increase in the severity of any previously identified significant biological resources effects.

Because the currently proposed Crossing Hotel Project is in the same location as the project area analyzed in the Initial Study, impacts to biological resources would be the same as those identified in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to biological resources.

Cultural Resources

The Specific Plan EIR concluded that development proposed as part of the Specific Plan could result in potentially significant impacts to historic resources at the U.S. Navy Site. However, it did not analyze the potential impacts to unidentified archaeological resources, unknown human remains, and unidentified paleontological resources.

The Specific Plan EIR incorporated **Mitigation Measures N.1a** and **N.1b** from the EIR for the now-terminated San Bruno Redevelopment Plan; those measures require the City to ensure there is no disruption or destruction of a unique archaeological or paleontological resource or of the significance of a historical resources, or disturbance of human remains, during construction-related exaction. Although the Redevelopment Plan is no longer in effect, these mitigation measures from the Redevelopment Plan EIR remain in effect by virtue of their having been incorporated into the Specific Plan EIR.

The Initial Study prepared in connection with the SEIR did not find any new significant effects to cultural resources or identify or new information of substantial importance since the Specific Plan EIR that resulted in any new significant environmental effects or substantial increase in the severity of previously identified significant effects related to cultural resources. The Initial Study found less-than-significant impacts to the City's cultural resources with implementation of mitigation.

Because the currently proposed Crossing Hotel Project is in the same location as the project area analyzed in the Initial Study, impacts to cultural resources would be the same as those identified in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to cultural resources.

Geology, Soils, and Seismicity

Geology, soils, and seismicity were analyzed in the Initial Study prepared in connection with the Specific Plan EIR. The Initial Study did not find any significant effects related to geology, soils and seismicity and no mitigation measures were proposed.

The Initial Study prepared in connection with the SEIR did not identify any new potentially significant effects related to geology, soils, and seismicity that were not identified in the Specific Plan EIR or a substantial increase in the severity of any previously identified significant such effects.

Under the currently proposed Crossing Hotel Project, the size of the building and floor plan is similar to the size analyzed in the Initial Study prepared in connection with the SEIR. Because the excavation quantity is slightly less than what was evaluated in the Initial Study, impacts to geology and soils would be slightly less substantial than those identified in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel

Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to geology, soils, and seismicity.

Hazards and Hazardous Materials

The Specific Plan EIR concluded that development proposed as part of the Specific Plan could result in significant impacts from hazards and hazardous materials due to inadvertent exposure of construction workers, site residents or the environment to hazardous substances.

Specific Plan EIR **Mitigation Measure L.1.d** requires all proposed project sponsors to submit a Phase II report and **Mitigation Measures C.1.a-c** reduces risk associated with development near an airport. The Initial Study prepared in connection with the SEIR determined that these mitigation measures, with minor edits, would also be applicable. The Initial Study concluded that no construction activities or operating services would result in contact with hazards or hazardous materials provided that Mitigation Measure L.1.d is implemented, and the Initial Study identified no new or substantially more severe impacts than were found in the Specific Plan EIR.

Because the currently proposed Crossing Hotel Project is in the same location as the project area analyzed in the Initial Study, impacts to hazards and hazardous resources would be the same as those identified in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to hazards and hazardous materials.

Hydrology and Water Quality

Hydrology and water quality were analyzed in the Initial Study prepared in connection with the Specific Plan EIR. The Specific Plan EIR incorporated Mitigation Measures G.2a, G.2b, and G.3a from the Redevelopment Plan EIR, which require proposed project sponsors to include post-construction stormwater controls and contractors to develop a Storm Water Pollution Prevention Plan; with these measures, impacts were reduced to a less-than-significant level.

The Initial Study prepared in connection with the SEIR concluded that with the incorporation of amended **Mitigation Measures G.2a** and **G.2b** from the Redevelopment Plan EIR, which requires all proposed project sponsors to include post-construction stormwater controls as well as amended **Mitigation Measure G.3a**, impacts to hydrology and water quality would be reduced to a less-than-significant level, similar to the finding in the Specific Plan EIR.

Under the currently proposed Crossing Hotel Project, the size of the building and site plan is similar to the size analyzed in the Initial Study prepared in connection with the SEIR, and therefore impacts would be similar to those identified in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial

importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to hydrology and water quality.

Land Use and Land Use Planning

The Specific Plan allows for a hotel use on the existing parcel. It is the last remaining undeveloped parcel within the 20-acre master planned development, referred to as The Crossing, which was the primary project area in the Specific Plan. The Specific Plan Amendment analyzed in the SEIR modified the size of the hotel and the SEIR evaluated the construction and operation impacts of the reduced size hotel. The SEIR found that the hotel project would not increase the severity of previously identified significant effects or introduce a new significant environmental effect.

Because the currently proposed Crossing Hotel Project is in the same location as the project area analyzed in the Initial Study, impacts to land use would be the same as those identified in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to land use and land use planning.

Mineral Resources

The Specific Plan EIR did not analyze the potential impacts to mineral resources with implementation of the Specific Plan. The Specific Plan site is not located in a known mineral resource area. The Initial Study prepared in connection with the SEIR did not result in any significant effects related to mineral resources, since these resources are not present in the Specific Plan area. There were no relevant changes to the physical environment since the certification of the Specific Plan EIR therefore the SEIR concluded that the Specific Plan amendment would have no impacts to mineral resources.

Because mineral resources are not present in the project area, the currently proposed Crossing Hotel Project would have no impact to these resources, similar to the finding in the Initial Study. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to mineral resources.

Noise

The Specific Plan EIR concluded that development under the Specific Plan could result in potentially significant impacts from noise or vibrations caused by construction and project-generated vehicle traffic. Implementation of projects under the Specific Plan could also cause

potentially significant impacts by exposing existing and/or new residences to new noise-sensitive uses.

The Initial Study prepared in connection with the SEIR concluded that Specific Plan EIR **Mitigation Measure I.1**, relating to compliance with the City's Noise Ordinance, and a modified **Mitigation Measure I.3.b**, requiring compliance noise insulation standards contained in Title 24 of the California Code of Regulations, would be applicable to the Specific Plan Amendment in order to address current construction practices. The Initial Study also concluded that adherence to the City's zoning ordinance would reduce daytime noise impacts and for tracking construction noise complaints and implementation of Mitigation Measures I.1 would reduce vibration impacts, resulting in less-than-significant impacts related to noise. The Initial Study identified no new or substantially more severe significant noise impacts than those identified in the Specific Plan EIR.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the Initial Study, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to noise.

Population and Housing

The Specific Plan EIR concluded that the Specific Plan would result in less-than-significant effects related to population and housing. In particular, the Specific Plan EIR concluded that future residential development in the Specific Plan area would be constrained by the density specifications and the land use restrictions in the Specific Plan. As a result, no further residential development would be permitted under the Specific Plan.

The Initial Study prepared in connection with the SEIR concluded that there were no substantial changes or new information of substantial importance since the Specific Plan EIR that would result in any new significant environmental effects or substantial increase in the severity of previously identified significant effects related to the City's population and housing. The Initial Study identified less-than-significant impacts related to population and housing, which was consistent with the findings of the Specific Plan EIR.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the Initial Study, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to population and housing.

Public Services

The Specific Plan EIR concluded that the Specific Plan would result in less-than-significant impacts related to fire and emergency services, and demand for school services. It also concluded that future buildout would result in an increase in calls for police services but such growth would not require alterations to existing facilities. The Specific Plan EIR determined that an incremental increase in calls to the San Bruno fire department could be mitigated with the installation of signals that can be pre-empted by fire protection or emergency medical response vehicles. The Initial Study prepared in connection with the SEIR incorporated **Mitigation Measure F.1** from the Specific Plan EIR to reduce this impact to less than significant as well as **Mitigation Measure F.4** which requires the assessment of the adopted school impact fee required by California Senate Bill 50.

The Initial Study prepared in connection with the SEIR concluded that there were no substantial changes in the Specific Plan Amendment or new information of substantial importance since the Specific Plan EIR that would result in any new significant environmental effects or substantial increase in the severity of previously identified significant effects related to public services. The Initial Study resulted in a less than significant impact determination for public services, which was consistent with the Specific Plan EIR.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the Initial Study, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to public services.

Recreation

The Specific Plan EIR concluded that the Specific Plan would result in less-than-significant impacts related to recreation. The Initial Study prepared in connection with the SEIR also determined that the Specific Plan Amendment would not result in any significant effects related to recreational resources.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the Initial Study, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to recreation.

Utilities and Service Systems

The Specific Plan EIR indicated that an increased use of the City's stormwater system would require implementation of **Mitigation Measures G.2a, G.2b, and G.3a** from the San Bruno Redevelopment Project Area Plan EIR. The Initial Study prepared in connection with the SEIR determined that in addition, **Mitigation Measures E.6, and E.7** from the Specific Plan EIR would also be required under the Specific Plan Amendment.

The Initial Study concluded that there were no substantial changes in the Specific Plan Amendment or new information of substantial importance since the Specific Plan EIR that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to utilities and service systems. The Initial Study resulted in a less-than-significant impact determination for utilities and service systems with the incorporation of mitigation, which was consistent with the Specific Plan EIR.

Because the land use program for the currently proposed Crossing Hotel Project, including the number of hotel rooms, meeting floor space and building size, is the same as that analyzed in the Initial Study, the impacts would be the same for the Crossing Hotel Project as those identified in the SEIR. This EIR Addendum concludes that there are no substantial changes to the proposed Crossing Hotel Site project as evaluated in the SEIR, substantial changes in circumstances, or new information of substantial importance since the SEIR was certified that would result in any new significant environmental effects or substantially increase the severity of previously identified significant effects related to utilities and service systems.

Conclusion

Based on an examination of the analysis, findings, and conclusions of the 2015 SEIR and the 2001 EIR, implementation of the currently proposed Crossing Hotel Project would not result in any new significant environmental effects or substantially increase the severity of significant impacts identified in the 2015 SEIR or 2001 EIR. Moreover, implementation of the Crossing Hotel Project would not result in project-specific significant effects which are peculiar to the project or its site and that have not been previously analyzed in the 2015 SEIR or 2001 EIR.

Attachment A. Mitigation Monitoring and Reporting Program

The Crossing Hotel Project

Mitigation Monitoring and Reporting Program

A. Introduction

When approving projects with Environmental Impact Reports (EIRs) that identify significant impacts, the California Environmental Quality Act (CEQA) requires public agencies to adopt monitoring and reporting programs or conditions of project approval to mitigate or avoid the identified significant effects (Public Resources Code Section 21081.6(a)(1)). A public agency is required to ensure that the measures are fully enforceable, through permit conditions, agreements, or other means (Public Resources Code Section 21081.6(b)). The mitigation measures required by a public agency to reduce or avoid significant project impacts not incorporated into the design or program for the project may be made conditions of project approval as set forth in a Mitigation Monitoring and Reporting Program (MMRP). The program must be designed to ensure project compliance with mitigation measures during project implementation.

The MMRP includes the mitigation measures identified in the EIR Addendum required to address the significant impacts associated with the proposed project.

B. Format

The MMRP is organized in a table format (see **Table 5-1**), keyed to each significant impact and each mitigation measure. Only mitigation measures adopted to address significant impacts are included in this program. Each mitigation measure is set out in full, followed by a tabular summary of monitoring requirements. The column headings in the tables are defined as follows:

- **Mitigation Measures:** This column presents the mitigation measure identified in the EIR.
- **Implementation Responsibility:** This column identifies the person/group responsible for implementation of the migration measure.
- **Monitoring Responsibility:** This column contains an assignment of responsibility for the monitoring and reporting tasks.
- **Monitoring and Reporting Action:** This column refers the outcome from implementing the mitigation measure.
- **Mitigation Schedule:** The general schedule for conducting each mitigation task, identifying where appropriate both the timing and the frequency of the action.
- **Verification of Compliance:** This column may be used by the lead agency to document the person who verified the implementation of the mitigation measure and the date on which this verification occurred.

C. Enforcement

If the proposed project is approved, the MMRP would be incorporated as a condition of such approval. Therefore, all mitigation measures for significant impacts must be carried out in order to fulfill the requirements of approval. A number of the mitigation measures would be implemented during the course of the development review process. These measures would be checked on plans, in reports, and in the field prior to construction. Most of the remaining mitigation measures would be implemented during the construction or project implementation phase and verified within a quarterly monitoring report.

**TABLE 5-1
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>Air Quality</p> <p>Mitigation Measure 4-A-2 (H.1.a): The City shall condition approval of individual development proposals under the Specific Plan on implementation of an appropriate dust abatement program, patterned after the BAAQMD approach described herein. The following will be required for all construction activities within the project area. These measures will reduce fugitive dust emissions primarily during soil movement and grading activities, but also during vehicle and equipment movement on unpaved project sites:</p> <ol style="list-style-type: none"> 1. All exposed surfaces (e.g., parking areas, staging areas, soil piles, graded areas, and unpaved access roads) shall be watered two times per day. 2. All haul trucks transporting soil, sand, or other loose material off-site shall be covered. 3. All visible mud or dirt track-out onto adjacent public roads shall be removed using wet power vacuum street sweepers at least once per day. The use of dry power sweeping is prohibited. 4. All vehicle speeds on unpaved roads shall be limited to 15 mph. 5. All streets, driveways, and sidewalks to be paved shall be completed as soon as possible. Building pads shall be laid as soon as possible after grading unless seeding or soil binders are used. 6. Idling times shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to 5 minutes (as required by the California airborne toxics control measure Title 13, Section 2485 of CCR). Clear signage shall be provided for construction workers at all access points. 7. All construction equipment shall be maintained and properly tuned in accordance with manufacturer's specifications. All equipment shall be checked by a certified mechanic and determined to be running in proper condition prior to operation. 8. A publicly visible sign shall be posted with the telephone number and person to contact at the Lead Agency regarding dust complaints. This person shall respond and take corrective action within 48 hours. BAAQMD's phone number shall also be visible to ensure compliance with applicable regulations. 	<ol style="list-style-type: none"> 1. City incorporates measure as a condition of approval. 2. Project sponsor includes dust abatement program in contractor specifications. 3. Contractor implements measures during construction. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. City reviews construction specifications to verify inclusion. 3. Project sponsor requires construction contractor to submit documentation of compliance following completion of project. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 3. Project sponsor and construction contractor 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. During construction 3. Prior to Issuance of Certificate of Occupancy 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>Mitigation Measure 4.A-3: The project sponsor shall ensure that construction contract specifications include a requirement that all off-road diesel-powered construction equipment used for project improvements be equipped with engines that meet or exceed either U.S. Environmental Protection Agency (USEPA) or California Air Resources Board (CARB) Tier 2 off-road emission standards, and are fitted with Level 3 Verified Diesel Emissions Control (VDEC), which would reduce diesel particulate emissions by at least 85 percent. (Engines meeting Tier 4 [Interim or Final] emission standards automatically meet the Level 3 VDEC requirement and no additional emissions control is required.)</p>	<ol style="list-style-type: none"> 1. City incorporates measure as a condition of approval. 2. Project sponsor includes dust abatement program in contractor specifications. 3. Contractor implements measures during construction. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. City reviews construction specifications to verify inclusion. 3. Project sponsor verify inclusion. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 3. Project sponsor and construction contractor City reviews construction specifications to verify inclusion. 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. Prior to issuance of grading permit. 3. Prior to Issuance of Certificate of Occupancy 	
<p>Biological Resources</p>					
<p>Mitigation Measure 4.a (G.1.b): In the event that tree removal is required, a tree removal permit would be obtained from the City of San Bruno. The City would assure that the conditions contained within the appropriate tree removal permit would be followed.</p> <p>The following measures are also applicable for removed trees.</p> <ul style="list-style-type: none"> • Tree removal will not occur between February 1 and August 31 without a bird survey conducted by a qualified biologist to determine that the tree is unused during the breeding season by avian species that are protected under California Fish and Game Codes 3503, 3503.5, and 3511. If bird species are detected or active nests are observed, the District will obtain the necessary permits from California Fish and Game, and will comply with permit conditions for protecting these species, which will likely involve seasonal avoidance or construction "exclusion zones" around nest sites. Buffer zones will be avoided during construction activities until young have fledged or the nest is otherwise abandoned. • A qualified biologist shall conduct bat surveys to determine whether any mature trees that would be removed during project construction provides hibernacula or nursery colony roosting habitat. Exclusion should be conducted at specific times of the year. Winter roosts are generally occupied between October 15 and February 28, and maternity colonies are generally occupied between April 15 and August 31. Therefore, 	<ol style="list-style-type: none"> 1. City incorporates measure as a condition of approval. 2. Project sponsor presents final site plan to City that delineates extent of tree removal. 3. If tree removal is proposed, project sponsor contracts with a qualified biologist to conduct preconstruction bird and bat surveys 4. If tree removal is proposed, project sponsor submits plan for replacement plantings. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. City reviews site plan. 3. Conduct bird and bat surveys. 4. City reviews landscaping plan. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 3. Project sponsor or contractor 4. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. Prior to issuance of grading or demolition permit. 3. Prior to construction activities. 4. Prior to issuance of grading or demolition permit. 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>exclusion, if required, should be conducted generally between August 15 and October 15 and between March 1 and April 15. If bats are using trees that need to be removed, the roosting season of the colony should be determined and the removal of the tree conducted when the colony is using an alternate roost.</p> <ul style="list-style-type: none"> The City of San Bruno will require replacement of all removed street trees with native trees and will require that the replaced trees be incorporated into a landscape plan for site development that is submitted to the City for review and approval. 					
<p>Mitigation Measure 4.e (G.1.a): The City of San Bruno, to the extent feasible, will avoid removal of, or damage to all street trees, as designated by the City of San Bruno Tree and Planting Ordinance. The following presents limitations for construction within and around trees to be preserved:</p> <ul style="list-style-type: none"> A certified arborist shall be contracted to perform a tree survey of the site to confirm the presence or absence of heritage trees within the project site prior to construction. The survey will also confirm the presence of trees that are to remain onsite according to the Tree Disposition Plan (2001). Should heritage trees be present within the project site, a certified arborist shall determine appropriate protective measures to be implemented during construction and which may include but is not limited to the following: <ul style="list-style-type: none"> A certified arborist shall be consulted prior to construction to accurately locate root protection zones and identify other specific measures that would limit potential indirect impacts on trees that may be encroached upon. For all development that would encroach into the feeder root zone (drip-line) or a 12-foot radius from the trunk, whichever is greater, of a preserved tree, special construction techniques to allow roots to breathe and obtain water will be required, as determined by the City of San Bruno (e.g., use hand equipment for trenching, protect natural resources with highly visible protective fencing, allow only one pass through an area with preserved trees). The existing ground surface within the drip-line of any tree will not be cut, filled, or compacted unless otherwise approved by the City of San Bruno. Excavation adjacent to any trees, when permitted, will be in such a manner that 	<ol style="list-style-type: none"> City of San Bruno incorporates measure as a condition of approval. Applicant contracts with a certified arborist to perform a tree survey on the project site. 	<ol style="list-style-type: none"> Verify and approve the incorporation of the measure into the construction plan. Conduct tree survey and present results to City. 	<ol style="list-style-type: none"> City of San Bruno Applicant and its contractor(s); City of San Bruno 	<ol style="list-style-type: none"> Prior to project approval. Prior to construction activities. 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<ul style="list-style-type: none"> - will cause only minimal root damage. Permission and inspection will be required prior to back-filling. - Construction staging areas shall be designated on plans and prohibit parking, loading, digging (especially trenching), and grading during all construction activities within root protection zones of all trees. There will be no parking or storing of vehicles, equipment, machinery, construction materials, or construction trailers, and no mechanical excavation, construction of buildings or dumping of oils or chemicals within the drip-lines of any trees. - Prior to the start of any clearing, stockpiling, trenching, grading, compaction, paving, or change in ground elevation on a site with saved protected trees, fencing will be installed at the drip-line. Prior to grading or issuance of any permits, the fences may be inspected and the location thereof approved by appropriate county staff. The county requires the installation of a 6-foot-high chain-link fence around the drip-line of preserved trees during construction and demolition phases. A 4-inch layer of chipped bark mulch should be placed over the soil surface within the fenced dripline prior to installing temporary fencing. Suitable mulch must contain bark "fines." Maintain this layer of mulch throughout construction. - Pruning shall be overseen by a certified arborist for all protected trees, and should be done to clean and raise canopy per International Society of Arboriculture pruning standards. - A drainage plan shall be designed that will avoid heritage trees. - Construction drawings shall accurately locate areas to be avoided such as tree trunks and root protection zones. - A pre-construction meeting conference shall be held with contractors to review BMPs and require bonding and fines to ensure the replacement of any inadvertently damaged trees. - Whenever possible, existing grade shall be maintained within the fenced portion of the dripline. 					

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>Hazards and Hazardous Materials</p> <p>Mitigation Measure 8.a (L.1.d): The City of San Bruno will require all proposed project sponsors to submit a Phase II report, based on the September 17, 2013 Phase I findings, and subsequent reports as may be required by the City of San Bruno, completed no more than 18 months prior to approval of a proposed project by City Council to assure no additional contamination is present from overlooked USTs or other unknown sources. The City of San Bruno will require that any project sponsor incorporate the recommendations of the Phase II report into the design of the proposed project.</p>	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor and its contractor(s) shall retain a qualified environmental professional to prepare a Phase II report. 3. Project sponsor and its contractor(s) shall incorporate the recommendations of the Phase II report into the design of the proposed project. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. Review construction plans for inclusion of Phase II results. 3. City reviews results of Phase II and any applicable project revisions. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. Project sponsor and its contractor 3. City of San Bruno 	<ol style="list-style-type: none"> 1. Prior to Project Approval. 2. Prior to ground-disturbing activities. 3. Prior to ground disturbing activities. 	
<p>Mitigation Measure 8.e</p> <p>C.1a: The City of San Bruno will require that all building heights and associated roof structures proposed under the Specific Plan be consistent with the height limitations defined by FAR Part 77. Prior to issuance of any demolition or construction permits, the City of San Bruno shall require the project sponsors for any project within the Specific Plan area to provide appropriate notification to the FAA via FAA Form 7460-1.</p> <p>C.1b: The City of San Bruno shall prohibit the following uses within the Specific Plan area:</p> <ul style="list-style-type: none"> • Any use that would direct a steady or flashing light of white, red, green, or amber color toward an aircraft engaged in an initial straight climb following takeoff or toward an aircraft engaged in a straight final approach toward a landing, other than FAA-approved navigational lights. • Any use that would cause sunlight to be reflected toward an aircraft engaged in an initial straight climb following takeoff or engaged in straight final approach toward a landing. • Any use that would generate smoke or rising columns of air. • Any use that would attract large concentrations of birds within 	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor to provide notification to the FAA via FAA Form 7460-1. 3. Project sponsor to submit disclosure notice to City for approval. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. City to review FAA response to Form 7460-1. 3. City review disclosure notice. 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno 3. City of San Bruno. 	<ol style="list-style-type: none"> 1. Prior to project approval 2. Prior to issuance of building permit. 3. Prior to issuance of building permit. 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>approach-climbout areas.</p> <ul style="list-style-type: none"> Any use that would generate electrical interference that may interfere with aircraft communications or aircraft instrumentation. <p>C.1c: The City shall require all sponsors of new dwelling units and/or buildings for human occupation to record a notice of fair disclosure, regarding the proximity of the proposed development to San Francisco International Airport and of the potential impacts of aircraft operation, per the recommendations contained in the Final Report of the 1998 San Mateo County Civil Grand Jury, as implemented by the City of San Bruno.</p> <p>Cultural References</p> <p>Mitigation Measure 5 b (N.1b): If cultural resources or human remains, prehistoric or historic-period archaeological resources are encountered during construction of a project, all construction activities within 100 feet will halt and the resources and their context shall not be further disturbed until a qualified cultural resource consultant has evaluated the situation. The City of San Bruno shall assure that identified cultural resources are recorded on proper historical properties forms.</p> <p>A Secretary of the Interior-qualified archaeologist will inspect the findings within 24 hours of discovery. If it is determined that the project could damage a historical resource or a unique archaeological resource (as defined pursuant to the CEQA Guidelines), mitigation will be implemented in accordance with PRC Section 21083.2 and Section 15126.4 of the CEQA Guidelines, with a preference for preservation in place. Consistent with Section 15126.4(b)(3), this may be accomplished through planning construction to avoid the resource; incorporating the resource within open space; capping and covering the resource; or deeding the site into a permanent conservation easement. If avoidance is not feasible, a qualified archaeologist will prepare and implement a detailed treatment plan in consultation with the City of San Bruno. Treatment of unique archaeological resources shall follow the applicable requirements of PRC Section 21083.2. Treatment for most resources would consist of (but would not be not limited to) sample excavation, artifact collection, site documentation, and historical research, with the aim to target the recovery of important scientific data contained in the portion(s) of the significant resource to be impacted by the project. The treatment plan will include provisions for analysis of data in a regional context, reporting of results within a timely manner.</p>	<ol style="list-style-type: none"> City of San Bruno incorporates measure as a condition of approval. If cultural/archeological resources are encountered, a Secretary of the Interior-qualified archaeologist shall inspect the findings within 24 hours of discovery and report to City. If cultural/archeological resources are encountered, Archaeologist shall conduct independent review and prepare treatment plan, if necessary. If cultural/archeological resources are encountered, Project sponsor or its contractor(s) shall implement treatment plan. If cultural/archeological resources are encountered, City of San Bruno shall assure that identified cultural resources are recorded on proper historical properties forms. 	<ol style="list-style-type: none"> City adopts condition of approval with project. If resources are encountered, verify work is suspended. If resources are encountered, review and approve treatment plan. If resources are discovered, implement treatment plan. Review historic properties forms. 	<ol style="list-style-type: none"> City of San Bruno Project Sponsor and Archaeologist; City of San Bruno Archaeologist; City of San Bruno. Project sponsor and archaeologist. City of San Bruno 	<ol style="list-style-type: none"> Prior to project approval Upon cultural resources discovery. Upon cultural resources discovery. Upon cultural resources discovery. Prior to issuance of Certificate of Occupancy. 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>curation of artifacts and data at an approved facility, and dissemination of reports to local and state repositories, libraries, and interested professionals.</p> <p>Hydrology and Water Quality</p> <p>Mitigation Measure 9a(G.3a): The City shall require all contractors to develop and implement a SWPPP, as required by the State Water Resources Control Board NPDES General Construction Permit, for areas to be disturbed by construction activities. . At a minimum, the SWPPP shall include the following:</p> <ul style="list-style-type: none"> • A construction schedule that restricts excavation and grading activities to the dry season (generally April 15 to October 15) to reduce erosion associated intense rainfall and surface runoff. The construction schedule shall indicate a timeline for earthmoving activities, hydroseeding, and stabilization of soils; • Soil stabilization techniques such as hydroseeding and short-term biodegradable erosion control blankets; • Silt fences, hay bales, or some kind of inlet protection at downstream storm drain inlets; and • The post-construction inspection of all drainage facilities and clearing of drainage structures of debris and sediment. <p>Mitigation Measure 9c</p> <p>G.2a: The City of San Bruno shall require, for incorporation into all redevelopment designs, permanent stormwater controls such as vegetated swales, retention ponds, landscape areas, etc., in accordance with MS4 NPDES and San Mateo Countywide Water Pollution Prevention Program (SMCWPPP) requirements, for the stormwater collected from new parking lots and other impervious surfaces.</p> <p>G.2b: To help minimize the amount of runoff containing urban pollutants, streets, and parking areas in the redevelopment subareas shall be frequently cleaned by the City of San Bruno using street-sweeping equipment, and the collected material properly disposed.</p>	<ol style="list-style-type: none"> 1. City of San Bruno incorporates measure as a condition of approval. 2. Project sponsor and its contractor(s) shall prepare a SWPPP that adheres to all specifications of this measure. 3. Inspect construction site for adherence to SWPPP. 	<ol style="list-style-type: none"> 1. City adopts condition of approval with project. 2. Review and approve SWPPP. 3. Include any findings in monitoring report 	<ol style="list-style-type: none"> 1. City of San Bruno 2. City of San Bruno; RWQCB 3. City of San Bruno; RWQCB 	<ol style="list-style-type: none"> 1. Prior to project approval. 2. Prior to issuance of construction and grading permit(s). 3. One inspection each phase of construction. 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
Noise					
Mitigation Measure 12.a					
<p>I.3.b. All development under the proposed Specific Plan shall be constructed to comply with the relevant noise insulation standards contained in Title 24 of the California Code of Regulations (Part 2, Appendix Chapter 12A).</p> <p>I.1: The project applicant will obtain a permit to construct from the Director of Public Works prior to the start of construction activities, since construction would exceed the specified noise levels in the City Municipal Code. Noise-generating construction activities would be limited to reasonable daytime hours, such as between the hours of 7:00 a.m. and 7:00 p.m. on weekdays. No construction activities will be allowed on weekends or national holidays. Information concerning construction-related activities and construction hours should be distributed throughout the affected areas and incorporated as part of the Specific Plan. Heavy construction activities would be prohibited on Saturdays and Sundays.</p>	<ol style="list-style-type: none"> Project sponsor shall submit building plans documenting compliance with Title 24 noise standards. Project sponsor shall obtain construction permit. 	<ol style="list-style-type: none"> Review plans for compliance. Review plans for compliance. 	<ol style="list-style-type: none"> City of San Bruno City of San Bruno 	<ol style="list-style-type: none"> Prior to Issuance of building permit. Prior to issuance of building or grading permit(s) 	
Public Services					
<p>Mitigation Measure 14.a.i (F.1): The City of San Bruno shall install signals that can be pre-empted by fire protection or emergency medical response vehicles. Developers shall contribute a "fair share" portion of the costs of these pre-emptive signals as determined by the City of San Bruno.</p>	<ol style="list-style-type: none"> City of San Bruno incorporates measure as a condition of approval. Project sponsor to pay fair share fee for traffic signal installation. 	<ol style="list-style-type: none"> City adopts condition of approval with project. Verification of fee payment 	<ol style="list-style-type: none"> City of San Bruno City of San Bruno 	<ol style="list-style-type: none"> Prior to project approval. Prior to issuance of Certificate of Occupancy. 	
<p>Mitigation Measure 14.a.iii (F.4): The City of San Bruno shall coordinate payment to the appropriate school districts of the school impact fee required by California Senate Bill 50.</p>	<ol style="list-style-type: none"> City of San Bruno to provide assistance as needed to project sponsor to coordinate payment of required school impact fee. 	<ol style="list-style-type: none"> Verification of fee payment 	<ol style="list-style-type: none"> City of San Bruno 	<ol style="list-style-type: none"> Prior to issuance of Certificate of Occupancy. 	
Utilities and Service Systems					
Mitigation Measure 17.c					
<p>E.6: Prior to the issuance of building permits, the City of San Bruno shall require that all designs for residential and commercial development within the Specific Plan area include adequate storage space for projected recyclable and compostable materials. The City shall require adequate storage space on each floor of</p>	<ol style="list-style-type: none"> City of San Bruno incorporates measure as a condition of approval. Project sponsor shall submit building 	<ol style="list-style-type: none"> City adopts condition of approval with project. Review plans for 	<ol style="list-style-type: none"> City of San Bruno City of San Bruno 	<ol style="list-style-type: none"> Prior to project approval. Prior to issuance 	

**TABLE 5-1 (Continued)
MITIGATION MONITORING AND REPORTING PROGRAM**

Mitigation Measures	Implementation Procedure	Monitoring and Reporting Action	Monitoring Responsibility	Monitoring Schedule	Verification of Compliance
<p>each building and in its enclosed garbage areas, as well as adequate loading space, to accommodate the City of San Bruno's recycling program.</p> <p>E.7: As a condition of project approval and before demolition and construction, the City of San Bruno shall require the demolition and construction contractors to maximize diversion of materials remaining from the demolition of structures and the byproducts of construction. The City shall require that project sponsors work with the City of San Bruno's Public Works Department and submit a recovery plan for maximizing diversion of construction and demolition materials associated with construction of any project in the Specific Plan area, so that at least 50 percent of the demolition debris is, if feasible, recycled or can be used as alternative landfill cover.</p>	<p>plans documenting compliance.</p> <p>3. Project sponsor and construction contractor(s) to submit construction/ demolition waste diversion plan to City for approval.</p>	<p>compliance.</p> <p>3. Review diversion plan.</p>	<p>3. City of San Bruno.</p>	<p>of building or grading permit.</p> <p>3. Prior to issuance of building or grading permit.</p>	

EXHIBIT F

US Navy Site and Its Environs Specific Plan, Amended 2015

Hotel (up to 152 rooms)

Design Guidelines

Building Orientation to Streets and Pedestrian Corridors: Any hotel building in the Specific Plan area shall be designed to give the appearance of fronting on El Camino Real and the main access street.

Ancillary commercial and restaurant uses should be focused towards the pedestrian, whether located at the corner of Admiral Court and El Camino Real, toward the central esplanade, or on an upper level near a potential pedestrian overpass across El Camino Real. Any such ancillary components shall be designed to physically connect and relate to the main building but shall also be designed as a distinct part of the building facade, with materials and treatments that offer visual interest to the pedestrian. Display windows, awnings, outdoor dining areas and/or landscaped plazas may be used to create attractive entryways.

The hotel building shall be designed to minimize shadows and encourage light and air exposure. It is envisioned that a connection to a future pedestrian overpass across El Camino Real could be incorporated into the design at the second or third level of the hotel.

Parking shall not dominate the experience along any pedestrian route, but should be located to the rear of the main building near I-380 and should be shared with other uses, to the extent possible. Below grade parking is a preferable design option.

Any above grade parking structures shall be designed as an integral part of the main building or blend with the commercial building adjacent to it. All parking structures shall be heavily landscaped and screened from pedestrian view. Passenger loading areas when feasible shall be designed as an integral part of the main building entry feature. Delivery, loading docks, and trash facilities shall be screened from public view and designated as loading zones only. Design elements shall be incorporated into the building(s) to facilitate recycling. Fire access and fire lanes shall be designed according to standards of the San Bruno Fire Department and incorporated as an integral part of the site plan.

Building Design: Hotels shall be oriented to maximize views of San Bruno Mountain, the coast range, San Francisco International Airport and the San Francisco Bay. All sides of the buildings should be fully articulated, particularly areas facing El Camino Real, pedestrian plazas and streets within the Specific Plan area. Multi-story buildings should be designed to clearly express a strong building base, middle and cornice/roof treatment.

The ground floor should provide a strong architectural base for the building within which varied storefront and commercial treatments could occur. The primary building entrance shall be at ground level, accessible from, and oriented toward a street or plaza. The middle floors should be fully articulated with variations provided in both vertical and horizontal dimensions. The uppermost part of the building should be designed as part of the roof/cornice treatment for the building and may include loggias or terraces.

Building materials must convey a sense of durability and permanence. Heating, ventilation, air-conditioning and generator units on the hotel may be roof mounted and shall be screened from

pedestrian view with materials matching the exterior of the building. Utility connections and transformers shall be screened from pedestrian view. All utilities shall be underground.

A landmark tower element is encouraged near El Camino Real.

Development Standards

Land Use Classification:

Major Hotel & Ancillary Uses

Number of Rooms: Up to 152

Minimum Site Area: ½ acre

Maximum Height

Ninety (90) feet (including elevator penthouse and ancillary mechanical equipment, measured from existing grade. However maximum height is also governed by FAA regulations related to protection of airspace in the vicinity of SFO. The developer will be required to notify the FAA of a proposed development project by filing Form 7460-1. The FAA will determine whether the project would constitute an obstruction or hazard to safe air navigation. The maximum height for this site based on preliminary analysis of FAA criteria is estimated to be approximately 73 feet.

Maximum # of floors

Five (5) floors of hotel rooms plus a lobby level for services and a potential parking below grade.

Building Setbacks

New buildings should be brought to the sidewalk's edge, except that along El Camino Real a minimum ten (10) foot wide landscaping area is required. Larger setbacks of up to twenty (20) feet are allowed for small plazas, entryways, and outdoor eating areas.

Landscaping

A minimum of 15% of the total site area shall be devoted to landscaping.

Parking

One (1) space per room with fifteen percent (15%) reduction for transit-oriented development and shared parking. Plus, one (1) space for each one hundred (100) square feet of meeting space.

Signage

A Master Sign Program, as regulated by the City of San Bruno Sign Ordinance, was developed for the entire development at The Crossing. Individual coordinated sign plans shall be provided for each building, consistent with the Master Sign Program, and must show sign placement, size, lettering style, and materials. Wall signs must be integrated in the design of the building with appropriate and compatible colors and materials.

Sustainability Requirements: The City shall require, as a Condition of Approval, that the hotel operator be committed to the reduction of long-term operational greenhouse gas emissions to the extent feasible through the implementation of strict control measures. Control measures shall include, but are not limited to:

- Install a minimum of 6,000 square feet of solar photovoltaic panels on the rooftop.
- Improve the building envelope energy efficiency 15% over 2013 Title 24 standards;

- Incorporate high efficiency lighting (such as LEDs, metal halide post top, metal halide cobrahead or cutoff lights, or high pressure sodium cutoff lights) into public areas, such as parking lots, garages, and other exterior areas to achieve at least 15% lighting energy reduction compared to the use of mercury cobrahead lights;
- Install energy efficient appliances that comply with the most recent U.S. EPA Energy Star criteria, including refrigerators, dish washers, fans, and clothes washers;
- Incorporate water conservation strategies, including the installation of low flow faucets, toilets, and showers, as well as water efficient irrigation-systems;
- Institute recycling and composting services in order to achieve at least a 10% reduction in waste disposed.



May 28, 2015

Mark Sullivan
City of South San Francisco
567 El Camino Real
San Bruno, CA 94066

Re: **Operations Statement and Support Statement**

OTO is planning to develop a SpringHill Suites hotel by Marriott at the proposed site at 380 and El Camino in San Bruno. The hotel will be designed for inspiration-seeking travelers who want more than a traditional hotel experience. SpringHill Suites by Marriott® provides a welcoming, upbeat, smart and stylish environment that adds enjoyment to travel. A breath of fresh air. Springhill Suites maintains the highest guest satisfaction scores in the Marriott Portfolio.

The Springhill Suites brand offering appeals directly to the Bay Area hi-tech professional with the following amenities:

- Upscale and contemporary design inside and out
- All rooms are spacious suites with a microwave and fridge in every room to accommodate longer stays more comfortably
- Fully-equipped business center
- High-speed internet with increased capacity to handle greater demand
- High-quality exercise facility
- Swimming pool
- Complimentary hot buffet breakfast
- Flat screen HD TV
- On-site meeting space
- Luxury bedding
- Beer, Wine and food pantry available 24 hrs.
- Marriott Rewards Points

EXHIBIT G

OTO Development is proposing to develop an approximately 152 room Springhill Suites by Marriott. The hotel will be five-stories in height, approximately 74 feet tall, 99,000 square feet in size and contain 163 parking spots. Two natural gas shuttles will be available to transport hotel patrons to local businesses and to pick up and drop them off at the San Francisco International airport. The hotel will feature 3,000 square feet of meeting space which can accommodate approximately 150-200 people for sit down meetings, weddings and parties for up to 300 people for theater style events, depending upon the configuration.

The hotel site in San Bruno at The Crossing is an exciting opportunity to develop a business class/life-style hotel in a major Bay Area market. This hotel will provide convenient access to both highway 101 and 280, via the 380 freeway. The hotel will serve as both a local amenity to the citizens of San Bruno as well as to those in the business community in Bay Hill Office Park, the bio-tech industry and the San Francisco airport. In addition, the hotel will serve overflow demand coming from the city of San Francisco.

The hotel will provide food service for hotel guests and conference and meeting attendees. The hotel will be operated 24 hours a day, but food service will only be available for breakfast (offered in a buffet format) and limited food service will be provided in the bar area in the evening for guests and during catering and banquet events. No entertainment will be provided in the bar/lounge other than televisions and periodic entertainment as provided by catering or banquet guests. In addition, no room service will be provided.

For smaller functions and in the evenings we will serve pizzas, burgers, salads, etc. from the food prep area (shown on the plans). The market/pantry (shown on the plans) will sell snacks, sandwiches, wine and beer, etc. and sundries to hotel guests. Guests are expected to eat lunch off-site, but some may eat a light meal in the bar area in the evening. The bar is expected to have seating which includes stools and sit down tables. OTO will apply for an ABC license to allow liquor sales for the food service, bar, and conference and/or special events such as weddings. Guests will also be able to eat and drink in the outside patio areas near the pool. Bar hours will be approximately 5 pm-9 pm or 10 pm. The seating area will be shown on the plans and will be approved by ABC when we obtain our liquor license. The "prep area" will include some kitchen/cooking facilities in order to reheat and prepare light meals. Food will likely be catered for events larger than 50 people or if the guests want a full sit down meal. OTO will work with Jack's restaurant in order to provide a small amount of shared parking, for a fee, during the lunch hour. Any additional shared parking will be considered after studying the parking situation for the first six months and only provided if the hotel can reasonably do so without negatively impacting its own parking needs. A parking area will be provided for employees, but they will be encouraged to use mass transit. Most of our employees work during the day, so the amount of parking required in the evening, when the hotel is full with guests, will be no more than 2-3 spots after 9 pm.

Parking

Parking will be provided for employees underneath the building. Additionally, bike parking will be provided for employees in a locked cage, inside a door way which leads to the outside parking and driveway area (location will be indicated on the plans). Tandem parking will be provided on both lower levels of the hotel parking, but it is primarily intended to be used for employee parking or for valet parking during a large event. When not in use, the tandem parking will be cordoned off so that guests do not park in the spots and get stuck with a car behind them. During larger events, OTO will hire a local valet parking company to park cars for a fee. OTO believes that we can increase parking spots by 30-40% by using valet parking.

Deliveries at the hotel will primarily come in the front entrance during non-peak check in and check-out times. For larger catering events, deliveries will come in to the building in the rear, adjacent to the underground parking entrance and come into the kitchen and holding area, or come into the building through the parking garage and elevator. Catering and event deliveries will be done prior to any event so that there is no conflict with guests arriving for the banquet.

JUL 01 2015

June 29, 2015

RECEIVED

■ 300 Frank H. Ogawa Plaza
Suite 375
Oakland, CA 94612

■ www.archdim.com

■ James M. Heilbronner
Architect C 11531

Ms. Paula Bradley, MCP, AICP
Associate Planner
City of San Bruno
Planning Department
567 El Camino Real
San Bruno, CA 94066

Re: SpringHill Suites
El Camino Real & Highway 380
San Bruno, CA 94066

Subject: **Proposed "Green Building" Design Techniques**

Dear Paula,

Below is a list of proposed green building techniques that will be incorporated into the SpringHill Suites project. This letter serves as our good-faith intent to make this project "green". Our proposed techniques are based on the *California Green Building Code* and *LEED 2009 for New Construction and Major Renovations Checklist*.

Planning and Design

- The site is located within ½ mile of various services to create community connectivity
- The site is located within a ¼ mile walking distance to three public transportation access points
- The proposed project will have bicycle storage and shower/changing rooms

Site and Landscape

- Erosion Control Plan to be developed
- LEFE Vehicle charging stations
- Preferred parking for carpools and vanpools
- Reduced heat island effect on roof and non-roof areas
- Landscaping to incorporate native species and drought tolerant plants

Energy Efficiency

- Zero use of chlorofluorocarbon (CFC) based refrigerants in HVAC systems
- Optimize energy performance in the HVAC system design
- Use solar voltaic technology

Water Use

- Water efficient landscaping

EXHIBIT H

- Reduced potable water waste/usage
- Employ low flow technology for water reduction

Materials and Resources

- Provide adequate space for storage and collection of recyclables
- Develop a construction waste management plan
- Recycle construction content and use regional materials

Environmental Quality

- Implement an IAQ Management Plan during construction and before occupancy
- Use low emitting materials
 - Low-VOC adhesives, sealants, paints and coatings
 - Low-VOC flooring systems
 - No urea-formaldehyde and adhesives on wood and agrifiber products
- Each individual area will have lighting and thermal control
- Design will provide clear views of the exterior to 90% of the occupants in regularly occupied spaces

Please contact our office if you need additional information.

Respectfully,
Architectural Dimensions



Jeffrey MacAdam, Project Architect

Enclosures
CC: OT040
2015-06-29AH

JUL 01 2015

RECEIVED

ARCHITECTURAL
DIMENSIONS

■ 300 Frank H. Ogawa Plaza
Suite 375
Oakland, CA 94612

■ www.archdim.com

■ James M. Heilbronner
Architect C 11531

June 18, 2015

Ms. Paula Bradley, MCP, AICP
Associate Planner
City of San Bruno
Planning Department
567 El Camino Real
San Bruno, CA 94066

Re: SpringHill Suites
El Camino Real & Highway 380
San Bruno, CA 94066

Subject: **Proposed "Green Building" Techniques during the Construction Stage**

Dear Paula,

Below is a list of proposed green building techniques that will be implemented during the construction stage of the SpringHill Suites project. This letter serves as our good-faith intent to make this project "green". Our proposed techniques are based on the *California Green Building Code* and *LEED 2009 for New Construction and Major Renovations Checklist*.

Construction Waste Management Plan

The Construction Waste Management Plan will be used by all subcontractors of this project and the General Contractor will be charged with implementing the Plan throughout the course of construction.

Project Goals

- The goal for this project is to meet the requirements set forth by the State of California to divert a minimum of 75% (by weight) of non-hazardous construction waste.
- In order to achieve this we will recycle and separate the following waste materials versus using landfills and incinerators:
 - Cardboard
 - Clean dimensional wood
 - Brick and CMU
 - Recyclable Plastics
 - Gypsum wallboard
 - Asphalt and concrete paving
 - Used equipment oil
 - Ferrous and non-ferrous metals
- All other waste materials, not listed above, will be handled as general refuse and disposed of at a license and permitted landfill.



Implementation

- The Construction Waste Management Plan will be distributed to all subcontractors working on this project. The General Contractor will discuss goals and handling procedures of the waste generated.
- There will be a coordination meeting at the beginning of the construction stage to discuss:
 - Plan requirements
 - A review of waste handling procedures
 - Location of dumpsters/bins
 - Waste segregation requirements
 - Discussion regarding cross contamination waste
 - Discussion regarding responsibility of moving waste from building to applicable bins
 - Enforcement requirements
- During future coordination meetings, the management staff will determine what type of waste will most likely be generated in the immediate future and instruct the subcontractors as to where it shall be placed for recycle, salvage, or disposal.

Expected Project Waste, Disposal and Handling

Construction of the project provides opportunity to divert waste and debris away from the landfill. Every member of the on-site construction team will play an important role in achieving the project Construction Waste Management goal. The list of materials to recycle includes, but may not be limited to: asphalt, concrete, masonry, metal, wood, drywall, ACT, carpet, and cardboard. Excavated soil, land-clearing debris and hazardous material will not be included in calculations for the waste diversion.

- The waste material generated will be separated according to the above list for recycling and kept in the building until it is ready to be placed in the proper recycling container.
- The waste materials shall be protected from contamination from hazardous materials that may keep them from being recycled properly.
- The recycled materials will be collected by a waste hauler and hauled to a license and permitted recycling center.
- OTO Development will hire a waste management company to sort recyclable material from landfill debris and to provide documentation for meeting LEED Credit MR 2.1.
- This information will be used to complete the LEED-2009 Template form.
- Monthly Waste Management reports will be submitted by the General Contractor at every other progress meeting to update progress to the Owner, Engineer, and LEED consultant.

Construction Indoor Air Quality (IAQ) Management Plan**Control Measures**

HVAC equipment and ductwork will be protected from dust and other pollutants via the following procedures:

- Openings into installed ductwork and air-handling equipment not in active use will be sealed using taped plastic, taped cardboard, or other reasonably air-tight coverings. Sealing will occur prior to, or immediately upon installation of the ductwork or equipment. Regular walk-throughs will be conducted by the Indoor Air Quality Representative to check for damaged or displaced coverings. Repair or replacement of damaged or displaced coverings will occur immediately upon discovery, at the direction of the Indoor Air Quality Representative.
- Construction work that generates air pollution will be avoided where ductwork or air handling equipment is being installed. If visible air pollutants are present in a space where ductwork is to be installed, spot cleaning or other measures will be used to prevent ductwork or equipment contamination.

Use of Mechanical Systems during Construction

HVAC equipment and ductwork will be protected from dust and other pollutants via the following procedures:

- Exhaust and makeup air supply systems: when a system is operated during construction, filters will be replaced upon completion of construction with MERV 13 filters.
- Central air systems will be subject to these provisions when operated during construction:
 - The central AHU will be protected with a temporary filter having a minimum rating of MERV 8, per ASHRAE 52.2-1999.
 - Distribution elements needing filters, including all return air ductwork, will be protected with temporary filters having a minimum rating of MERV 8 per ASHRAE 52.2-1999 unless otherwise noted below.
 - If used for prolonged periods, filters will be periodically inspected and replaced if dirty.
 - All components of the distribution on the return side will be protected, including but not limited to:
 - The portion of the air handler upstream of the central fan
 - Return vents, ducts and shafts
 - VAV box intakes
 - Transfer ducts
 - Components of the distribution system on the supply side will typically not need protection except:
 - If portions of the supply system become contaminated, coarse filters will be applied to completely cover supply outlets and prevent the distribution of particulates into building spaces.

Filter Replacement and Tracking

- MERV 8 filters used for ductwork protection will be replaced on an as-needed basis, as determined by the IAQ Representative.
- Upon completion, the MERV 8 filters used for ductwork protection will be discarded. New filters will be installed at all air handlers.

- A schedule of filter replacements (showing location, time, and filter type) will be recorded and included in the final Plan documentation.

Duct Cleaning

Duct Cleaning will be considered a last resort measure in the event of a failure of other measures. If duct cleaning is needed:

- The work will be done by experienced professionals skilled in the task, using specialized equipment and following the requirements of the *General Specifications for the Cleaning of HVAC Systems*.
- If it is found that duct liner, ductwork, or equipment is too contaminated to be cleaned successfully, it will be replaced.
- If construction is still underway at the completion of cleaning, all openings required to be sealed per this Indoor Air Quality Management Plan will be resealed as soon as possible after cleaning.
- A log recording all duct cleaning that takes place during construction will be created.

Source Control

This IAQ Management Plan is predicated on the use of low-emission interior products which comply with the following VOC limit standards:

- Adhesives, Sealants, and Sealant Primers: Bay Area Air Quality Management District (BAAQMD) Rule #1168.
- Composite wood, agrifiber products, and laminating adhesives: urea formaldehyde-free.
- Clear wood finishes, floor coverings, stains, sealers, and shellacs applied to interior elements: Bay Area Air Quality Management District (BAAQMD) Rule 1113.

Modifying Equipment Operation

To reduce air pollution during construction:

- Electric equipment will be used instead of gasoline-powered equipment whenever practical.
- Bottled gas will be used in place of diesel fuel whenever practical.
- Exhaust from gasoline or diesel vehicles will be kept away from air intake pathways.
- Fuel-burning equipment will be cycled off during extended periods between uses.

Local Temporary Practices

Where available, operable vents and windows will be opened to ventilate the building during application of interior finishes when weather conditions are suitable. Spaces with fixed glazing or no windows will be ventilated by localized temporary exhaust, as described below, or by using building mechanical systems.

Protection of Stored Materials

Measures will be taken to minimize dust accumulation on material surfaces and the absorption of other pollutants by absorbent materials. The measures will include the following:

- Materials will be handled and stored according to the manufacturer's recommendations.

- Unwrapped absorbent materials will be shrink-wrapped if necessary.
- Highly absorbent materials like duct liner, acoustic tile, carpeting, or insulation will be stored indoors in the original packaging, or covered and sealed.
- Moderately porous materials like gypsum board will be stored indoors, wrapped or away from dust and materials prone to off-gas VOCs.
- Framing lumber will be stored indoors whenever possible. If stored outdoors, the lumber will be covered with a water proof covering, stored off the ground, and located away from standing water.
- Dense material like glass, metal framing, ductwork and equipment will be covered and kept dry.
- If condensation forms on cold material, care will be taken not to expose it to dust or other particles. If exposed to pollution, housekeeping measures will be used promptly to clean the material before installation.

Preventing Contamination of Completed Areas from Work under Construction

When work is completed in an area, the area will be protected from pollutants generated in other parts of the building still under construction. One or more of the following methods of pathway interruption will be used:

- Erecting barriers between completed areas and areas still under construction
- Where present, doors and windows will be closed and locked between completed portions of the building and portions of the building still under construction. The closures will be further sealed with tape, plastic sheeting and/or sealant, if necessary.

Housekeeping

The following housekeeping measures will be employed as part of the Indoor Air Quality Management Plan:

- A regular housekeeping schedule will be instituted. Cleaning measures and frequency will be selected according to the pollutants generated in a space.
- Where applicable, dust will be suppressed by the use of low-odor wetting agents and sweeping compounds.
- Low-odor cleaning agents will be used.
- Spills of water or solvent will be cleaned up immediately.
- Attention will be given to cleaning hidden or hard-to-reach surfaces, such as wall cavities, tops of door, ledges, and behind water closets.

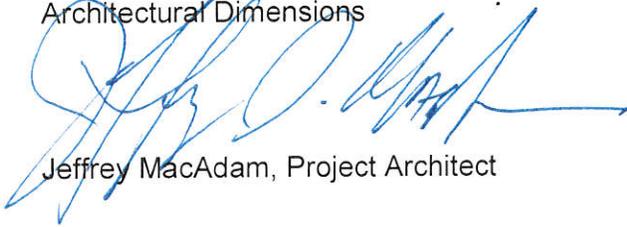
Scheduling

- All material deliveries will be scheduled so that materials are installed quickly and storage is minimized. Sawing, sanding, and cutting of wood and metals will be completed in the designated and unoccupied area outside the tenant space. All wood should arrive pre-finished to avoid the strong odor from coating wood within a confined space. Installation of all products requiring epoxy should be completed after-hours due to odor potential.

- The main air handling unit serving the floor will be shut down during drywall sanding so as to limit contaminating the mechanical system and minimize dust migration throughout the space. Additionally, the mechanical system will be covered and protected during drywall sanding. No ductwork will be installed while drywall is sanded. Vacuum sanding equipment will be utilized to limit drywall dust.

Please contact our office if you need additional information

Respectfully,
Architectural Dimensions



Jeffrey MacAdam, Project Architect

Enclosures

CC: OT040

2015-06-18AH

ARC-Fixture selected

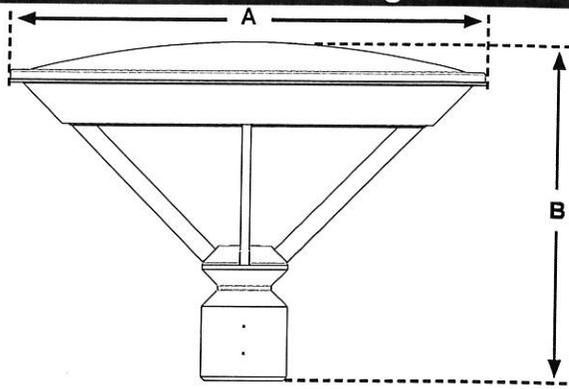
San Bruno - Option 3

Applicant's submittal
12/10/15

Premier II LED	Project Name:	Catalog Number:	Type
-----------------------	---------------	-----------------	------



Dimensional Drawings



Fixture	A	B	Max. LEDs	Lbs
PRE-2-L	27"	19"	96	56



Contemporary design meets the new generation of LED green technology in this stunning luminaire, the **Premier II**.

The **Premier II** fixture is built with a cast aluminum housing, extruded aluminum arms and a spun aluminum, removable top.

The **Premier II** series provides up to fifty percent energy savings over traditional HID light sources; and offers excellent beam control and LED life beyond 100,000 hours for 350 milliamp systems. The **Premier II** is offered in six optical distributions.

The popular **Premier II** is the perfect compliment to any university campus, business park, or walkway project where contemporary, architectural design is desired.

The **Premier II**'s fully-sealed housing features an exceptionally well-designed thermal management system that provides superior heat dissipation.

Model	Optics	Source	Milliamps	Kelvin	Voltage	Mounting	Finish	Options
PRE-2-L	Type I (T1) 	# of LEDs 32 (32LC)	mA 350 (3)	4000K *Neutral white (4K)	120-277 *Universal voltage (UNV)	Post Top *Slips over 4" ØD tenon x 4" tall (PT)	Bronze (BZ)	<u>Button Type Photocell</u> <small>*Specify voltage</small> (PC120) (PC208) (PC240) (PC277)
	Type II (T2) 	48 (48LC)	530 (5)	5000K *Cool white (5K)	480 (5)		Smooth Black (SBK)	
	Type III (T3) 	64 (64LC)			347 (8)		Smooth White (SWH)	
	Type IV (T4) 	80 (80LC)					Forest Green (FG)	
	Type V (T5) 	96 (96LC)					Silver Metallic (SL)	
	Type V-W (T5W) 						Graphite (GP)	
						Weathered Brown (WB)	0-10v Dimming Driver No Controls (DIM)	
						Soft Gold (SG)	Motion Sensor/Control Watt Stopper FSP-211 22' Distance @ 8' CLG HT Must specify Dimming Driver. (WSC)	
						Custom Color (CC)	Motion Sensor 20' Distance @ 8' CLG HT Must specify Dimming Driver. Can use with Wireless Control (MS)	
							Wireless Control (WC) Consult Factory	

For more detailed information on mounting, wiring or installation instructions, please consult factory. If poles are not ordered with fixtures, please specify mounting requirements. This document contains proprietary information of Visionaire Lighting, LLC. Any use of this information requires the written approval of Visionaire Lighting, LLC. In keeping with our TQM policy of continuous improvement, Visionaire reserves the right to change any specifications contained herein without prior notice.



EXHIBIT I

Housing

- The Premier II's housing is constructed from durable, corrosion-resistant, cast aluminum.
- The spun aluminum top cap is .080 gauge, and easily removable for service.
- Silicone gasketing is provided for complete weather and insect protection.

Thermal Management

- The Premier II provides excellent overall thermal management by maximizing the efficiency of the heat sink in the fixture. This enables the Premier II to withstand higher ambient temperatures and higher drive currents without degrading LED life.

Optical System

- The highest lumen output **Evo/C LEDr** are utilized. High-performance acrylic optics feature industry leading Type I, II, III, IV, V and V-W optical distributions. Non-fading acrylic optics are impact-resistant and rated to 94 percent transmittance.
- L_{70} life of our LEDs is rated over 150,000 hours (for 350 mA), based on second party, usable life calculations.
- The optical system qualifies as IES full cutoff to restrict light trespass, glare and light pollution for neighborhood-friendly lighting.
- CRI values are 70 for 4000K and 75 for 5000K.

Quali-Guard® Finish

- Fixture components are chemically pretreated through a multiple-stage washer, and finished with an electrostatically-applied, thermoset polyester powder coat textured paint with a 3 to 5 mils thickness. Finish is oven-baked at 400 °F for maximum adherence and finish hardness.
- Available in standard and custom colors.
- Finish is guaranteed for five (5) years.

Post Top Mount

- The Premier II's contractor-friendly mounting hardware slips over 4" O.D. x 4" tall tenon utilizing stainless steel hardware.
- Round Internal Tenon Adaptors (RITA) are recommended for a smooth-looking connection. Standard for 4" and 5" poles, custom size required for 3" poles.

Electrical Assembly

- The Premier II is supplied with a high-performance driver available in 350 and 530 mA. The driver is integrally-located in the housing, and is operational from 120 V through 480 V, 50 Hz to 60 Hz input. Power factor is 90%. • Rated for -40° F operation.
- 10 kV surge protector supplied as standard.

Warranty

- Five (5) year Limited Warranty on entire system, including finish. For full warranty information, please visit VisionaireLighting.com.

Options

- Button type photocell
 - 0-10v Dimming Driver
 - WattStopper FSP-211
 - Motion Sensor
 - Wireless Control
- Please consult factory for detailed custom options.

Listings

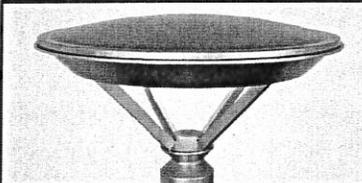
- Premier II is ETL listed, suitable for wet locations.
- DLC Listed (120v only)
- IP65 Rated.
- LM79 and LM80.
- Powder Coated Tough™.



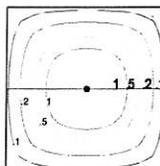
DesignLights Consortium (DLC) qualified Product. Some configurations of this product family may not DesignLights Consortium (DLC) listed, please refer to the DLC qualified products list to confirm listed configurations. <http://www.designlights.org/>

DesignLights Consortium (DLC) qualified Product. Some configurations of this product family may not DesignLights Consortium (DLC) listed, please refer to the DLC qualified products list to confirm listed configurations. <http://www.designlights.org/>

Alternate View



Isolux Curve



PREMIER II-1
TYPE V
LED OPTICS
48 LEDa @ 530 mA
Scale: 1 Square = 18 ft
Initial Light Loss Factor = 1.00
Mounting Height = 18 ft
Arrangement: Single

EPA Data

Fixture Only
PRE-2-L
●
1.2

LED 5500K*LUMEN DATA CHART FOR

# LED's	Output mA	Type 1	Type 2	Type 3	Type 4	Type 5	Type5-W	System Watts	L70 Hours
32	350	3141	2734	3438	2964	3787	3943	38	150,000
	530	4580	3206	4389	4249	5050	5127	51	100,000
48	350	4642	3877	4937	4574	5323	5519	56	150,000
	530	6785	4853	6584	6358	7541	7237	75	100,000
64	350	6120	5084	6458	6107	6960	7022	72	150,000
	530	9011	5611	8779	8469	10166	9352	102	100,000
80	350	7680	5714	8006	7656	8536	8637	89	150,000
	530	11270	6952	10980	10579	12645	11462	127	100,000
96	350	9176	7326	9572	9246	10079	10126	103	150,000
	530	13524	8130	13181	12688	15174	14572	144	100,000

Visit www.VisionaireLighting.com for up-to-the-minute chart information, including types not listed here.

*For 4000k multiply values by 0.90

19645 Rancho Way Rancho Dominguez, CA 90220
Tel: (310) 512-6480 Fax: (310) 512-6486
www.visionairelighting.com

C/CAG
CITY/COUNTY ASSOCIATION OF GOVERNMENTS
OF SAN MATEO COUNTY

*Atherton • Belmont • Brisbane • Burlingame • Colma • Daly City • East Palo Alto • Foster City • Half Moon Bay • Hillsborough • Menlo Park
Millbrae • Pacifica • Portola Valley • Redwood City • San Bruno • San Carlos • San Mateo • San Mateo County • South San Francisco • Woodside*

March 3, 2016

Paula Bradley
Community Development Department
City of San Bruno
567 El Camino Real
San Bruno, CA 94066

RE: SFO Comprehensive Airport Land Use Compatibility Plan Consistency Review – City of San Bruno, The Crossing Springhill Suites Hotel Project

Ms. Bradley:

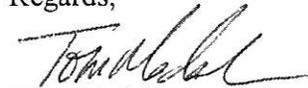
The C/CAG Board of Directors, acting as the Airport Land Use Commission, determined that The Crossing Springhill Suites Hotel Project is conditionally consistent with the applicable airport/land use policies and criteria contained in the Comprehensive Airport Land Use Compatibility Plan for the Environs of San Francisco International Airport (SFO ALUCP) at the February 11, 2016 C/CAG Board of Directors meeting.

The Crossing Springhill Suites Hotel Project would become fully consistent once the following two conditions have been met:

- Sound insulation to be provided to reduce interior noise levels from exterior sources to CNEL 45 dB or lower.
- Avigation easement to be granted to the City and County of San Francisco as operator of SFO. The avigation easement to be used in fulfilling this condition is provided in Appendix G of the SFO ALUCP.

Please let me know if you have any questions.

Regards,



Tom Madalena
Transportation Planning Manager

EXHIBIT J



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-11056-OE
Prior Study No.
2015-AWP-657-OE

Issued Date: 12/11/2015

Joe Vieira
OTO Development
100 Dunbar Street #420
Spartanburg, SC 29306

**** DETERMINATION OF NO HAZARD TO AIR NAVIGATION ****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Building SpringHill Suites
Location:	San Bruno, CA
Latitude:	37-38-00.52N NAD 83
Longitude:	122-25-12.64W
Heights:	68 feet site elevation (SE) 74 feet above ground level (AGL) 142 feet above mean sea level (AMSL)

This aeronautical study revealed that the structure does not exceed obstruction standards and would not be a hazard to air navigation provided the following condition(s), if any, is(are) met:

It is required that FAA Form 7460-2, Notice of Actual Construction or Alteration, be e-filed any time the project is abandoned or:

- At least 10 days prior to start of construction (7460-2, Part 1)
 Within 5 days after the construction reaches its greatest height (7460-2, Part 2)

Based on this evaluation, marking and lighting are not necessary for aviation safety. However, if marking/lighting are accomplished on a voluntary basis, we recommend it be installed and maintained in accordance with FAA Advisory circular 70/7460-1 L.

The structure considered under this study lies in proximity to an airport and occupants may be subjected to noise from aircraft operating to and from the airport.

This determination expires on 06/11/2017 unless:

- the construction is started (not necessarily completed) and FAA Form 7460-2, Notice of Actual Construction or Alteration, is received by this office.
- extended, revised, or terminated by the issuing office.
- the construction is subject to the licensing authority of the Federal Communications Commission (FCC) and an application for a construction permit has been filed, as required by the FCC, within

6 months of the date of this determination. In such case, the determination expires on the date prescribed by the FCC for completion of construction, or the date the FCC denies the application.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates , heights, frequency(ies) and power . Any changes in coordinates , heights, and frequencies or use of greater power will void this determination. Any future construction or alteration , including increase to heights, power, or the addition of other transmitters, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of the structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

If we can be of further assistance, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-11056-OE.

Signature Control No: 271370740-274845533

(DNE)

Karen McDonald
Specialist

Attachment(s)

Map(s)



Mail Processing Center
Federal Aviation Administration
Southwest Regional Office
Obstruction Evaluation Group
10101 Hillwood Parkway
Fort Worth, TX 76177

Aeronautical Study No.
2015-AWP-11058-OE

Issued Date: 11/30/2015

Joe Vieira
OTO Development
100 Dunbar Street #420
Spartanburg, SC 29306

****DETERMINATION OF NO HAZARD TO AIR NAVIGATION FOR TEMPORARY STRUCTURE****

The Federal Aviation Administration has conducted an aeronautical study under the provisions of 49 U.S.C., Section 44718 and if applicable Title 14 of the Code of Federal Regulations, part 77, concerning:

Structure:	Crane Portable Crane
Location:	San Bruno, CA
Latitude:	37-38-00.52N NAD 83
Longitude:	122-25-12.64W
Heights:	68 feet site elevation (SE) 112 feet above ground level (AGL) 180 feet above mean sea level (AMSL)

This aeronautical study revealed that the temporary structure does exceed obstruction standards but would not be a hazard to air navigation provided the following condition(s), if any, is (are) met:
As a condition to this Determination, the structure is marked/lighted in accordance with FAA Advisory circular 70/7460-1 K Change 2, Obstruction Marking and Lighting, flags/red lights - Chapters 3(Marked),4,5(Red),&12.

It is required that the manager of SAN FRANCISCO INTL, (650) 821-5000 be notified at least 3 business days prior to the temporary structure being erected and again when the structure is removed from the site.

It is required that the manager of SAN FRANCISCO ATCT @ 650 876-2883 X 109 be notified at least 3 business days prior to the temporary structure being erected and again when the structure is removed from the site. Additionally, please provide contact information for the onsite operator in the event that Air Traffic Control requires the temporary structure to be lowered immediately.

Any height exceeding 112 feet above ground level (180 feet above mean sea level), will result in a substantial adverse effect and would warrant a Determination of Hazard to Air Navigation.

This determination expires on 05/30/2017 unless extended, revised, or terminated by the issuing office.

NOTE: REQUEST FOR EXTENSION OF THE EFFECTIVE PERIOD OF THIS DETERMINATION MUST BE E-FILED AT LEAST 15 DAYS PRIOR TO THE EXPIRATION DATE. AFTER RE-EVALUATION OF CURRENT OPERATIONS IN THE AREA OF THE STRUCTURE TO DETERMINE THAT NO SIGNIFICANT AERONAUTICAL CHANGES HAVE OCCURRED, YOUR DETERMINATION MAY BE ELIGIBLE FOR ONE EXTENSION OF THE EFFECTIVE PERIOD.

This determination is based, in part, on the foregoing description which includes specific coordinates and heights. Any changes in coordinates and/or heights will void this determination. Any future construction or alteration, including increase to heights, requires separate notice to the FAA.

This determination does include temporary construction equipment such as cranes, derricks, etc., which may be used during actual construction of a structure. However, this equipment shall not exceed the overall heights as indicated above. Equipment which has a height greater than the studied structure requires separate notice to the FAA.

This determination concerns the effect of this temporary structure on the safe and efficient use of navigable airspace by aircraft and does not relieve the sponsor of compliance responsibilities relating to any law, ordinance, or regulation of any Federal, State, or local government body.

Any failure or malfunction that lasts more than thirty (30) minutes and affects a top light or flashing obstruction light, regardless of its position, should be reported immediately to (877) 487-6867 so a Notice to Airmen (NOTAM) can be issued. As soon as the normal operation is restored, notify the same number.

A copy of this determination will be forwarded to the Federal Aviation Administration Flight Procedures Office if the structure is subject to the issuance of a Notice To Airman (NOTAM).

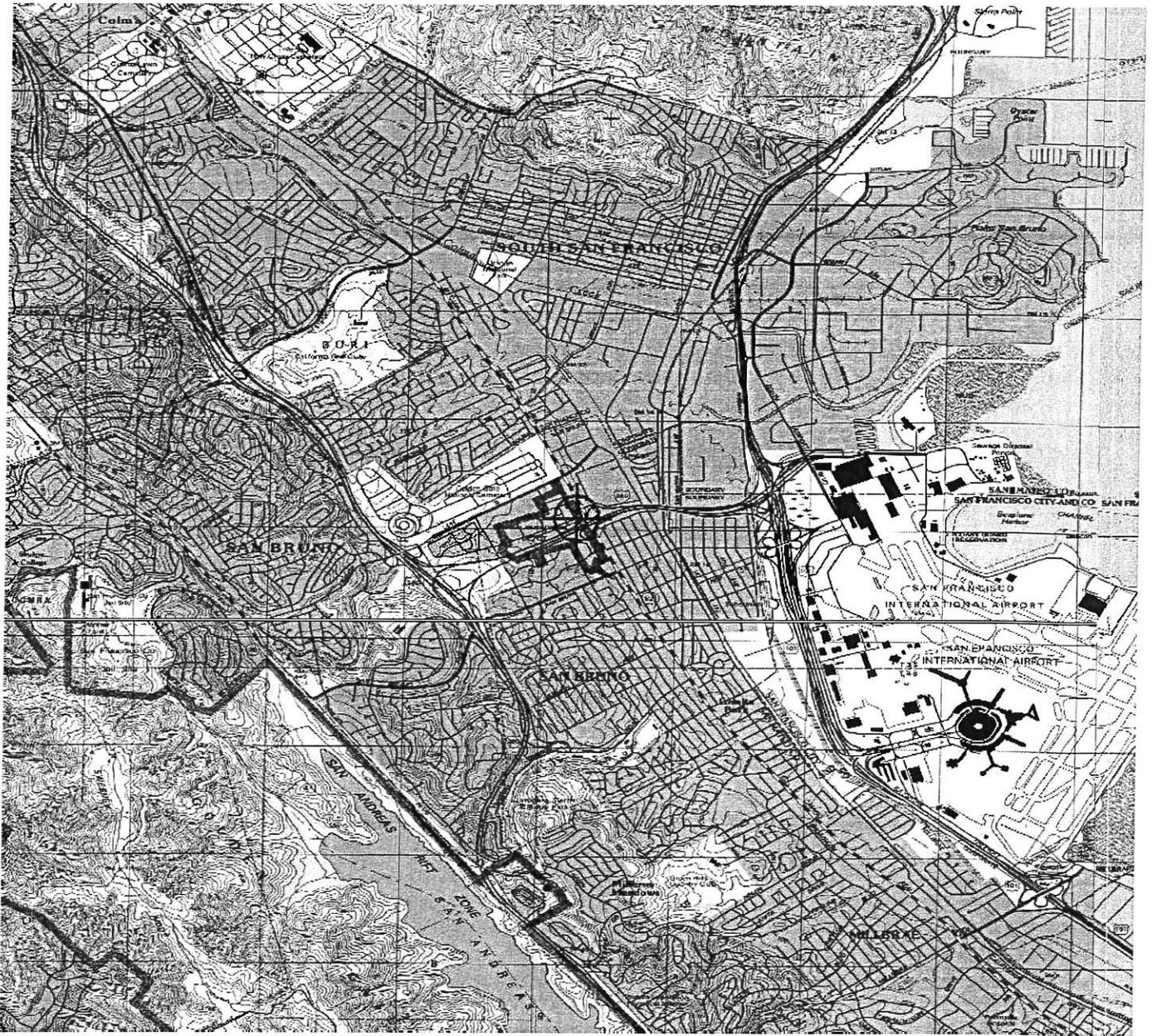
If you have any questions, please contact our office at (310) 725-6557. On any future correspondence concerning this matter, please refer to Aeronautical Study Number 2015-AWP-11058-OE

Signature Control No: 271370874-273765855

(TMP)

Karen McDonald
Specialist

Attachment(s)
Map(s)





SANDIS

CIVIL ENGINEERS
SURVEYORS
PLANNERS

Transportation Demand Management (TDM) Plan The Springhill Suites Marriott, San Bruno, CA



Prepared For: OTO Development
16147 Camellia Terrace
Los Gatos, CA 95032

Prepared By: SANDIS
636 9th Street
Oakland, CA 94607
(510) 873-8866
March 12, 2015
Revised February 24, 2016

EXHIBIT L

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Section 1 – Introduction and Project Description

This report introduces the Transportation Demand Management (TDM) plan for the commercial hotel development located at intersection of I-380 and El Camino Real. A vicinity map can be found on Figure 1.

The proposed project site consists of a 1.65 Acre (72,244 Square Feet) vacant lot. The proposed development will include a five story building purposed for hotel use with 97,474 of Gross Square Feet (GSF) of floor area with 152 total available rooms. A site plan can be found on Figure 2.

Trip Generation Estimates

Vehicle trip generation estimates are calculated to provide the number of anticipated vehicle trips (employees, deliveries, visitors, etc.) that are produced by the project site's specific land use during a specified time period. The number of vehicle trips generated by the project site was estimated using trip generation rates for a Hotel (ITE Land Use 310 – Hotel) published in the Institute of Transportation Engineers' (ITE), *Trip Generation, 9th Edition (2012)*.

The estimated number of vehicle trips for the project site is forecast to be 81 AM peak hour trips and 91 PM peak hour trips. The complete details of the trip generation can be found in Table 1.

Trip Reduction Goals

The goal of this TDM plan is to identify trip reduction methods to be implemented in order to reduce the number of AM and PM peak hour single occupant vehicle (SOV) trips that are generated by the project site. This TDM plan is estimated to reduce the number of new SOV trips to 52 AM and 62 PM peak hour trips.

Table 1

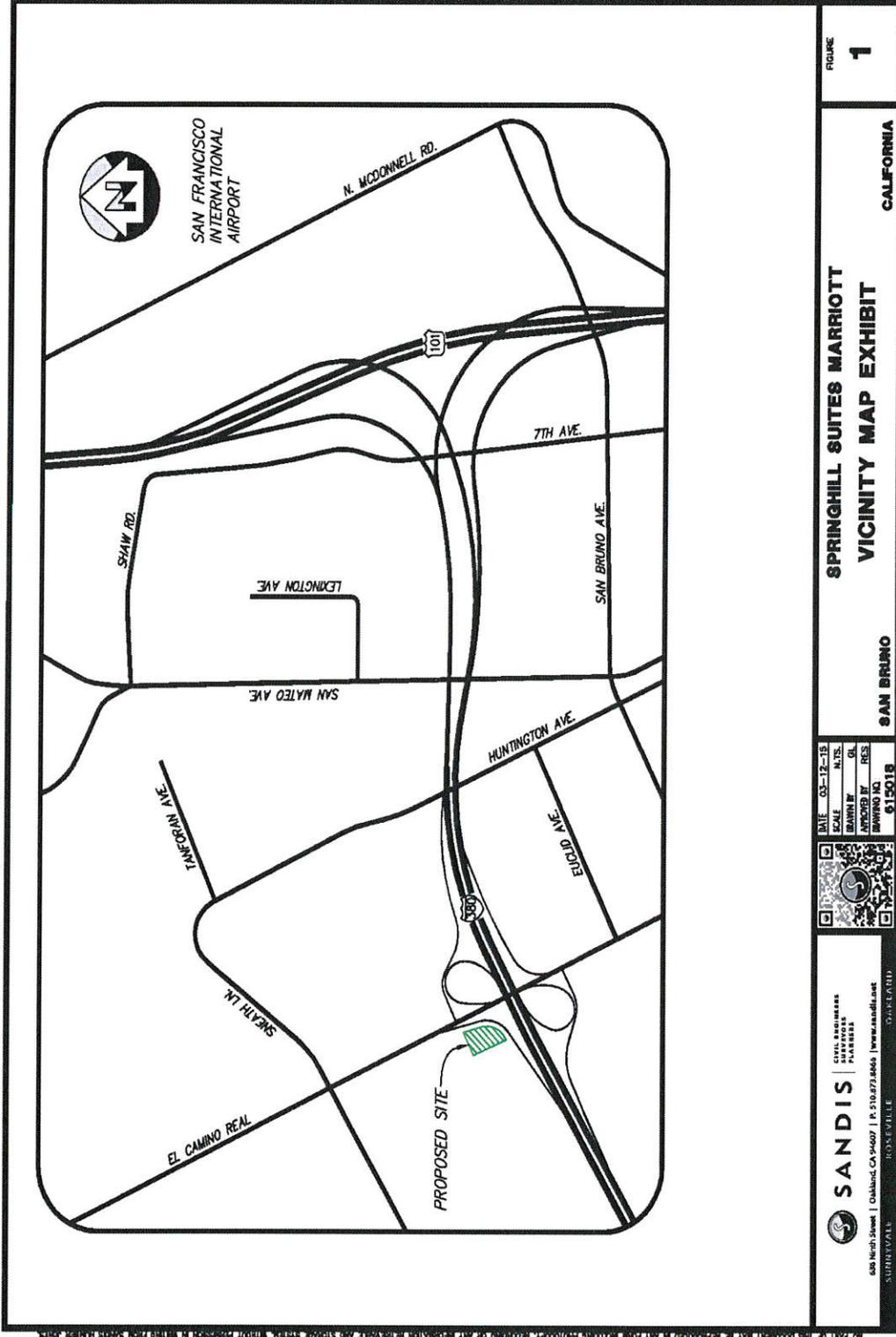
Trip Generation												
	ITE Code	Rooms	Daily		AM Peak Hour			PM Peak Hour				
			Rate	Trips	Rate	In	Out	Total	Rate	In	Out	Total
Proposed Project	310	152.0	8.17	1,242	0.53	48	33	81	0.60	47	44	91
Vehicle Trips (A)												
Total Trips after TDM Reduction of 29 Peak Hour Trips						30	21	52		32	30	62
Anticipated Project Trips												
						30	21	52		32	30	62

Notes:

1. Rate per room
2. From ITE trip generation equations (ITE 9th Edition):
 - (A) ITE Code 310 Hotel - Rooms
 - AM Peak Hour Rate: 0.53 Trips/Room
 - PM Peak Hour Rate: 0.60 Trips/Room

Source: *Trip generation Manual (9th Edition), ITE, 2012*

Figure 1



Transportation Demand Management (TDM) Plan The Springhill Suites Marriott, San Bruno, CA



Prepared For: OTO Development
16147 Camellia Terrace
Los Gatos, CA 95032

Prepared By: SANDIS
636 9th Street
Oakland, CA 94607
(510) 873-8866
March 12, 2015
Revised February 24, 2016

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Trip Reduction Goals.....	2
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Section 1 – Introduction and Project Description

This report introduces the Transportation Demand Management (TDM) plan for the commercial hotel development located at intersection of I-380 and El Camino Real. A vicinity map can be found on Figure 1.

The proposed project site consists of a 1.65 Acre (72,244 Square Feet) vacant lot. The proposed development will include a five story building purposed for hotel use with 97,474 of Gross Square Feet (GSF) of floor area with 152 total available rooms. A site plan can be found on Figure 2.

Trip Generation Estimates

Vehicle trip generation estimates are calculated to provide the number of anticipated vehicle trips (employees, deliveries, visitors, etc.) that are produced by the project site's specific land use during a specified time period. The number of vehicle trips generated by the project site was estimated using trip generation rates for a Hotel (ITE Land Use 310 – Hotel) published in the Institute of Transportation Engineers' (ITE), *Trip Generation, 9th Edition (2012)*.

The estimated number of vehicle trips for the project site is forecast to be 81 AM peak hour trips and 91 PM peak hour trips. The complete details of the trip generation can be found in Table 1.

Trip Reduction Goals

The goal of this TDM plan is to identify trip reduction methods to be implemented in order to reduce the number of AM and PM peak hour single occupant vehicle (SOV) trips that are generated by the project site. This TDM plan is estimated to reduce the number of new SOV trips to 52 AM and 62 PM peak hour trips.

Table 1

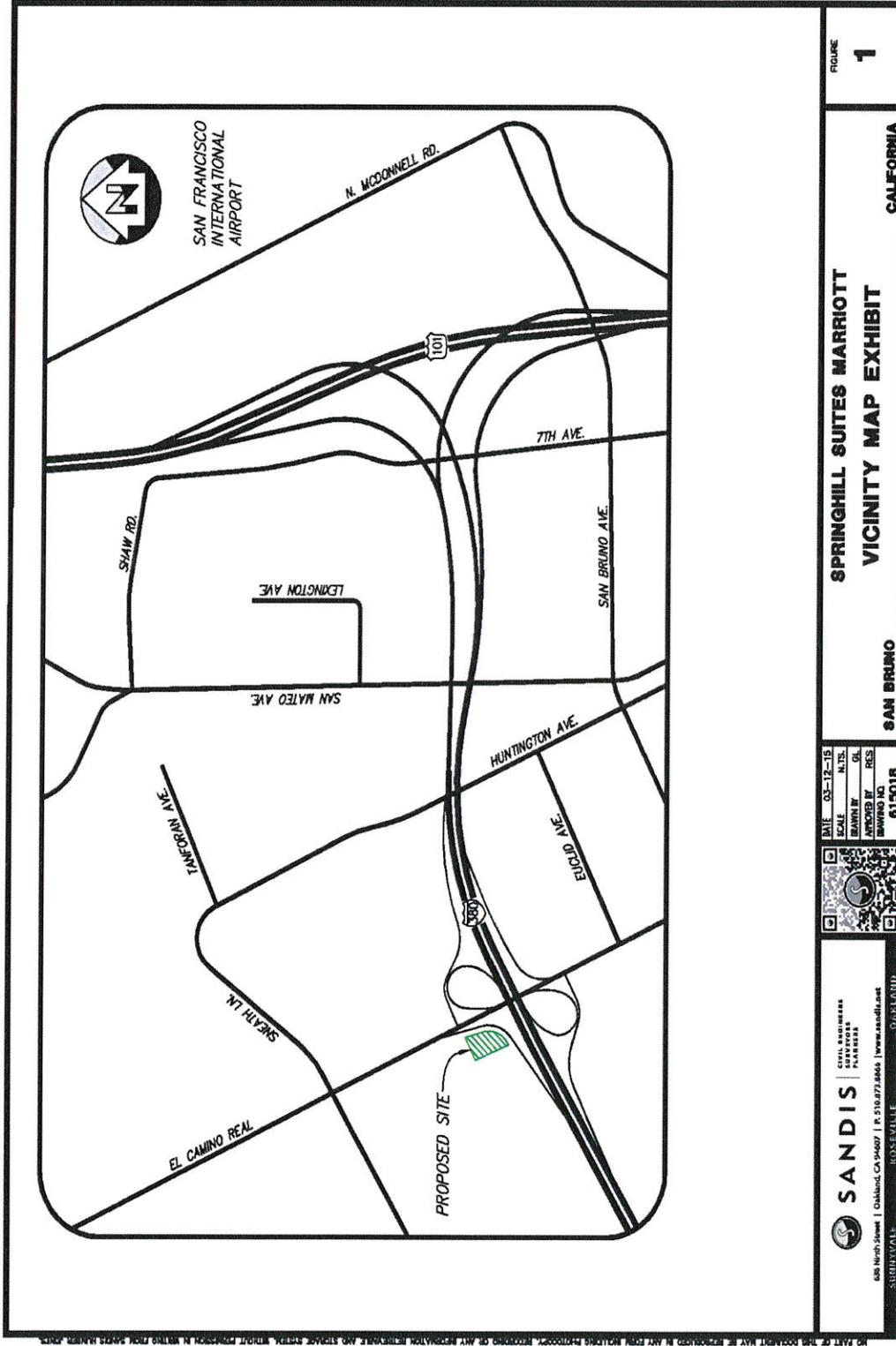
Trip Generation											
ITE Code	Rooms	Daily		AM Peak Hour			PM Peak Hour				
		Rate	Trips	Rate	In	Out	Total	Rate	In	Out	Total
Proposed Project	152.0	8.17	1,242	0.53	48	33	81	0.60	47	44	91
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Notes:

1. Rate per room
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Source: Trip generation Manual (9th Edition), ITE, 2012

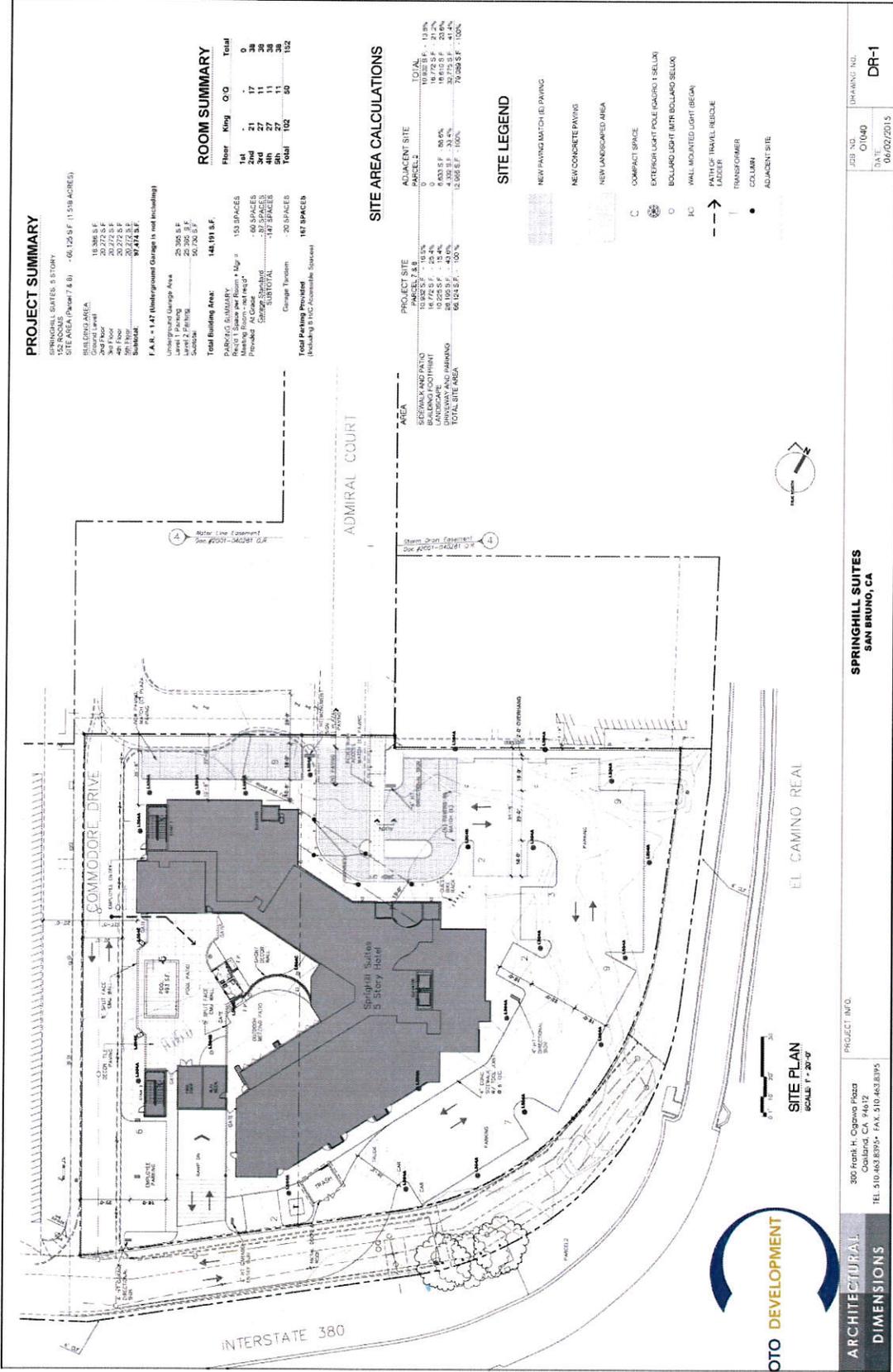
Figure 1



 SANDIS CIVIL ENGINEERS SURVEYORS PLANNERS 630 North Street Oakland, CA 94607 P. 510.877.8866 www.sandis.com		SUBDIVISION: ROSEVILLE COUNTY: QUAKELEND	
DATE: 03-12-18 SCALE: N.T.S. DRAWN BY: [Redacted] CHECKED BY: [Redacted]		PROJECT NO.: 15018 SHEET NO.: 1	
SPRINGHILL SUITES MARRIOTT VICINITY MAP EXHIBIT		SAN BRUNO CALIFORNIA	
Copyright © 2018 by Sandis			

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Figure 2



Section 2 – Existing Transportation Facilities and Services

The following transportation facilities provide alternate modes of transportation to the conventional single-occupant vehicle (SOV):

- Commuter Rail
- BART
- SamTrans
- Pedestrian Facilities

This section discusses the existing and planned facilities located in proximity to The Springhill Suites Marriott. Schedules for transit systems in the area are subject to change and can be found at: <http://www.samtrans.com/schedulesandmaps.html>

The San Francisco International Airport is located approximately 1.8 miles from the project site. It is estimated that a high number of users of the project site will originate from the SFO airport. Many of these users will rely on forms of transportation other than SOV. There are currently a large number of options for commuters to utilize other than signal occupant vehicles to travel to and from the project site and SFO. These include, BART, Shuttle Services, SamTrans, and for hire vehicles.

A local transit map of the surrounding networks can be found on Figure 3.

Commuter Rail – CALTRAIN

Caltrain provides intercity commuter rail service between Santa Clara County, San Mateo County and San Francisco County. Currently, there is one Caltrain Station located in proximity to The Springhill Suites Marriott—the San Bruno Station.

The San Bruno Station is located .7 miles (a 16 minute walk) away from the project site. The station is equipped with 7 bike racks and 40 bike lockers which are available through a rental agreement. The .7 mile distance between the station and the project site is an acceptable distance for biking which allows this to be a viable option getting to and from the project site and the station.

The San Bruno Station schedule is subject to change and can be found at: <http://www.caltrain.com/stations/sanbrunostation.html>

BART

BART provides intercity commuter rail service between Santa Clara County, San Francisco County, San Mateo County, Alameda County, and Contra Costa County. Currently, there is one BART Station located in proximity to The Springhill Suites Marriott—the San Bruno Station.

The San Bruno Bart station provides a direct link to the SFO airport. Passengers can travel directly to and from the San Bruno station and SFO.

The San Bruno Station is located .6 miles (a 12 minute walk) away from the project site. The station is equipped with bike racks and 30 bike lockers which are available through a rental agreement. The .6 mile distance between the station and the project site is an acceptable distance for biking which allows this to be a viable option getting to and from the project site and the station.

The San Bruno Station schedule is subject to change and can be found at:
<https://www.bart.gov/stations/sbrn/schedule>

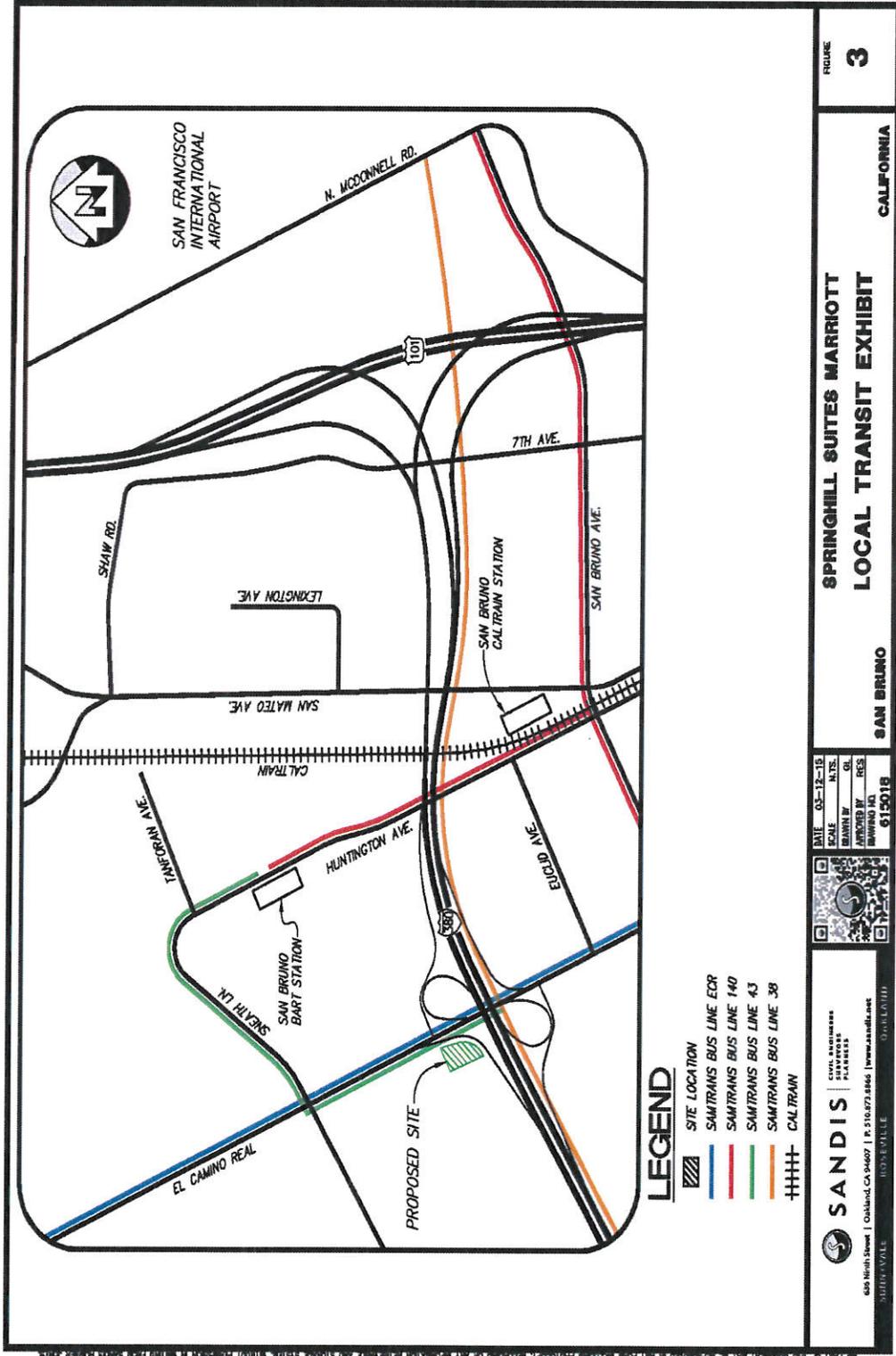
Samtrans Bus Routes

The project site area is serviced by San Mateo County Transit District (SamTrans) which provides access through San Mateo County and to San Francisco County as well as Santa Clara county. Detailed information on services and routes can be found at <http://www.samtrans.com/schedulesandmaps.html>

Pedestrian Facilities

The nearby pedestrian facilities are comprised of sidewalks, warning devices and signals, crosswalks, and trails. Currently, there are extensive pedestrian facility networks located on El Camino Real, Huntington Avenue, Sneath Lane, San Bruno Avenue West and Commodore Drive. These existing facilities provide access to local transit facilities as well as local commercial areas.

Figure 3



Section 3 – Potential TDM Design Measures and Programs

The San Mateo County Association of Governments (C/CAG) has developed a list of TDM design measures that are defined in the "Guidelines for Implementing the Land Use Component of the Congestion Management Program". Table 2 provides a summary of potential measures and programs as defined in the TDM Guidelines. The TDM plan for The Springhill Suites Marriott includes applicable measures from the above mentioned list, as well as other current state of the practice approaches. Section 4 of this TDM lists the specific measures that will be implemented at The Springhill Suites Marriott.

Table 2

C/CAG - TDM Measures	
TDM Measure	Summary of Trip Credit
Dedicated shuttle service during the peak period to a rail station or an urban residential area.	One peak hour trip will be credited for each peak-hour round trip seat on the shuttle. Increases to two trips if a Guaranteed Ride Home Program is also in place.
Charging employees for parking.	One peak hour trip will be credited for each parking spot charged out at \$20 per month for one year.
Subsidizing transit tickets for employees.	One peak hour trip will be credited for each transit pass that is subsidized at least \$20 per month for one year.
Parking cash out program.	One peak hour trip will be credited for each parking spot where the employee is offered a cash payment in return for not using parking at the employment site.
Compressed workweek program.	One peak hour trip will be credited for every 5 employees that are offered the opportunity to work four compressed days per week.
Assistance to employees so they can live close to work.	If an employer develops and offers a program to help employees find acceptable residences within five miles of the employment site, a credit of one trip will be given for each slot in the program.
Alliance's guaranteed ride home program	One peak hour trip will be credited for every 2 slots purchased in the program.
Work with the Alliance to develop or implement a Transportation Action Plan	Five peak hour trips will be credited.
Infill development	Two percent of all peak hour trips will be credited for each infill development.
Bicycle lockers and racks	One peak hour trip will be credited for every 3 new bike lockers/racks, installed and maintained.
Showers and changing rooms	Two peak hour trips will be credited for each new combination shower and changing room installed.

C/CAG - TDM Design Measures	
TDM Design Measure	Summary of Trip Credit
Ramp metering	Three hundred peak hour trips will be credited if the local jurisdiction in cooperation with CalTrans, installs and turns on ramp metering lights during the peak hours at the highway entrance ramp closest to the development.
High-band width connections in employees homes to the Internet to facilitate home telecommuting.	One peak hour trip will be credited for each connection installed.
Video conferencing centers that are available for use by the tenants of the facility.	Twenty peak hour trips will be credited for a center installed at the facility.
Combine any ten of these elements	Five peak hour trips will be credited.

Section 4 - Designated TDM Measures

TDM Measures

The following includes measures that are either incorporated into the project design or will be implemented by the tenants following occupation of the building.

1) Shower and Changing Facilities

The building will have shower and changing facilities to accommodate bicyclists. These facilities will help promote bicycling as an alternative commute option for interested users. Adding this facility will result in a reduction of two peak hour trips.

2) On Site Amenities

The building will provide on-site amenities such recreational facilities, a market place, a meeting space, and a lounge with food service. These amenities will help reduce the need of a user to leave the facility and will help to reduce peak hour SOV trips. Providing these amenities on site is estimated to result in a reduction of five peak hour trips.

3) Dedicated Shuttle Service

Due to the land use of this project, a large number of trips generated will result from users traveling to and from the San Francisco Airport International. The tenants of the project site will provide a free shuttle service that will transport users to and from SFO as well as other local destinations. This service will greatly reduce the amount of SOV trips and is estimated to result in a reduction of 15 peak hour trips.

4) Bicycle lockers and Racks

The project site will provide bicycle racks for commuters to use. This results in a reduction of two peak hour trips.

5) Transportation Informational Kiosk

The project site will provide a Transportation Informational Kiosk that will be available to interested commuters. The Kiosk will provide informational pamphlets and other transportation resources that highlight non SOV transportation options. Transportation informational Kiosks are industry proven to reduce peak hour trips results in a reduction of five peak hour trips.

Section 5 – Evaluation

The purpose of this TDM plan is to reduce the overall SOV trips and decrease the traffic impact resulting from the proposed development. TDM measure should be evaluated on an annual basis to determine the effectiveness of the implemented measures.

Modifications to the TDM Program

The TDM plan should be considered a living document with the primary goal of reducing SOV trips. As techniques and programs change and/or advance, the TDM plan should be revised accordingly.

Section 6 – Summary

This TDM plan has been produced for the use at The Springhill Suites Marriott. Several design and program measures were presented to facilitate the use of alternate modes of transportation, including transit, carpooling, bicycling, and walking.

This TDM plan is anticipated to achieve a reduction of 29 peak hour trips; however, actual trip volumes shall be monitored and TDM measures may be adjusted as a result.



SPRINGHILL SUITES
 EL CAMINO REAL AND INTERSTATE 380
 SAN BRUNO, CA 94066

DEVELOPER:
Palmetto Hospitality of San Bruno, L.L.C.

124 Johnston Court
 Folsom, CA 95630

CONTACT: Steve Pieters, Real Estate Manager, Tel. 916.983.2533

ARCHITECT:
ARCHITECTURAL DIMENSIONS

300 FRANK H. OGAWA PLAZA, SUITE 375
 OAKLAND, CALIFORNIA 94612
 510.463.8300, FAX: 510.463.8395

SUBMITTED TO:
 CITY OF SAN BRUNO
 FOR:
 Planned Development Permit Submittal

Project Site



Vicinity Map

DRAWING INDEX

DR-0	Cover Sheet
DR-1	Site Plan
DR-2	Code Data
DR-2.1	Floor Plans
DR-2.2	Floor Plan, Roof Plan
DR-3	Parking Garage Plans
DR-4	Building Sections
DR-5	North and North-East Elevations
DR-6	South and North-West Elevations
DR-6A	West (REAR) Elevations
DR-6B	South and North Courtyard Elevations
DR-7	Perspective Views
DR-8	Enlarged Elevation and Materials
DR-9	Building Height Study
C-1	Topographic Survey
C-2	Grading Plan
C-3	Utility Plan
C-4	Stormwater Management Plan
C-5	Grading Sections
C-6	Water Pollution Control Plan
C-7	Erosion Control Details
C-8	San Mateo County Best Management Practices
C-9	Fire Access Plan
1 of 1	Driveway Sight Distance Exhibit
L-1	ALTA Survey
L-2	Planting Plan
L-3	Hydrozone Plan
PM-1	Site Sections Lighting / Photometric Plan



ARCHITECTURAL DIMENSIONS

300 Frank H. Ogawa Plaza
 Oakland, CA 94612
 TEL. 510.463.8395 • FAX. 510.463.8395

PROJECT INFO.

SPRINGHILL SUITES
 EL CAMINO REAL @ HWY 380
 SAN BRUNO, CA 94066

JOB NO. CT 040
 DATE: 01.04.2016
 DRAWING NO. **DR-0**

PROJECT SUMMARY

SPRINGHILL SUITES, 5 STORY.
 SITE AREA (Parcel 7 & 8) - 661,125 S.F. (15.18 ACRES)
 Portion of Parcel 2 - 12,865 S.F.
 BUILDING AREA TOTAL - 79,089 S.F. (1.815 ACRES)
 Ground Level: 16,366 S.F.
 2nd Floor: 20,740 S.F.
 3rd Floor: 20,633 S.F.
 4th Floor: 20,633 S.F.
 5th Floor: 20,633 S.F.
 Subtotal: 99,022 S.F.
 F.A.R. = 1.50 (Underground Garage is not including)

Underground Garage Area:
 Level 1 Parking: 25,366 S.F.
 Level 2 Parking: 14,723 S.F.
 Subtotal: 50,730 S.F.
 Total Building Area: 149,752 S.F.

ROOM SUMMARY

Floor	King	Q/O	Total
1st	0	0	0
2nd	26	12	38
3rd	26	12	38
4th	26	12	38
5th	26	12	38
Total	104	48	152

PARKING SUMMARY
 Req'd 1 Space per Room + Mgr = 153 SPACES
 Meeting Room - not req'd = 40 SPACES
 Provided Garage Standard = 147 SPACES
 SUBTOTAL = 147 SPACES
 Garage Tandem = 20 SPACES
 Total Parking Provided: 167 SPACES
 (Including 6 HC Accessible Spaces)

Valet Parking
 Garage Level P1: 17 SPACES
 Garage Level P2: 14 SPACES
 Garage Level P3: 45 SPACES
 TOTAL: 76 SPACES

SITE AREA CALCULATIONS

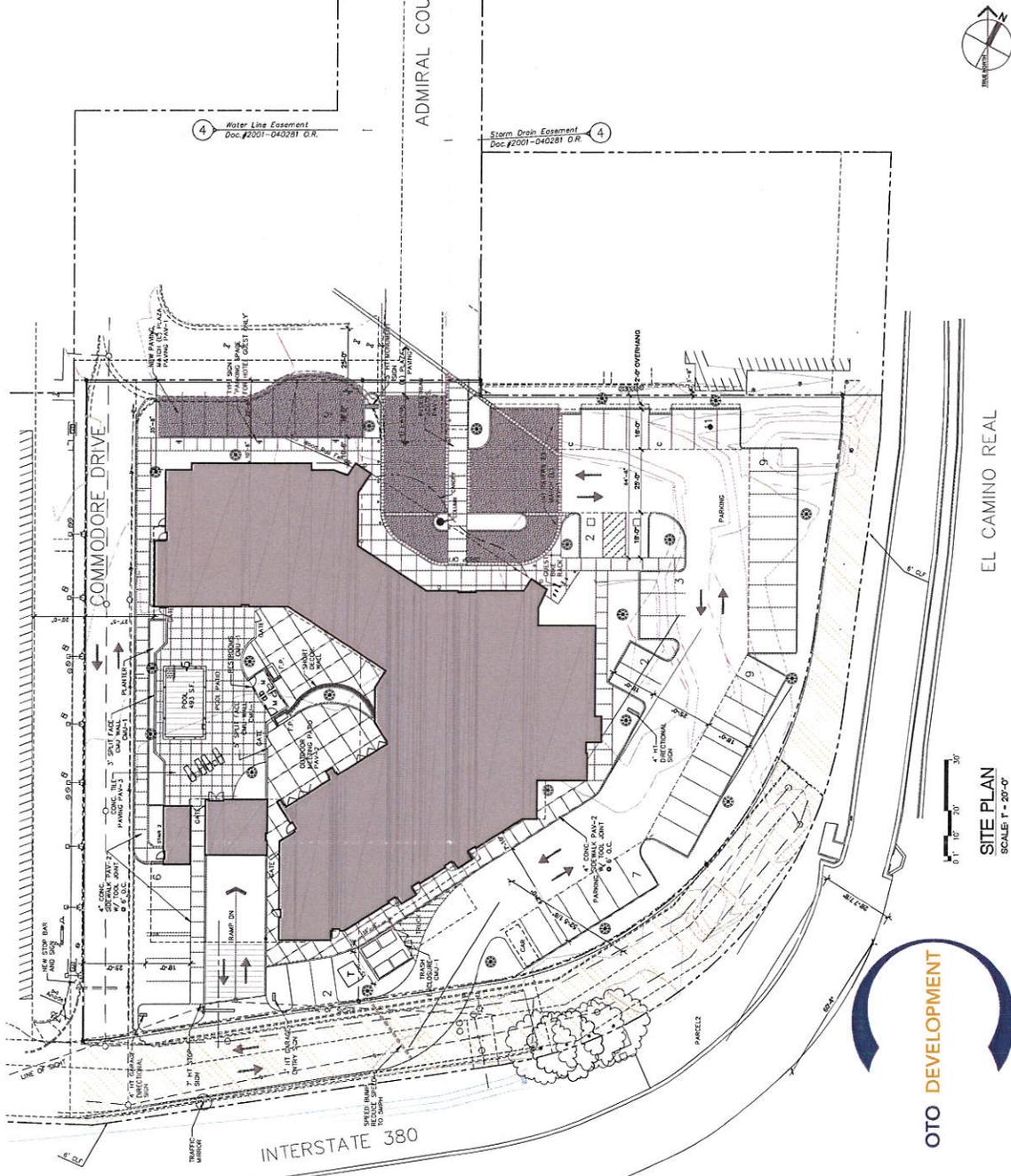
AREA	PROJECT SITE	ADJACENT SITE	TOTAL
SIDEWALK AND RATIO	18,773 S.F. - 25.4%	0	18,773 S.F. - 25.4%
BUILDING FOOTPRINT	18,773 S.F. - 25.4%	0	18,773 S.F. - 25.4%
LANDSCAPE	8,633 S.F. - 11.4%	0	8,633 S.F. - 11.4%
DRIVEWAY AND PARKING	28,125 S.F. - 37.6%	0	28,125 S.F. - 37.6%
TOTAL SITE AREA	86,124 S.F. - 100%	0	86,124 S.F. - 100%

SITE LEGEND

PAVING AND FENCE MATERIALS SEE SHEET DR-4

- NEW PAVING MATCH (E) PAVING PAV-1
- NEW CONCRETE PAVING PAV-2
- NEW CONCRETE PAVING PAV-3
- NEW LANDSCAPED AREA
- COMPACT SPACE
- EXTERIOR LIGHT POLE (QADRO 1 SELUX)
- BOLLARD LIGHT (MTR BOLLARD SELUX)
- WALL MOUNTED LIGHT (BEGA)
- PATH OF TRAVEL RESCUE LADDER
- TRANSFORMER
- COLUMN
- ADJACENT SITE PORTION OF PARCEL 2

BIKE PARKING SUMMARY:
 Short Term Parking (Guest Parking) - 5 Stalls
 Long Term Parking - 10 Stalls



SCALE 1" = 20'-0"

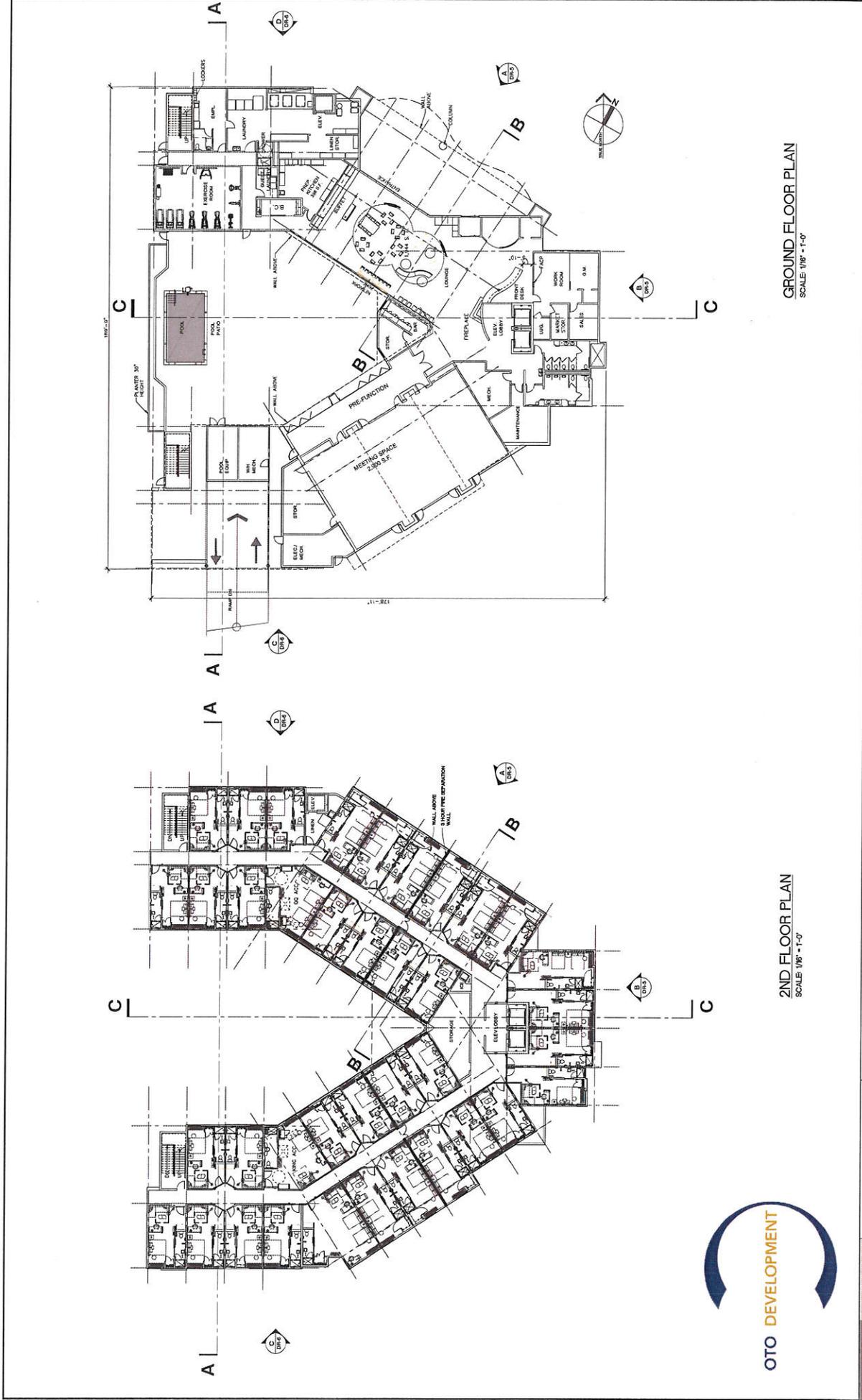
OTO DEVELOPMENT

SPRINGHILL SUITES
 EL CAMINO REAL @ HWY 380
 SAN BRUNO, CA 94066

PROJECT INFO:
 300 Frank H. Ogawa Plaza
 Oakland, CA 94612
 TEL 510.463.8395 - FAX 510.463.8395

ARCHITECTURAL DIMENSIONS

JOB NO. 01040
 DATE: 12.02.2015
 DRAWING NO. DR-1

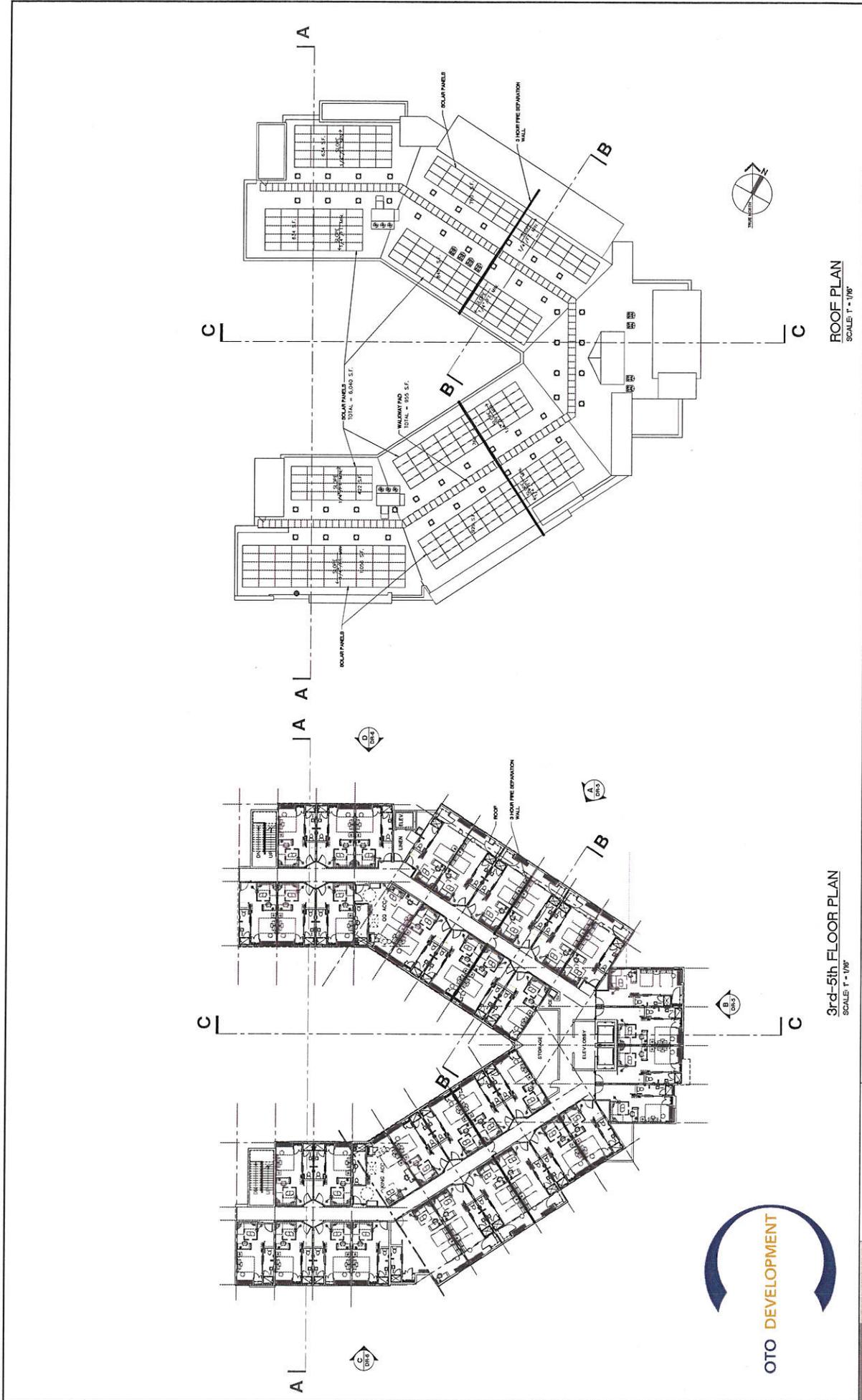


GROUND FLOOR PLAN
SCALE: 1/8" = 1'-0"

2ND FLOOR PLAN
SCALE: 1/8" = 1'-0"



ARCHITECTURAL DIMENSIONS	PROJECT INFO: 300 Frank H. Ogawa Plaza Oakland, CA 94612 TEL. 510.463.8975 • FAX. 510.463.8975	JOB NO. 01 040 DATE: 12.02.2015	DRAWING NO. DR-2.1
	SPRINGHILL SUITES EL CAMINO REAL @ HWY 380 SAN BRUNO, CA 94066		

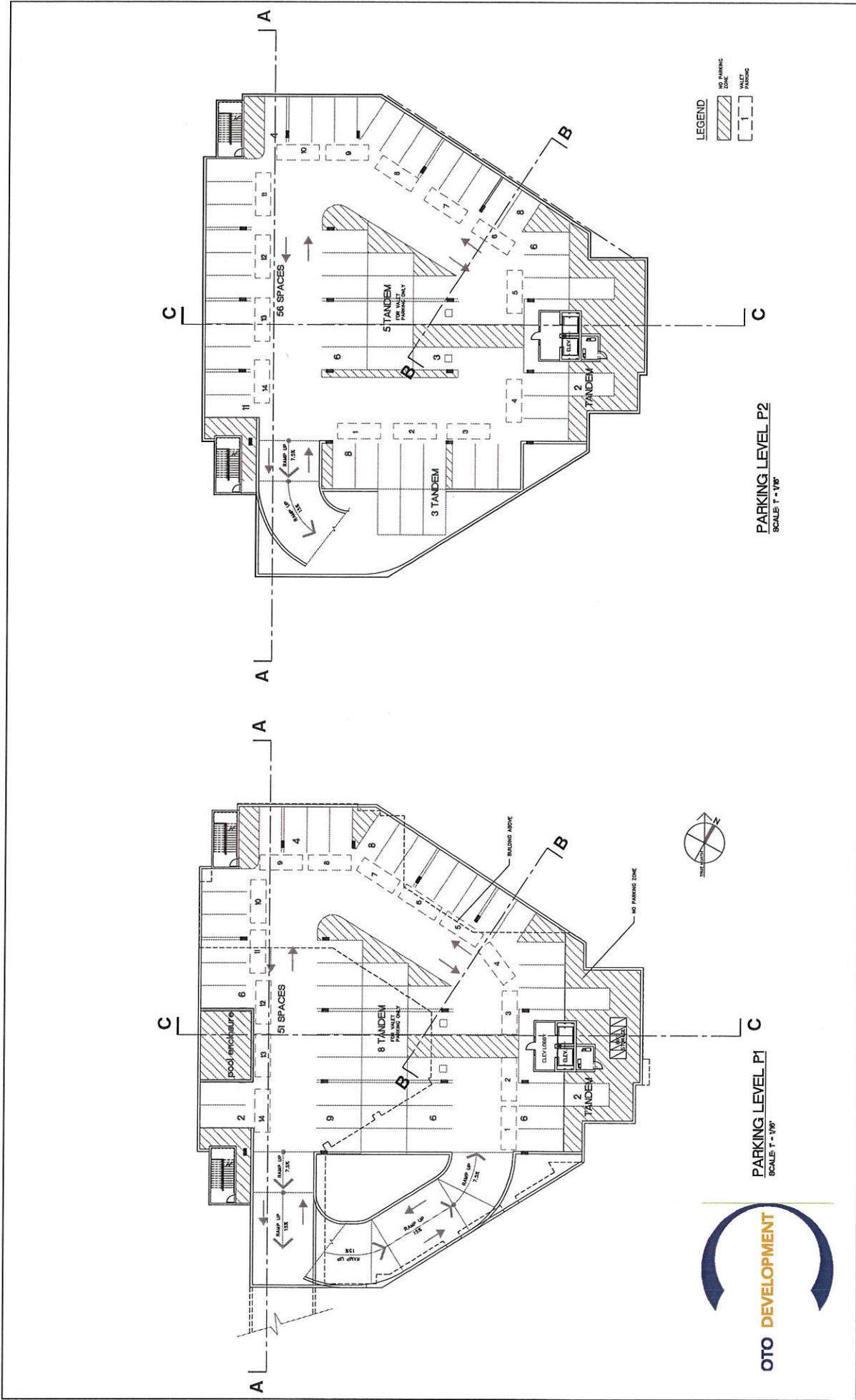


ROOF PLAN
SCALE: 1" = 1/8"

3rd-5th FLOOR PLAN
SCALE: 1" = 1/8"



ARCHITECTURAL DIMENSIONS	PROJECT INFO. 300 Frank H. Ogawa Plaza Oakland, CA 94612 TEL. 510.463.8995 • FAX. 510.463.8995		JOB NO. OT 040	DRAWING NO. DR-2.2
	SPRINGHILL SUITES EL CAMINO REAL @ HWY 380 SAN BRUNO, CA 94066		DATE 12.02.2015	



ARCHITECTURAL DIMENSIONS	PROJECT INFO.		DRAWING NO. DR-3
	300 Frank H. Ogawa Plaza Oakland, CA 94612 TEL. 510.463.8395 • FAX 510.463.8395		
SPRINGHILL SUITES EL CAMINO REAL @ HWY 380 SAN BRUNO, CA 94066		JOB NO. OT 040	DATE 08.10.2015

General Exterior Elevation Notes

- Colors indicated on this drawing are approximate and will vary depending on printer model or digital color. Refer to Colors and Materials Boards for true representation of all proposed finishes.
- All landscaping indicated on this drawing is diagrammatic and intended only to convey a sense of general landscaped areas. Refer to actual Landscape Plan for all proposed landscaping.

Material/Finish Legend

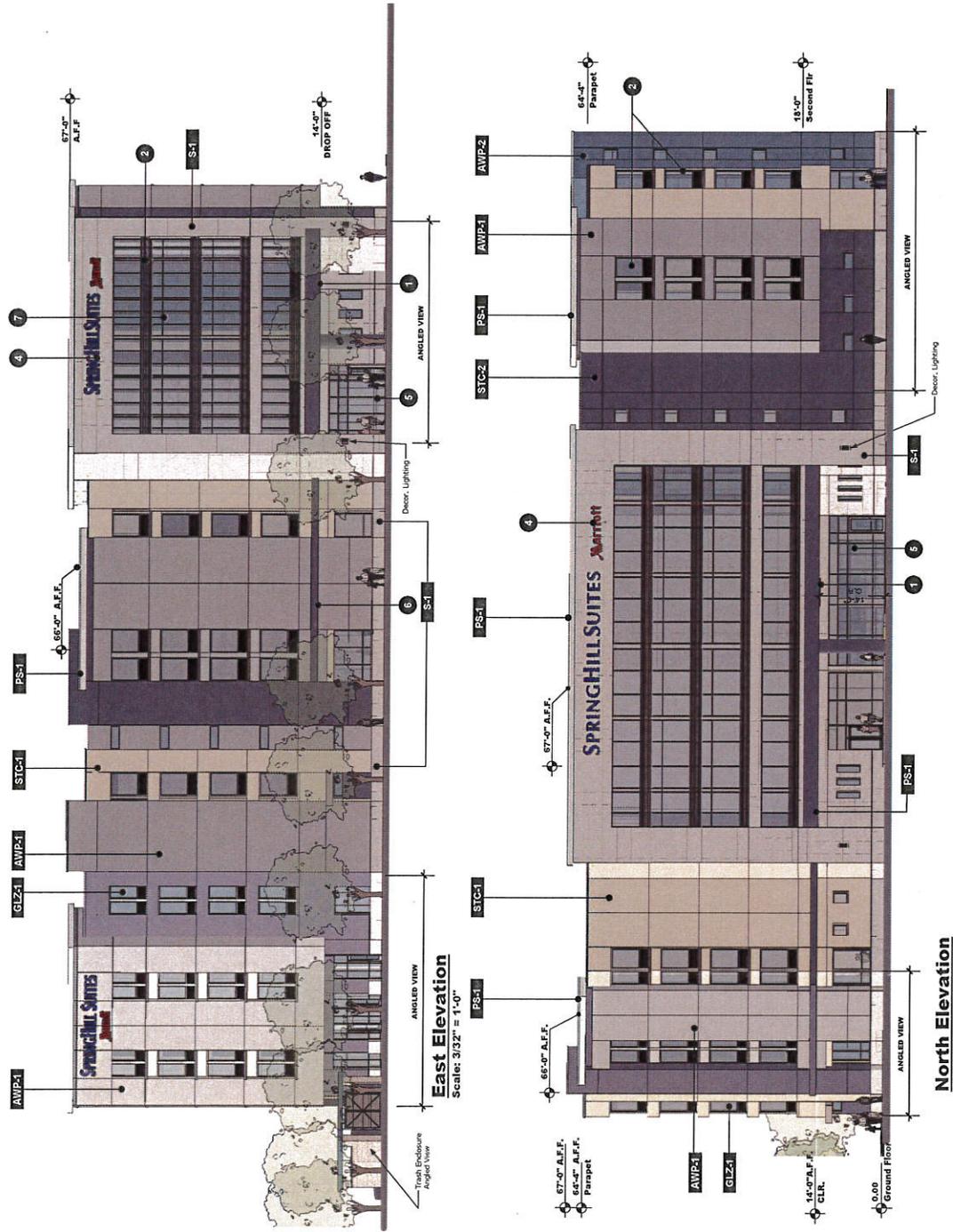
Refer to Colors and Materials Boards for true representation of all proposed finishes.

- STC-1** Painted Smooth Stucco
- STC-2** Painted Smooth Stucco
- S-1** Stone Veneer
- GLZ-1** Glazing (Vision Glass)
- PS-1** Painted Metal
- AWP-1** Metal Wall Panel System (Grey)
- AWP-2** Metal Wall Panel System (Blue)
- AWP-3** Framed Perforated Panel
- CMU-1** Splitface CMU - Basaltite

Keynotes

Note: Not all keynotes listed apply to this particular sheet.

- 1 Porte Cochere.
- 2 Packaged Terminal Air Conditioning (PTAC) unit integral to window system. Color and finish to match aluminum windows.
- 3 Metal Sunscreen Awning.
- 4 Internally Illuminated Channel Letter Signage.
- 5 Aluminum Storefront System.
- 6 Metal Canopy.
- 7 Aluminum Window Wall System



East Elevation
Scale: 3/32" = 1'-0"

North Elevation
Scale: 3/32" = 1'-0"

SPRINGHILL SUITES
SAN BRUNO, California

PROJECT INFO.
300 Frank H. Ogawa Plaza, Suite 375
Oakland, CA 94612
TEL. 510.463.8300 | FAX. 510.463.8375

ARCHITECTURAL
DIMENSIONS

JOB NO. **OTO40**
DATE **01/04/2016**

DRAWING NO. **DR-5**

General Exterior Elevation Notes

- Colors indicated on this drawing are approximate and will vary slightly from the actual installed colors. Refer to Colors and Materials Boards for true representation of all proposed finishes.
- All landscaping indicated on this drawing is diagrammatic and intended only to establish a general sense of scale and context. Refer to actual Landscape Plan for all proposed landscaping.

Material/Finish Legend

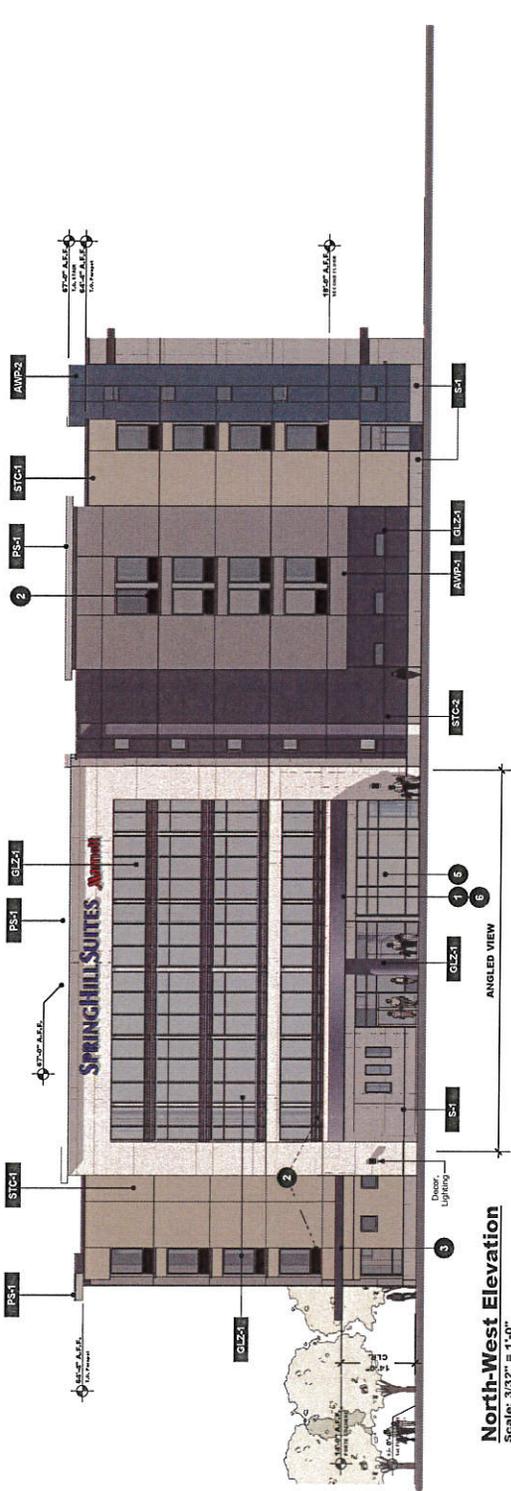
Refer to Colors and Materials Boards for true representation of all proposed finishes.

- STC-1** Painted Smooth Stucco
- STC-2** Painted Smooth Stucco
- SA-1** Stone Veneer
- GLZ-1** Glazing (Vision Glass)
- PS-1** Painted Metal
- AWP-1** Metal Wall Panel System (Grey)
- AWP-2** Metal Wall Panel System (Blue)
- AWP-3** Framed Perforated Panel
- CMU-1** Spifface CMU - Basaltite

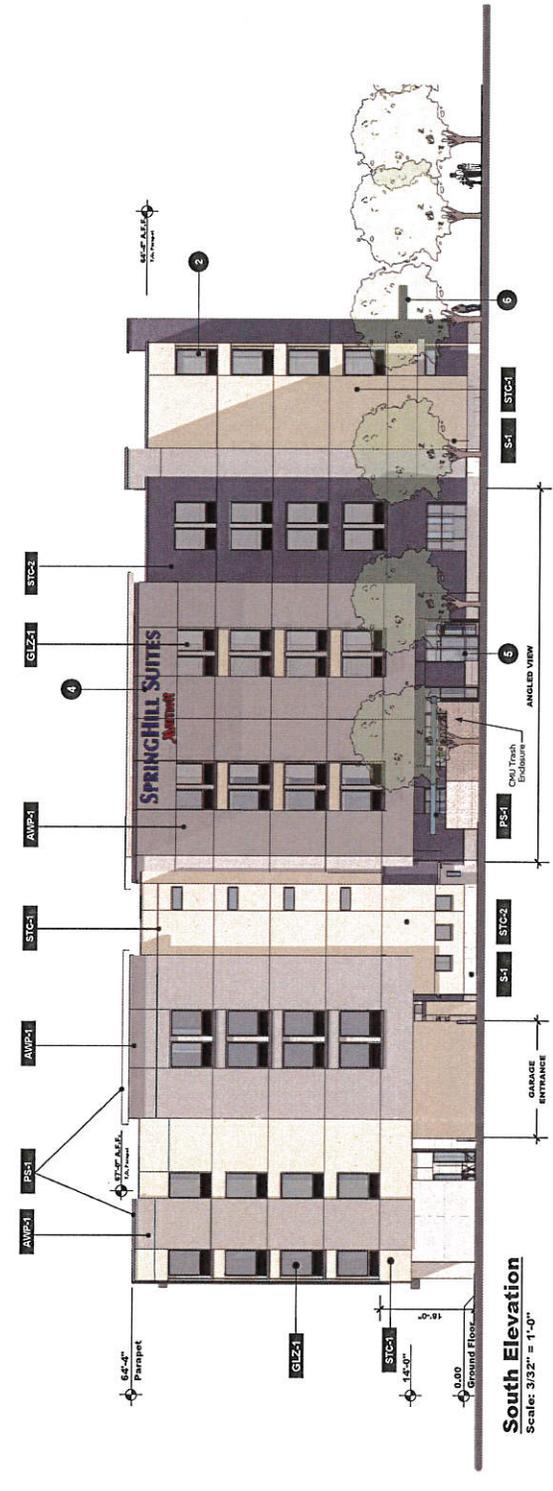
Keynotes

Note: Not all keynotes listed apply to this particular sheet.

- Porte Cochere.
- Packaged Terminal Air Conditioning (PTAC) unit integral to window system. Color and finish to match aluminum windows.
- Metal Sunscreen Awning.
- Internally Illuminated Channel Letter Signage.
- Aluminum Storefront System.
- Metal Canopy.
- Aluminum Window Wall System



North-West Elevation
Scale: 3/32" = 1'-0"



South Elevation
Scale: 3/32" = 1'-0"



SPRINGHILL SUITES
SAN BRUNO, California

PROJECT INFO.

300 Frank H. Ogawa Plaza, Suite 375
Oakland, CA 94612
TEL: 510.443.8300 | FAX: 510.443.8375

ARCHITECTURAL
DIMENSIONS

JOB NO.
OT040

DATE
07/04/2016

DRAWING NO.
DR-6

General Exterior Elevation Notes

- Colors indicated on this drawing are approximate and will vary depending on printer/material display source. Refer to Colors and Materials Boards for true representation of all proposed finishes.
- All landscaping indicated on this drawing is diagrammatic and intended only to convey a sense of general landscaped areas. Refer to actual Landscape Plan for all proposed landscaping.

Material/Finish Legend

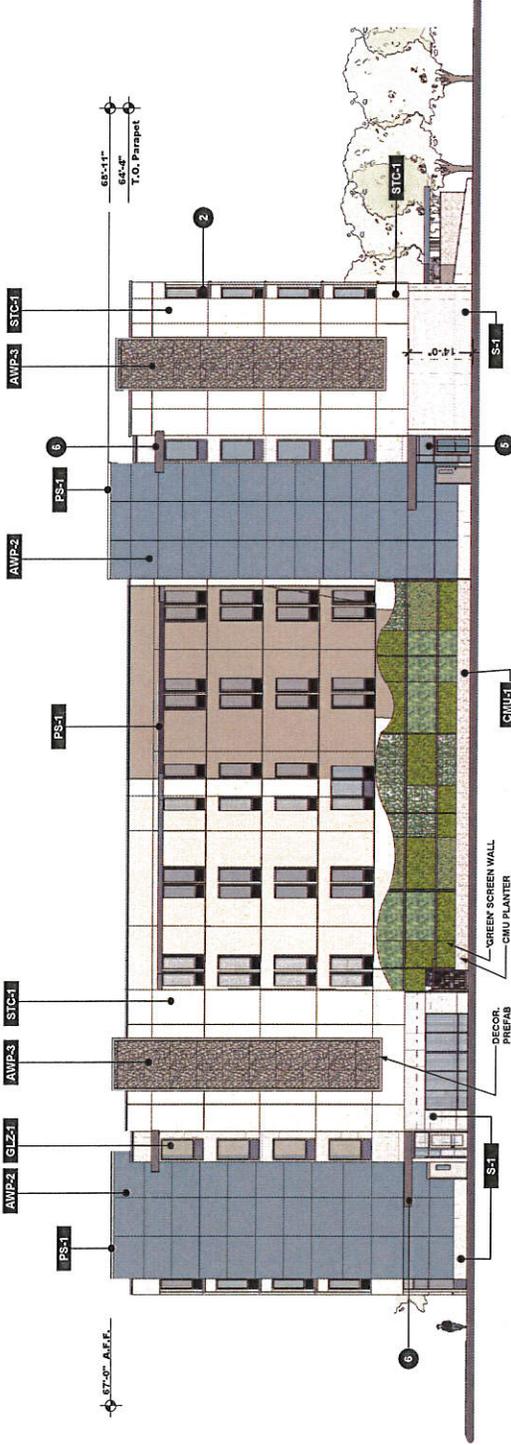
Refer to Colors and Materials Boards for true representation of all proposed finishes.

- STC-1** Painted Smooth Stucco
- STC-2** Painted Smooth Stucco
- S-1** Stone Veneer
- GLZ-1** Glazing (Vision Glass)
- PS-1** Painted Metal
- AWP-1** Metal Wall Panel System (Grey)
- AWP-2** Metal Wall Panel System (Blue)
- AWP-3** Framed Perforated Panel
- CMU-1** Spiffice CMU - Basaltite

Keynotes

Note: Not all keynotes listed apply to this particular sheet.

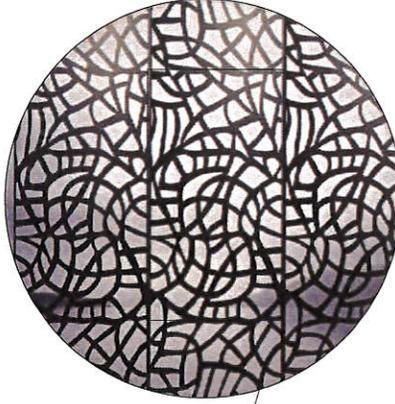
- Porte Cochere.
- Packaged Terminal Air Conditioning (PTAC) unit integral to window system. Color and finish to match aluminum windows.
- Metal Sunscreen Awning.
- Internally Illuminated Channel Letter Signage.
- Aluminum Storefront System.
- Metal Canopy.
- Aluminum Window Wall System



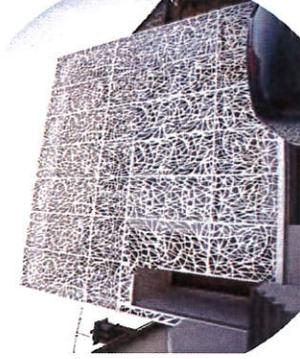
West (REAR) Elevation
Scale: 3/32" = 1'-0"



Partial Rear View
not in scale



Enlarged Detail of AWP-3
10'x5' prefab panels
not in scale
FINAL PATTERN TBD



Example
not in scale



PROJECT INFO.

300 Frank H. Ogawa Plaza, Suite 375
Oakland, CA 94612
TEL: 510.463.8300 | FAX: 510.463.8395

JOB NO.
OTO40

DATE
01/04/2016

**SPRINGHILL SUITES
SAN BRUNO, California**

**ARCHITECTURAL
DIMENSIONS**

DRAWING NO.
DR-6A

General Exterior Elevation Notes

- Colors indicated on this drawing are approximate and will vary depending on printer/monitor display source. Refer to Colors and Materials Boards for true representation of all proposed finishes.
- All landscaping indicated on this drawing is diagrammatic and intended only to convey a sense of general landscaped areas. Refer to actual Landscape Plan for all proposed landscaping.

Material/Finish Legend

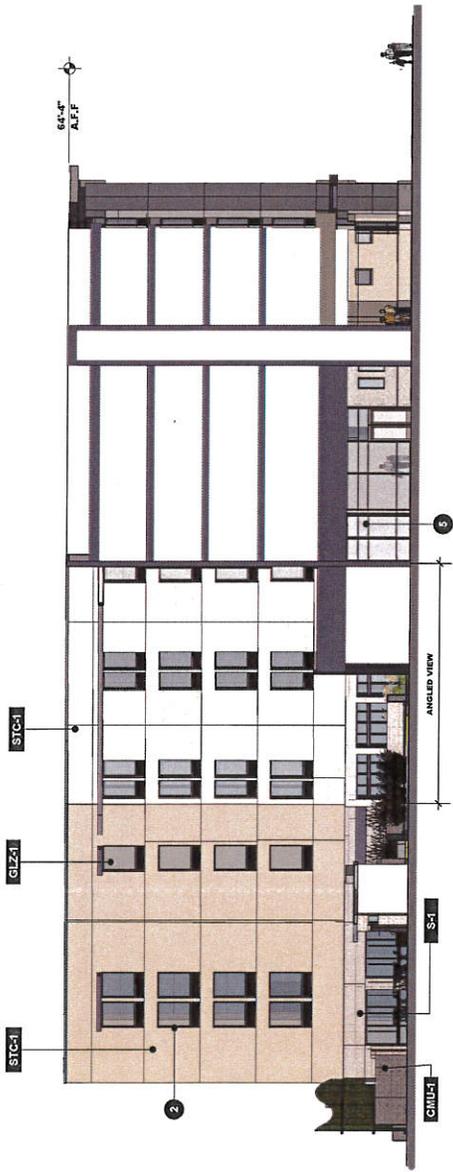
Refer to Colors and Materials Boards for true representation of all proposed finishes.

- STC-1** Painted Smooth Stucco
- STC-2** Painted Smooth Stucco
- S-1** Stone Veneer
- GLZ-1** Glazing (Vision Glass)
- PS-1** Painted Metal
- AWP-1** Metal Wall Panel System (Grey)
- AWP-2** Metal Wall Panel System (Blue)
- AWP-3** Framed Perforated Panel
- CMU-1** Spiffice CMU - Basaltite

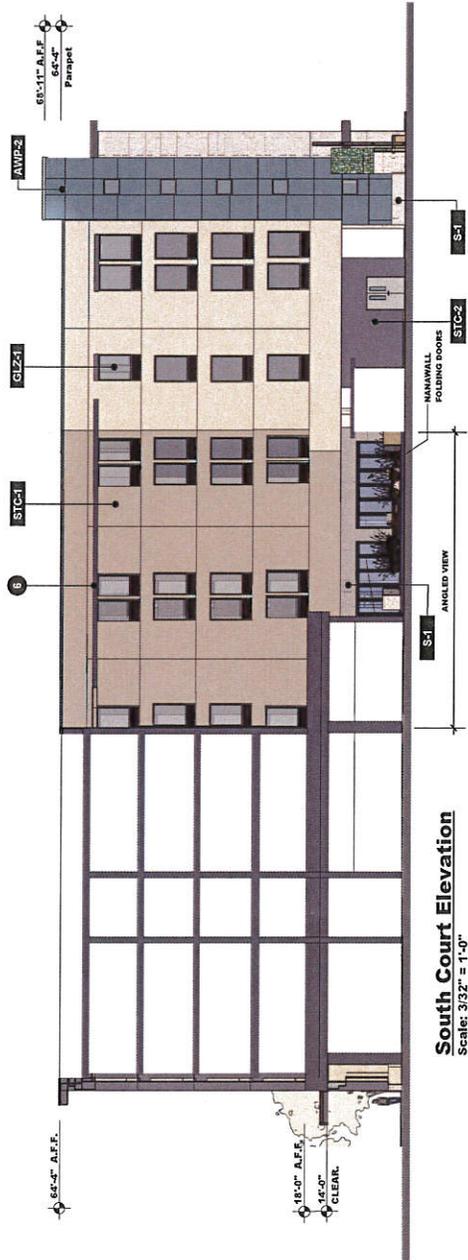
Keynotes

Note: Not all keynotes listed apply to this particular sheet.

- Porte Cochere.
- Packaged Terminal Air Conditioning (PTAC) unit integral to window system. Color and finish to match aluminum windows.
- Metal Sunscreen Awning.
- Internally Illuminated Channel Letter Signage.
- Aluminum Storefront System.
- Metal Canopy.
- Aluminum Window Wall System



North Court Elevation
Scale: 3/32" = 1'-0"



South Court Elevation
Scale: 3/32" = 1'-0"



PROJECT INFO.

300 Frank H. Ogawa Plaza, Suite 375
Oakland, CA 94612
TEL: 510.443.8300 | FAX: 510.443.8395

SPRINGHILL SUITES
SAN BRUNO, California

DRAWING NO.

OTO40

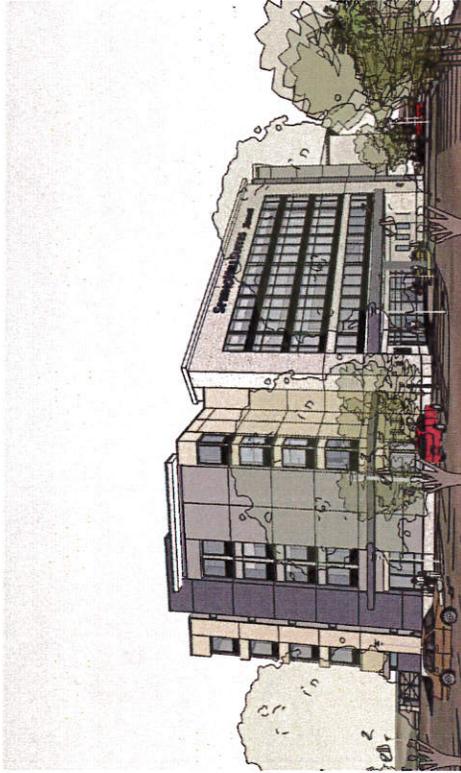
DRAWING NO.

DR-6B

DATE

01/04/2016

ARCHITECTURAL
DIMENSIONS



View from El Camino Real
Scale: NTS



View from Admiral Ct.
Scale: NTS



View from FWY 380 Ramp
Scale: NTS



Porte Cochere
Scale: NTS



PROJECT INFO.
300 Frank H. Ogawa Plaza, Suite 375
Oakland, CA 94612
TEL: 510.463.8300 | FAX: 510.463.8395

ARCHITECTURAL DIMENSIONS
SPRINGHILL SUITES
SAN BRUNO, California
PERSPECTIVE VIEWS
JOB NO. OTO40
DATE 01/04/2016
DRAWING NO. DR-7

SITE MATERIALS



Pool Patio- 24" sq. Concrete Pavers
"TECTURA" OceanView



Main Entrance Interlocking Paving-
"MCNEAR" Paving



Patio Design Sample



Concrete Sidewalk



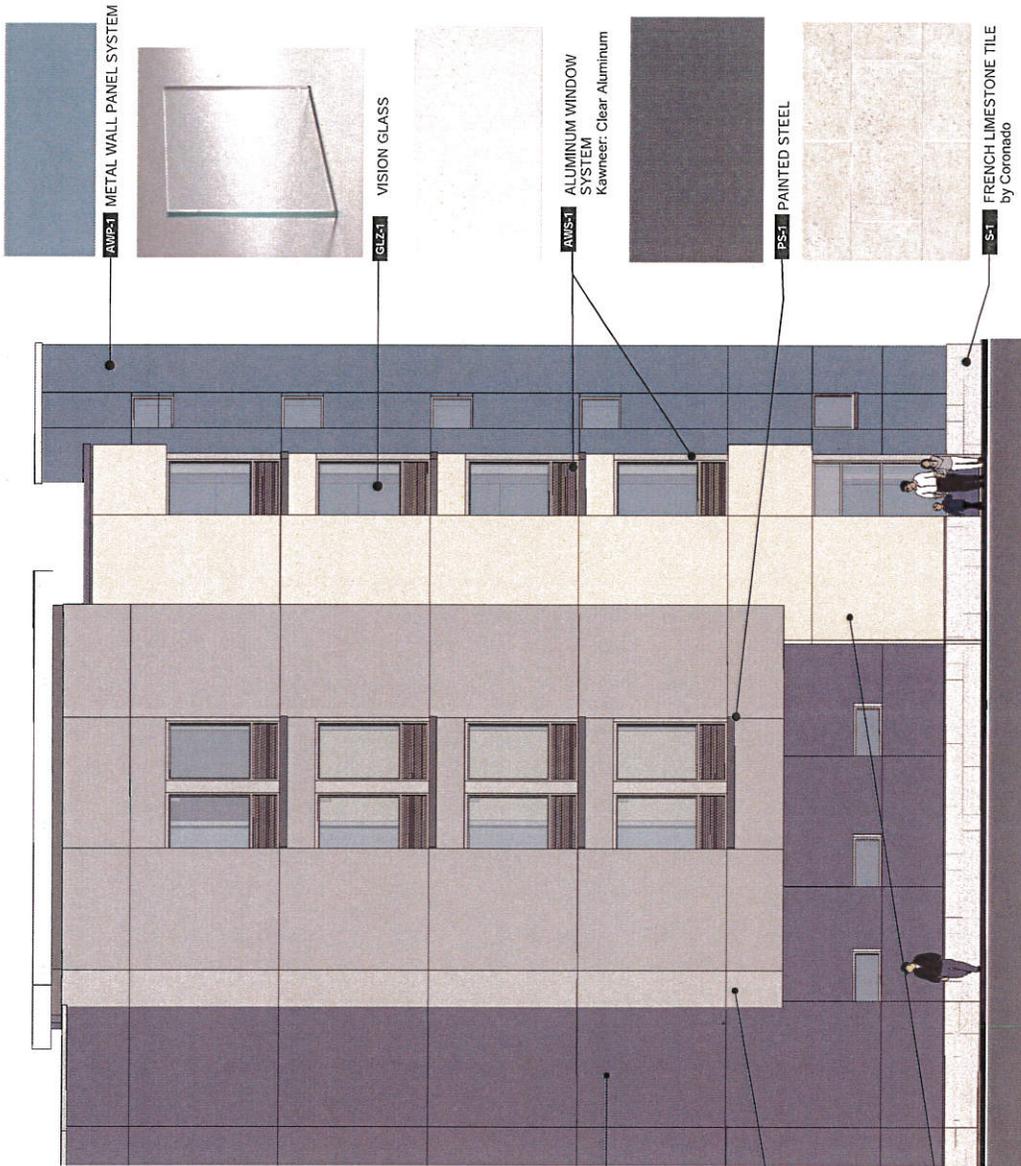
PAINTED SMOOTH STUCCO **STC-2**
Sherwin Williams



METAL WALL PANEL SYSTEM **AWP-2**



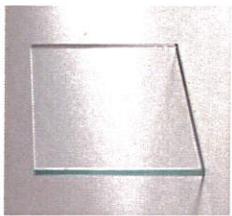
PAINTED SMOOTH STUCCO **STC-1**
Sherwin Williams



Enlarged Partial North Elevation
Scale: 1/4" = 1'-0"



AWP-1 METAL WALL PANEL SYSTEM



GLZ-1 VISION GLASS



AWIS-1 ALUMINUM WINDOW SYSTEM
Kawneer: Clear Aluminum



PS-1 PAINTED STEEL



S-1 FRENCH LIMESTONE TILE
by Coronado

PROJECT INFO.

300 Frank H. Ogawa Plaza, Suite 375
Oakland, CA 94612
TEL. 510.463.8300 | FAX. 510.463.8395

**SPRINGHILL SUITES
SAN BRUNO, California**

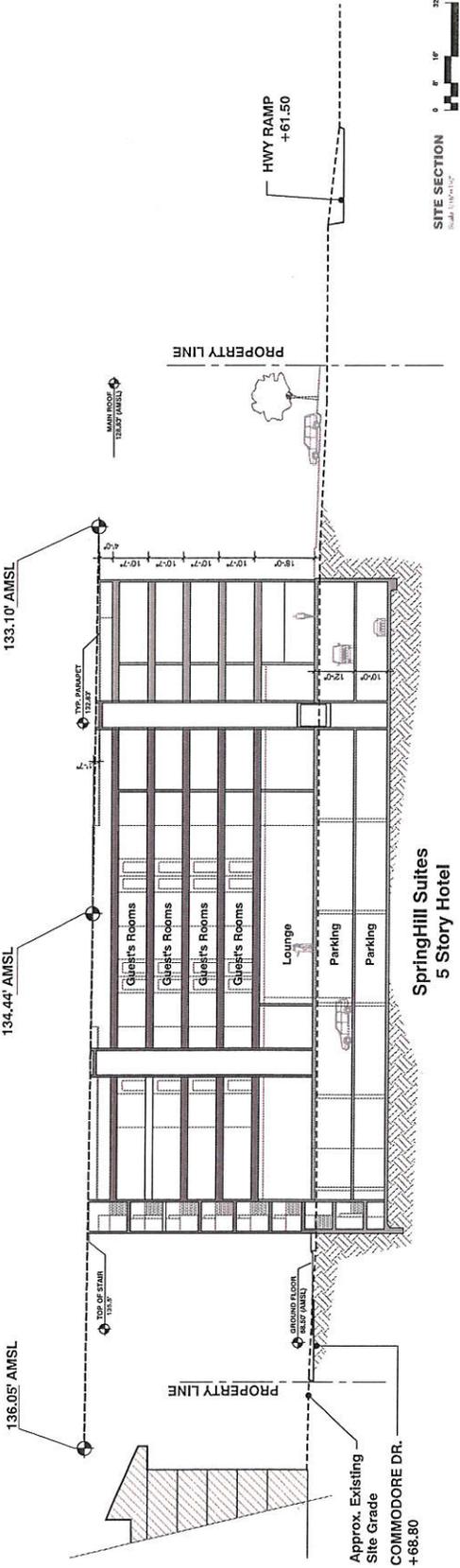
DRAWING NO.

JOB NO.
OTO40

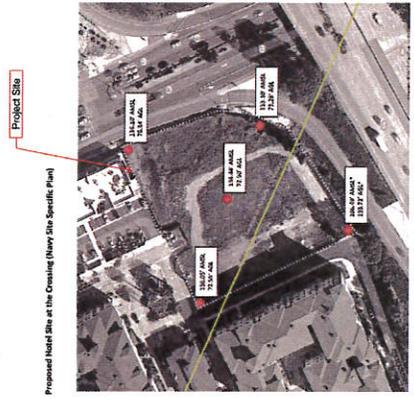
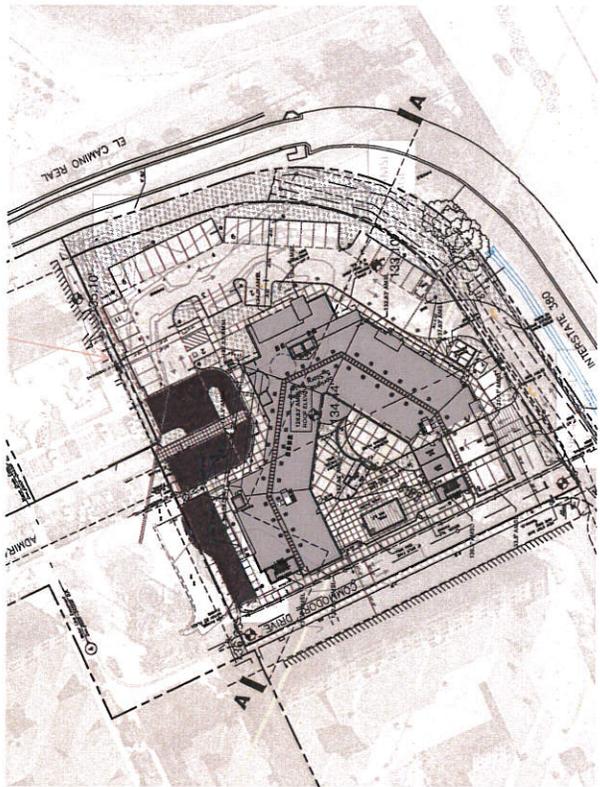
DATE
01/04/2016

DR-8

**ARCHITECTURAL
DIMENSIONS**



SpringHill Suites
5 Story Hotel



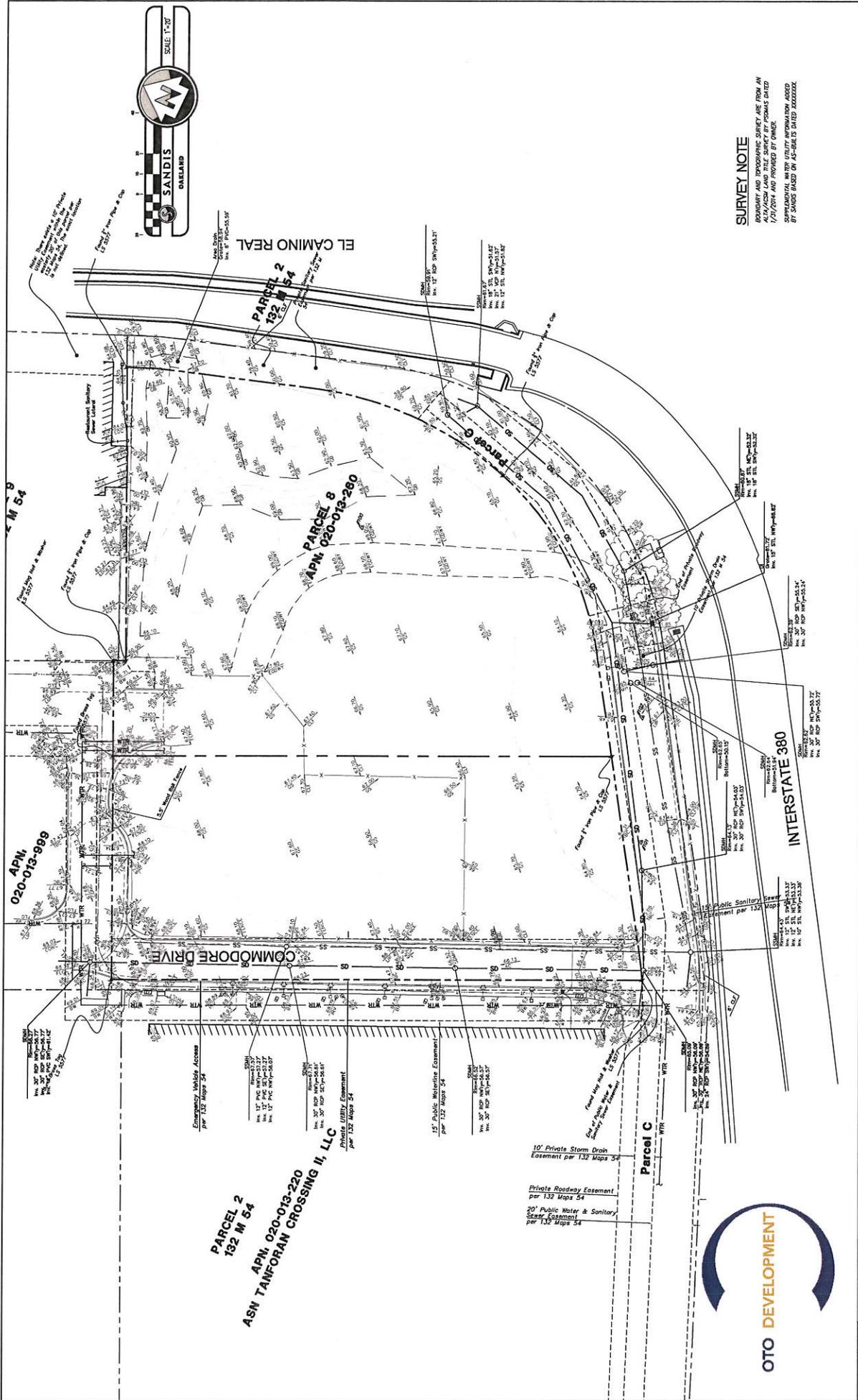
AMSL: Above Mean Sea Level
 Note: This plan is not intended to be used for construction. It is intended for informational purposes only. The final ground conditions, for this building, ground conditions, and other factors, shall be determined by the geotechnical engineer and approved by the local government. For this building, ground conditions shall be determined by the geotechnical engineer and approved by the local government. For this building, ground conditions shall be determined by the geotechnical engineer and approved by the local government. For this building, ground conditions shall be determined by the geotechnical engineer and approved by the local government.



PROJECT INFO.
 300 Frank H. Ogawa Plaza
 Oakland, CA 94612
 TEL. 510.463.8395 • FAX. 510.463.8395

SPRINGHILL SUITES
 SAN BRUNO, CA

HEIGHT STUDY JOB NO. 01040 DRAWING NO. DR-9
 DATE: 12.02.2015



SURVEY NOTE

BOUNDARY AND ELEVATION SURVEY ARE FROM AN
 ADJACENT SURVEY DATED 7/21/2014 AND PROVIDED BY OWNER.
 SUPPLEMENTAL METRIC UTILITY INFORMATION ADDED
 BY SANDIS BASED ON AS-BUILT DATA ACCESSIBLE.

ARCHITECTURAL DIMENSIONS	PROJECT INFO: 300 Frank H. Ogawa Plaza Oakland, CA 94612 TEL. 510.463.8395 • FAX. 510.463.8395	JOB NO. C1040	DRAWING NO. C-1
	OTO DEVELOPMENT	DATE 8/19/2016	
SPRINGHILL SUITES SAN BRUNO, CA TOPOGRAPHIC SURVEY			

The A:\PROJECTS\PLAN SETS\C-1.dwg Date: Aug 19, 2016

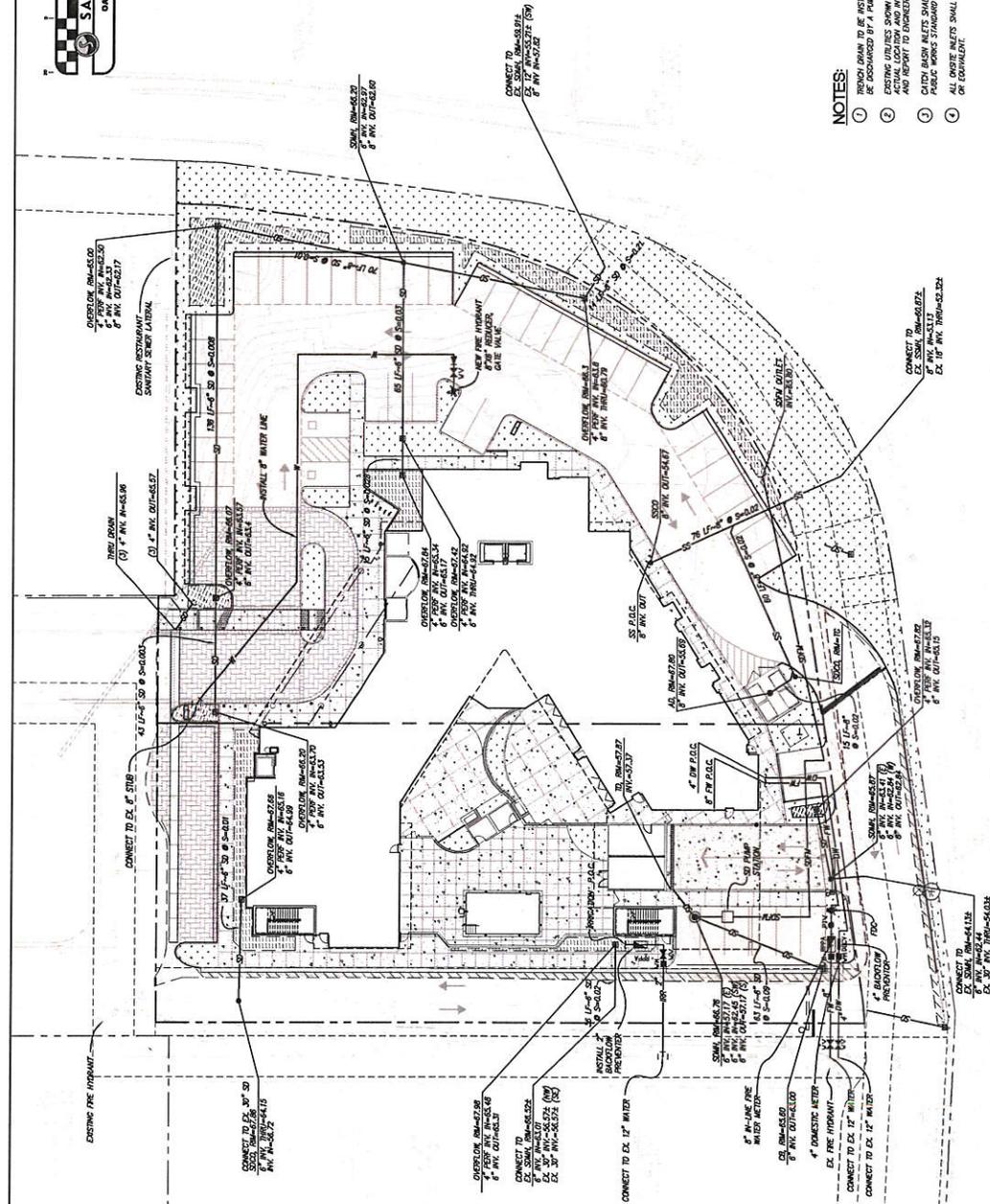


LEGEND

	PROPERTY LINE
	SWALLOW LINE
	AC PAVEMENT
	KEEP LEFT
	CONCRETE PAVEMENT
	CIRCULAR CONCRETE PAVEMENT
	LANDSCAPING AREA
	BIO-TREATMENT AREA ON SHEET C-4
	STAMPED CONCRETE SEE LANDSCAPE PLANS

NOTES:

1. BRANCH DRAIN TO BE INSTALLED AT BOTTOM OF RAMP, RUN-OFF TO BE DISCHARGED BY A PUMP.
2. EXISTING UTILITIES SHOWN ARE APPROXIMATE. CONTRACTOR TO VERIFY ACTUAL LOCATION AND DEPTH. EXISTING UTILITIES TO BE PROTECTED AND REPAIRED AS NECESSARY. EXISTING UTILITIES TO BE PROTECTED AND REPAIRED AS NECESSARY.
3. ALL CONCRETE SHALL BE FINISHED TO MATCH EXISTING CONCRETE OF EQUIVALENT.
4. ALL CONCRETE SHALL BE FINISHED TO MATCH EXISTING CONCRETE OF EQUIVALENT.



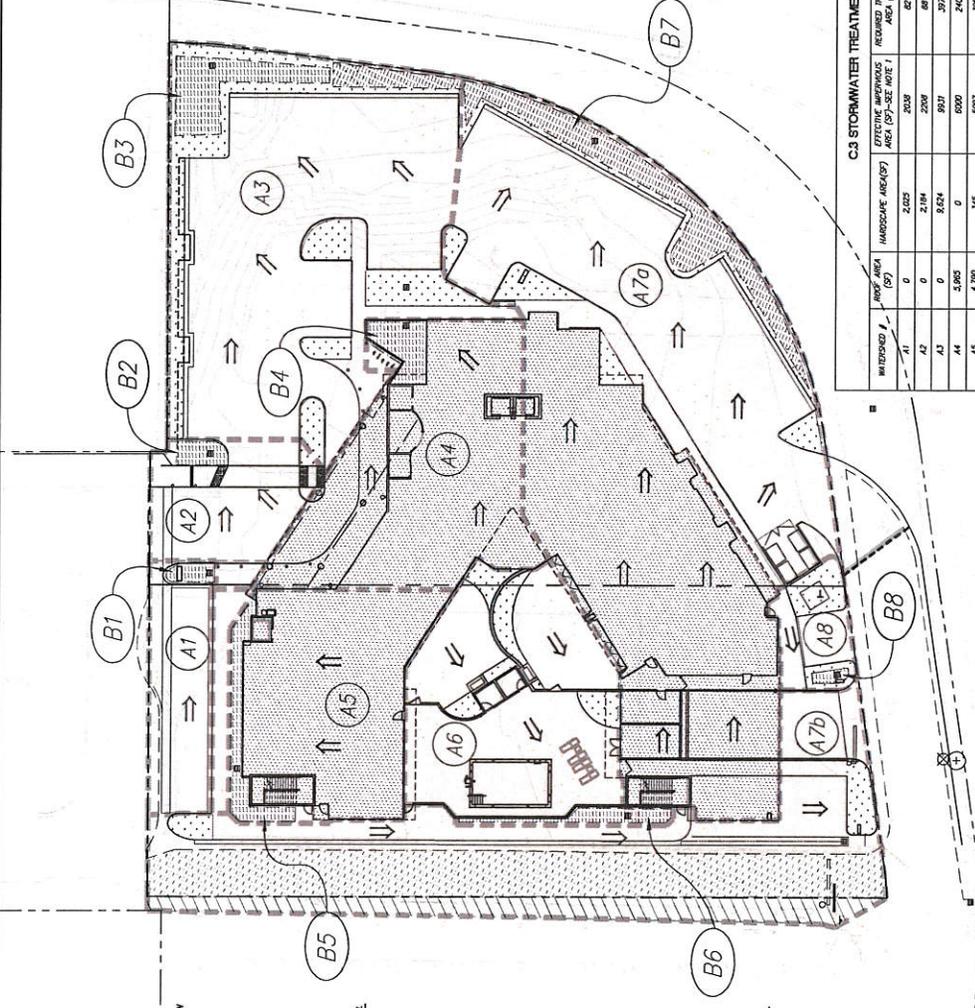
JOB NO. C10140
DRAWING NO. C-3
DATE 8/10/2015

SPRINGHILL SUITES
SAN BRUNO, CA
UTILITY PLAN

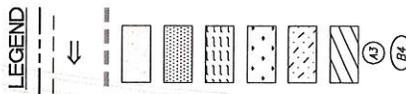
PROJECT INFO.
300 Frank H. Ogawa Plaza
Oakland, CA 94612
TEL 510.463.8393 • FAX 510.463.8395



ARCHITECTURAL
DIMENSIONS



NOTES:
A. THIS PLAN PRESENTS METHODS FOR MITIGATING THE POLLUTION PREVENTION PROGRAM STORMWATER QUALITY CONTROL REQUIREMENTS.
B. THE FOLLOWING RETENTION MEASURES PROPOSED COMPLY WITH STORMWATER QUALITY REGULATIONS.



C3 STORMWATER TREATMENT MEASURES

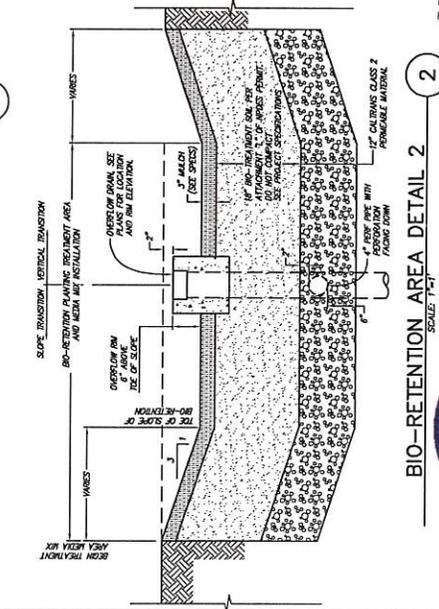
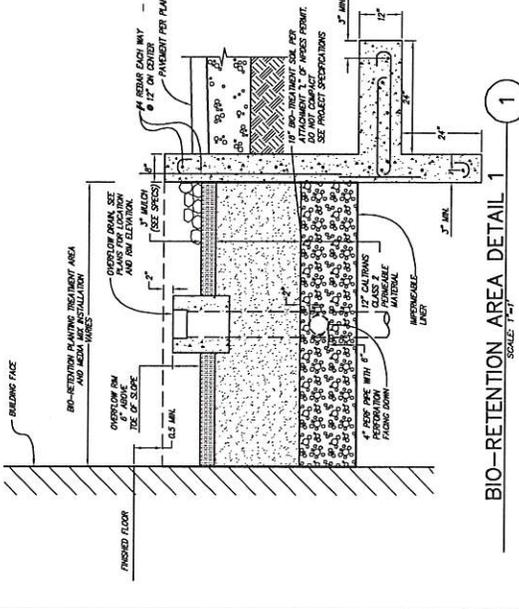
WATERSHED #	IMPERVIOUS AREA (SQ)	EFFECTIVE IMPERVIOUS AREA (SQ) - SEE NOTE 1	RETIRED IMPERVIOUS AREA (SQ)	BIOPRETENTION AREA (SQ)	BMP #
A1	0	2,029	2038	82	B1
A2	0	2,194	2208	88	B2
A3	0	8,824	8921	397	B3
A4	0	4,985	6,000	240	B4
A5	0	4,790	5,927	255	B5
A6	0	5,119	5,929	278	B6
A7a	8,801	8,803	10,500	1,126	B7
A7b	0	8,253	N/A	330	B7
A8	0	622	664	27	B8
TOTAL	20,306	36,835	38,815	2,894	3,890

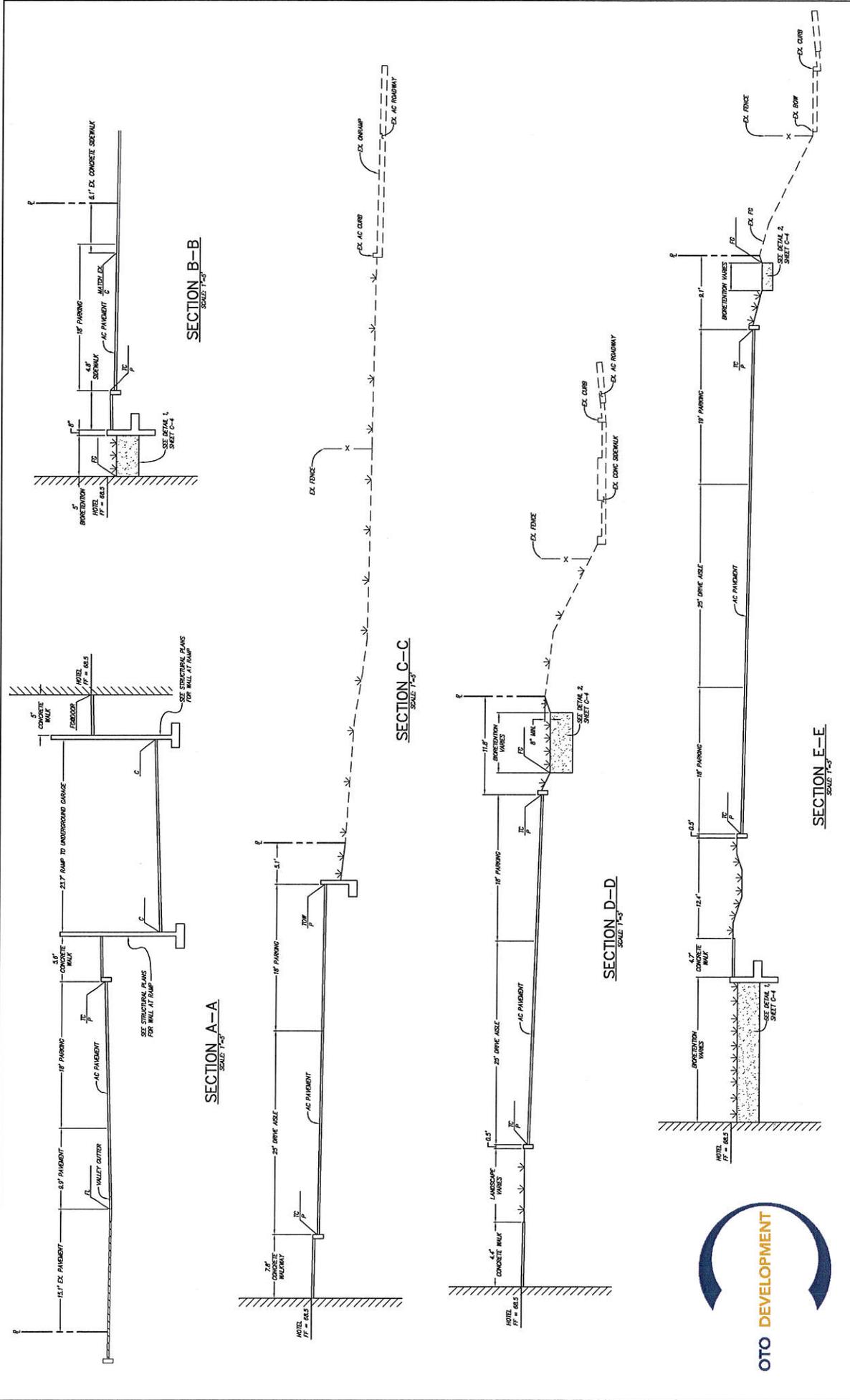
NOTES:
1. EFFECTIVE IMPERVIOUS AREA IS EQUAL TO (IMPERVIOUS AREA) FOR WATERSHEDS WHOSE PERVIOUS AREA IS LESS THAN 2% OF THE TOTAL WATERSHED AREA, AND IS EQUAL TO (IMPERVIOUS AREA) x (IMPERVIOUS AREA) FOR WATERSHEDS WHOSE PERVIOUS AREA IS GREATER THAN 2% OF THE TOTAL WATERSHED AREA.
2. AREA A7B INCLUDES 154 SQUARE FEET OF EXISTING IMPERVIOUS AREA. WE ARE PROPOSING THAT THE BIOPRETENTION OF THIS AREA BE CREDITED TO THE NEW IMPERVIOUS AREAS BEING CREATED IN PARCEL 2. SEE PARCEL 2 TABLE AND NOTES FOR ADDITIONAL NOTES ON THE APPROACH.

PARCEL 2

TOTAL PARCEL SIZE	18,629 SQ. FT.
AREA OF PARCEL ATTACHED	8,824 SQ. FT.
EXISTING IMPERVIOUS AREA REPLACED	103 SQ. FT.
EXISTING PERVIOUS AREA REPLACED	103 SQ. FT.
EXISTING PARCEL NOT ATTACHED	7,411 SQ. FT.
	1,899 SQ. FT.

NOTE: THE AREA OF THE PROJECT IS NOT WITHIN THE SITE PROPERTY. THERE IS ALSO AN ADJACENT PROPERTY TO THE PROJECT. THIS ADJACENT PROPERTY IS ALSO WITHIN THE PROJECT'S WATERSHED. WE ARE PROPOSING A-LEAK CHECK DRAIN TO THIS AREA TO INCLUDE 75% OF EXISTING IMPERVIOUS AREA WHICH EXCEEDS THE IMPERVIOUS AREA BEING CREDITED OR REPLACED WITHIN PARCEL 2.



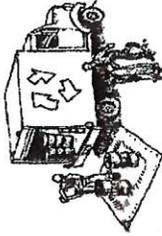


ARCHITECTURAL DIMENSIONS	PROJECT INFO. 300 Frank H. Ogawa Plaza Oakland, CA 94612 TEL. 510.463.8395 • FAX. 510.463.8395		JOB NO. C1040 DATE 8/10/2005	DRAWING NO. C-5
	SPRINGHILL SUITES SAN BRUNO, CA GRADING SECTIONS			

Construction Best Management Practices (BMPs)

Construction projects are required to implement the stormwater best management practices (BMP) on this page, as they apply to your project, all year long.

Materials & Waste Management

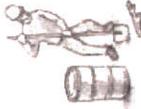


- Non-Hazardous Materials**
- Form and cover stockpiles of sand, dirt or other construction material with tarps when rain is forecast or if not actively being used within 14 days.
 - Use (but don't on cruise) reclaimed water for dust control.
- Hazardous Materials**
- Label all hazardous materials and hazardous wastes (such as pesticides, paints, thinners, solvents, fuel, oil, and antifreeze) in accordance with city, county, state and federal regulations.
 - Store hazardous materials and wastes in water tight containers, store in appropriate secondary containment, and cover them at the end of every work day or during wet weather or when rain is forecast.
 - Follow manufacturer's application instructions for hazardous spills, chemicals outdoors when rain is forecast within 24 hours.
 - Arrange for appropriate disposal of all hazardous wastes.

- Waste Management**
- Cover waste disposal containers securely with tarps at the end of every work day and during wet weather.
 - Check waste disposal containers frequently for leaks and to make sure they are not overfilled. Never hose down a dumpster on the construction site.
 - Clean or replace portable toilets, and inspect them frequently for leaks and spills.
 - Dispose of paint and fabric properly. Recycle materials and wastes that can be recycled (such as asphalt, concrete, aggregate base materials, wood, dry board, pipe, etc.).
 - Dispose of liquid residues from paints, thinners, solvents, glues, and cleaning fluids as hazardous waste.

- Construction Entrances and Perimeter**
- Establish and maintain effective perimeter controls and stabilize all construction entrances and exits with silt fencing and erosion and sediment control devices from site and tracking off site.
 - Sweep or vacuum any street tracking immediately and secure sediment source to prevent further tracking. Never hose down streets to clean up tracking.

Equipment Management & Spill Control



- Maintenance and Parking**
- Designate an area, fitted with appropriate BMPs, for vehicle and equipment parking and storage.
 - Perform major maintenance, repair jobs, and vehicle and equipment washing off site.
 - If refueling or vehicle maintenance must be done onsite, work in a bermed area away from storm drains and over a drip pan or drop cloths big enough to collect fluids. Recycle or dispose of fluids as hazardous waste.
 - If vehicle or equipment cleaning must be done onsite, clean with water only in a bermed area that will not allow rinse water to run into gutters, streets, storm drains, or surface waters.
 - Do not clean vehicle or equipment onsite using soaps, solvents, degreasers, or steam cleaning equipment.

- Spill Prevention and Control**
- Keep spill cleanup materials (e.g., rags, absorbents and oil filter) available at the construction site at all times.
 - Inspect vehicles and equipment frequently for and until repairs are made. Use dry pans to catch leaks and clean up spills or leaks immediately and dispose of cleanup materials properly.
 - Do not hose down surfaces where fluids have spilled, filter, and/or rag.
 - Use dry cleanup methods (absorbent materials, cat litter, and/or rag).
 - Sweep up spilled dry materials immediately. Do not try to wash them away with water, or bury them.
 - Clean up spills on dirt areas by digging up and properly disposing of contaminated soil.
 - Report significant spills immediately. You are required by law to report all significant releases of hazardous materials, including oil. To report a spill: 1) Dial 911 or your local emergency response number, 2) Call the Governor's Office of Emergency Services Warning Center, (800) 452-7350 (24 hours).

Earthmoving



- Schedule grading and excavation work during dry weather.
- Stabilize all denuded areas, install and maintain temporary erosion controls (such as silt fences or straw bales) on exposed slopes, and revegetate as soon as possible.
- Remove existing vegetation only when absolutely necessary, and seed or plant vegetation for erosion control on slopes or where construction is not immediately planned.
- Prevent sediment from migrating offsite and protect storm drain inlets, gutters, ditches, and drainage courses by installing and maintaining appropriate BMPs, such as filter socks, silt fences, sediment basins, and silt traps.
- Keep excavated soil on site and transfer it to dump trucks on site, not in the streets.

- Contaminated Soils**
- If any of the following conditions are present, stop work immediately and contact the Regional Water Quality Control Board:
 - Unusual soil conditions, discoloration, or odor.
 - Abandoned underground tanks.
 - Abandoned wells.
 - Buried barrels, drums, or tanks.

Paving/Asphalt Work



- Avoid paving and seal coating in wet weather or when rain is forecast, to prevent materials that have not cured from collecting stormwater runoff.
- Cover storm drain inlets and manholes with pre-applied seal coats, thick coat slurry water-applying seal, etc.
- Collect or recycle or appropriately dispose of excess concrete or seal coats. Do NOT sweep or wash it into gutters.
- Do not use water to wash down fresh asphalt concrete pavement.

- Sawcutting & Asphalt/Concrete Removal**
- Protect nearby storm drain inlets, gutters, saw cuttings. Use filter fabric, catch basin inlet filters, or gravel bags to keep slurry out of the storm drain system.
 - Shovel, absorb, or vacuum saw-cut slurry and dispose of all waste as soon as you are finished in one location or at the end of each work day (whichever is sooner).
 - If sawcut slurry enters a catch basin, clean it up immediately.

Concrete, Grout & Mortar Application



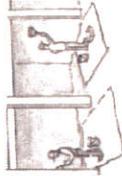
- Store concrete, grout, and mortar away from storm drains or waterways, and on pallets under cover to protect them from rain, runoff, and wind.
- Wash out concrete equipment/trucks offsite or in a designated washout area. Wash concrete into a storm drain temporary waste pit, and in a storm drain that will prevent leachate into the underlying soil or onto surrounding areas. Let concrete harden and dispose of as garbage.
- When washing exposed aggregate, prevent washwater from entering storm drains. Block any intake and vacuum gauges, hose washwater onto dirt areas, or other areas that will not be pumped and disposed of properly.

Landscaping



- Protect stockpiled landscaping materials from wind and rain by storing them under a tarp all year-round.
- Stack bagged material on pallets and under cover.
- Discourage application of any erodible landscaping materials on slopes or in areas forecast to erode or during wet weather.

Painting & Paint Removal



- Painting Cleanup and Removal**
- Never clean brushes or rinse paint containers into a street, gutter, storm drain, or stream.
 - For water-based paints, paint out brushes to the extent possible, and rinse into a drain that goes to the sanitary sewer. Never pour paint down a storm drain.
 - For oil-based paints, paint out brushes to the extent possible and clean with thinner or mineral spirits. Dispose of the thinner and excess thinner and solvents. Dispose of excess liquid as hazardous waste.
 - Paint chips and dust from non-hazardous dry stripping and sand blasting may be swept up or collected in plastic drop cloths and disposed of as trash.
 - Chemical paint stripping residue and chips and dust from marine paints or paints containing lead, mercury, or tributyltin must be disposed of as hazardous waste. Lead-based paint removal requires a state-certified contractor.

Dewatering



- Discharges of groundwater or captured runoff from dewatering operations must be properly managed and disposed. When possible send dewatering discharge to landscaped area or sanitary sewer. If discharging to the sanitary sewer call your local wastewater treatment plant.
- Divert run-on water from offsite away from all disturbed areas.
- When dewatering, notify and obtain approval from the local authority before discharging water to a storm drain, through a basin, tank, or sediment trap may be required.
- In areas of known or suspected contamination, call your local agency to determine whether the ground water must be tested. Pumped groundwater may need to be collected and treated off-site for treatment and proper disposal.

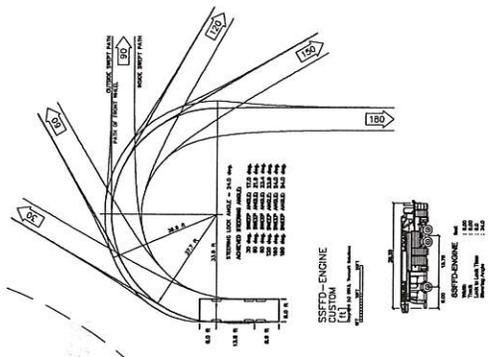
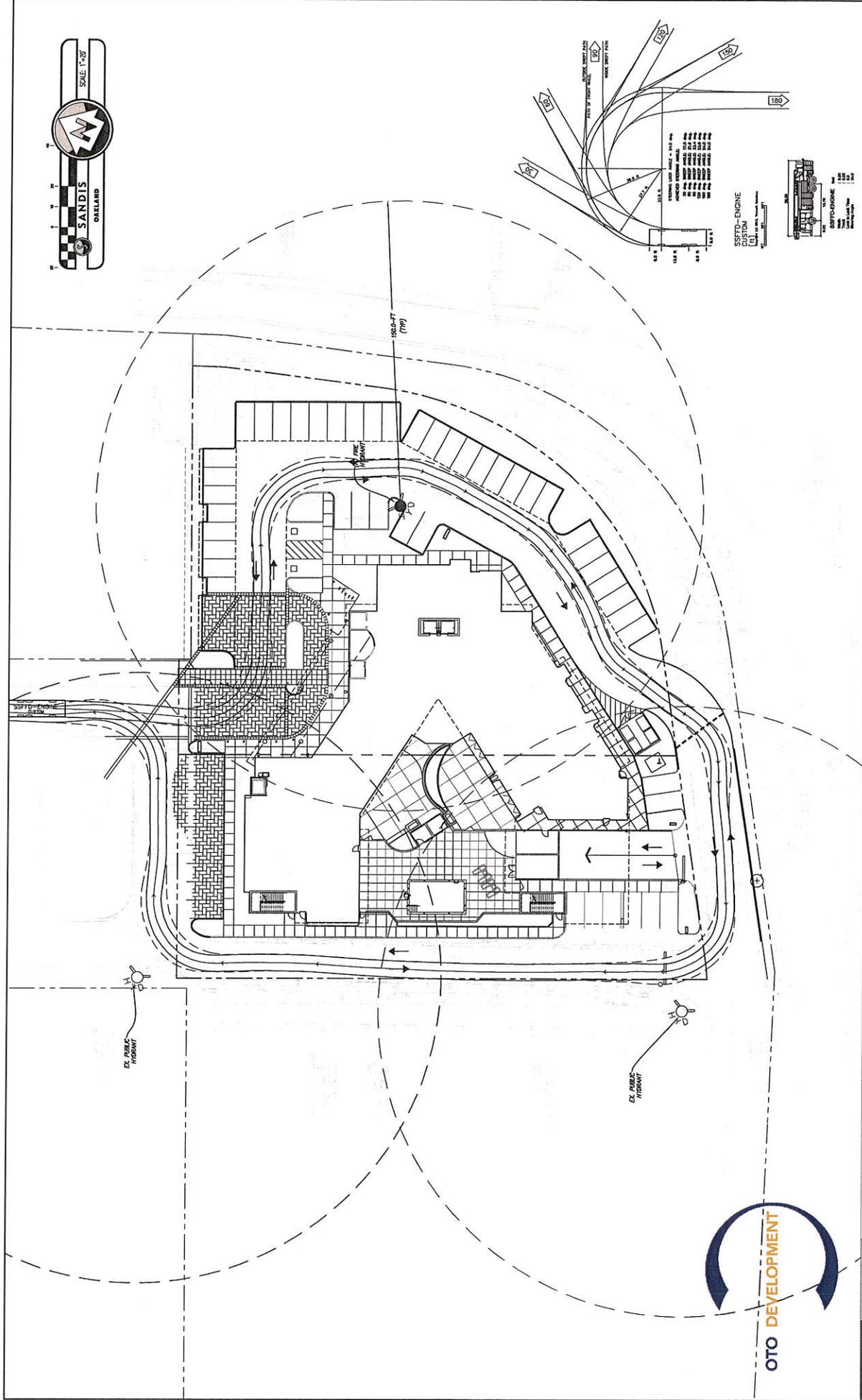
Storm drain polluters may be liable for fines of up to \$10,000 per day!

PROJECT INFO

300 Frank H. Ogawa Plaza
 Oakland, CA 94612
 TEL 510.463.8595 • FAX 510.463.8595

ARCHITECTURAL
DIMENSIONS

SPRINGHILL SUITES
SAN BRUNO, CA
SAN MATEO COUNTY BEST MANAGEMENT PRACTICES



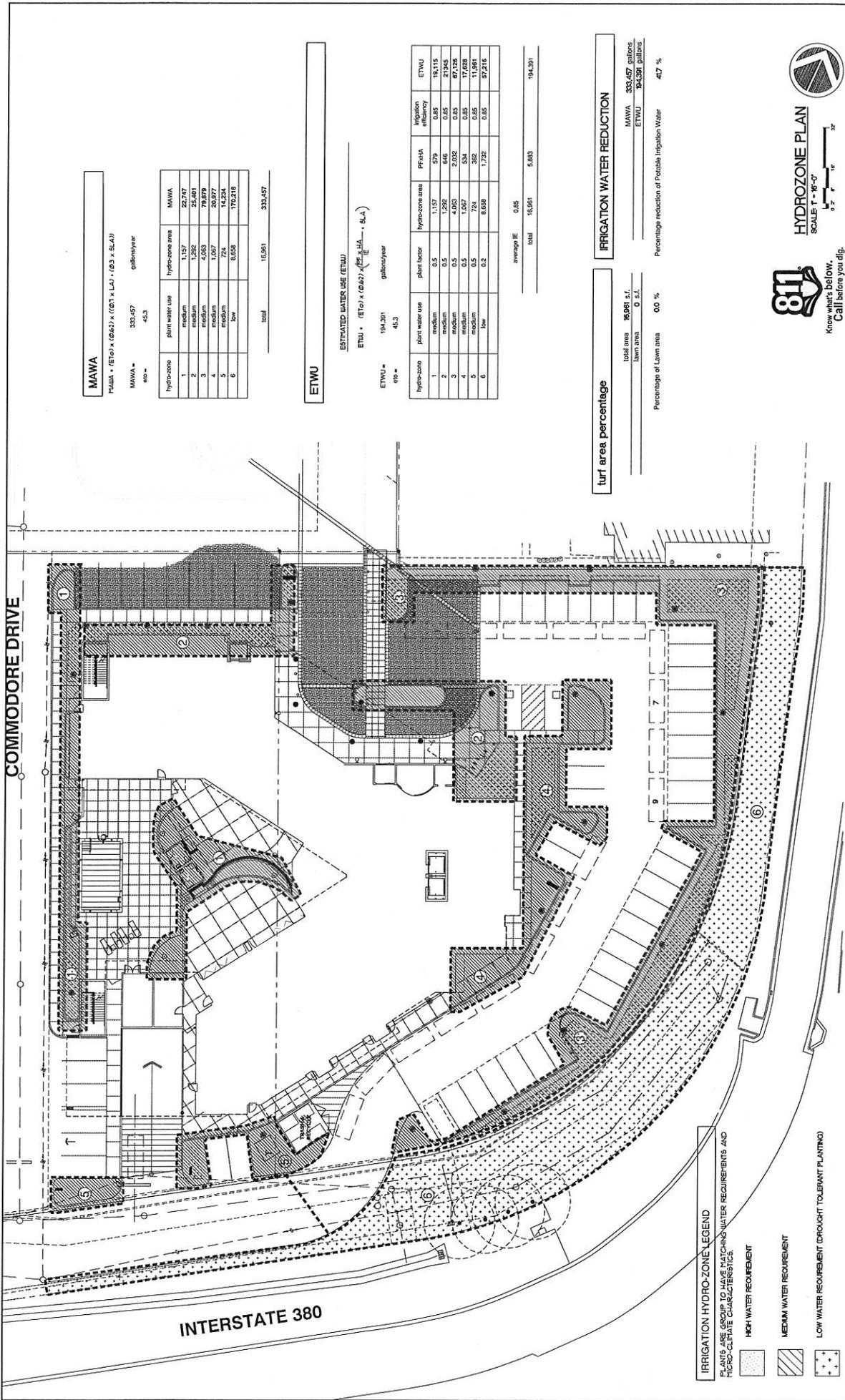
PROJECT INFO.
 300 Frank H. Ogawa Plaza
 Oakland, CA 94612
 TEL. 510-463-8395 • FAX. 510-463-8395

ARCHITECTURAL
 DIMENSIONS

SPRINGHILL SUITES
 SAN BRUNO, CA
 FIRE ACCESS PLAN

JOB NO. C1040
 DATE 8/10/2005
 DRAWING NO. C-9

File: A:\P165059\PLAN_SRTS\C-9.dwg Date: Aug 10, 2005



MAWA
 $MAWA = (ET \times I) \times (0.62) \times (0.61 \times LA) + (0.3 \times SLA)$

MAWA = 333,457 gallons/year
 60 = 45.3

hydro-zone	plant water use	hydro-zone area	MAWA
1	medium	1,157	22,767
2	medium	4,063	83,725
3	medium	1,057	20,977
4	medium	724	14,234
5	medium	6,558	130,218
6	low		
total		16,561	333,457

ETWU

ESTIMATED WATER USE (ETWU)
 $ETWU = (ET \times I) \times (0.62) \times (0.61 \times LA) + (0.3 \times SLA)$

ETWU = 194,391 gallons/year
 60 = 45.3

hydro-zone	plant water use	plant factor	hydro-zone area	FFVH/A	ETWU	injection efficiency
1	medium	0.5	1,157	579	19,115	0.85
2	medium	0.5	4,063	2,032	67,725	0.85
3	medium	0.5	1,057	524	17,628	0.85
4	medium	0.5	724	362	11,981	0.85
5	medium	0.2	6,558	1,752	97,218	0.85
6	low					
average I/E					0.05	
total					194,391	

lurf area percentage

local area 8,098 s.f.
 lawn area 0 s.f.

Percentage of Lawn area 0.0 %

IRRIGATION WATER REDUCTION

MAWA 333,457 gallons
 ETWU 194,391 gallons

Percentage reduction of Potable Irrigation Water 41.7 %

IRRIGATION HYDRO-ZONE LEGEND
 PLANTS ARE GROUPED TO HAVE MATCHING WATER REQUIREMENTS AND MICRO-CLIMATE CHARACTERISTICS.

- HIGH WATER REQUIREMENT
- MEDIUM WATER REQUIREMENT
- LOW WATER REQUIREMENT (DROUGHT TOLERANT PLANTING)

811
 Know what's below.
 Call before you dig.

HYDROZONE PLAN
 SCALE: 1" = 16'-0"

ARCHITECTURAL DIMENSIONS

300 Frank H. Ogawa Plaza
 Oakland, CA 94612
 TEL. 510.463.8395 • FAX. 510.463.8395

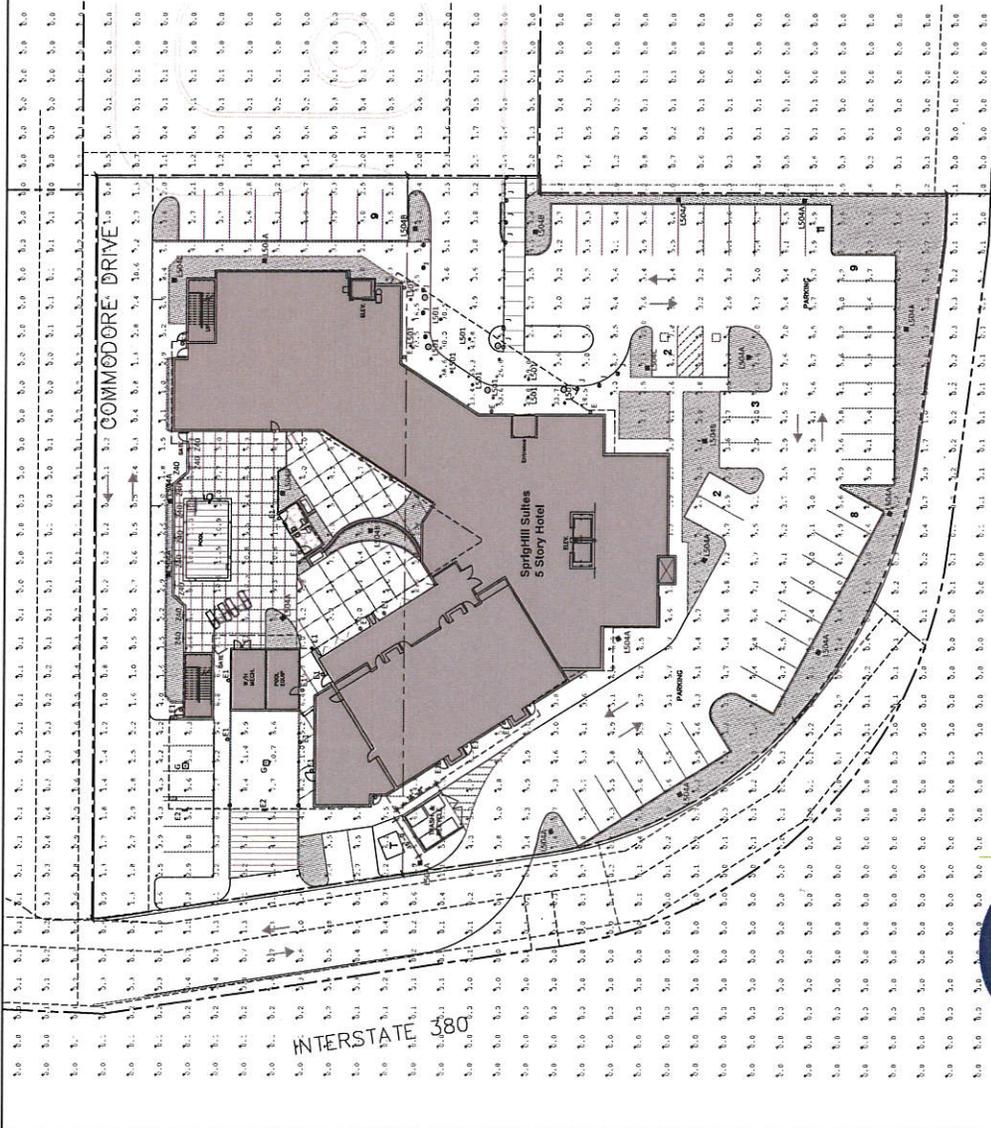
SPRINGHILL SUITES
 SAN BRUNO, CA

PROJECT INFO.

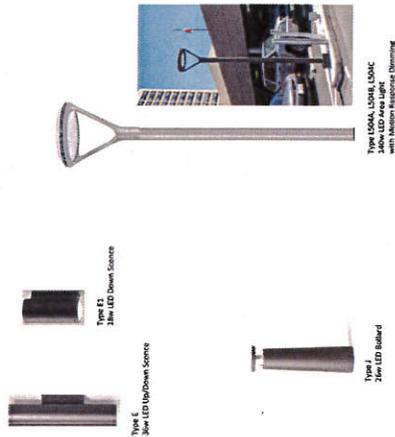
REED ASSOCIATES
 LANDSCAPE ARCHITECTURE
 SUITE 100
 SAN BRUNO, CALIFORNIA 94066
 408.481.9520 / 408.481.9822 FAX

JOB NO. 01040
 DATE: 8.10.2015

DRAWING NO. L2.0



PARAGON
DISTRIBUTION
SPRINGHILL SUITES SAN BRUNO



Type E
30w LED Down Scents

Type F
30w LED Down Scents

Type G
30w LED Down Scents

Type H
30w LED Down Scents

49/715

Tag	Arrangement	Qty	Label	Description	LF
E <td>SINGLE</td> <td>5</td> <td>051LED 537</td> <td>30w LED Down Scents Wall Scents</td> <td>0.000</td>	SINGLE	5	051LED 537	30w LED Down Scents Wall Scents	0.000
F <td>SINGLE</td> <td>21</td> <td>659LED 537</td> <td>30w LED Down Scents Wall Scents</td> <td>0.000</td>	SINGLE	21	659LED 537	30w LED Down Scents Wall Scents	0.000
G <td>SINGLE</td> <td>2</td> <td>DAW2_LED_30C_200C_30C_171W_M_1</td> <td>100w LED Down Scents Wall Scents</td> <td>0.000</td>	SINGLE	2	DAW2_LED_30C_200C_30C_171W_M_1	100w LED Down Scents Wall Scents	0.000
H <td>SINGLE</td> <td>2</td> <td>GI 485 5-4 110X4 4470 WW_1</td> <td>137w LED Down Scents Wall Scents</td> <td>0.000</td>	SINGLE	2	GI 485 5-4 110X4 4470 WW_1	137w LED Down Scents Wall Scents	0.000
I <td>SINGLE</td> <td>15</td> <td>779LED</td> <td>26.4w LED Down Scents</td> <td>0.000</td>	SINGLE	15	779LED	26.4w LED Down Scents	0.000
J <td>SINGLE</td> <td>12</td> <td>102_30_30_30_30_170_1</td> <td>6" LED Down Scents</td> <td>0.000</td>	SINGLE	12	102_30_30_30_30_170_1	6" LED Down Scents	0.000
K <td>SINGLE</td> <td>15</td> <td>394A-4-102A-6470 NW</td> <td>140w LED Area Light, 30C, Type IV, Motion Response Dimming, 15' Mounting Height</td> <td>0.000</td>	SINGLE	15	394A-4-102A-6470 NW	140w LED Area Light, 30C, Type IV, Motion Response Dimming, 15' Mounting Height	0.000
L <td>SINGLE</td> <td>5</td> <td>394A-3W-402A-6470 NW</td> <td>140w LED Area Light, 30C, Type V, Motion Response Dimming, 15' Mounting Height</td> <td>0.000</td>	SINGLE	5	394A-3W-402A-6470 NW	140w LED Area Light, 30C, Type V, Motion Response Dimming, 15' Mounting Height	0.000
M <td>SINGLE</td> <td>2</td> <td>394A-2-102A-6470 NW</td> <td>140w LED Area Light, 30C, Type II, Motion Response Dimming, 15' Mounting Height</td> <td>0.000</td>	SINGLE	2	394A-2-102A-6470 NW	140w LED Area Light, 30C, Type II, Motion Response Dimming, 15' Mounting Height	0.000
N <td>SINGLE</td> <td>11</td> <td>102C_M_102C_M_1</td> <td>6" LED Down Scents</td> <td>0.000</td>	SINGLE	11	102C_M_102C_M_1	6" LED Down Scents	0.000

Calculation Summary	CalcType	Units	Area	Min	Avg/Min	Max	Min/Min
Perimeter	Perimeter	FC	42.62	23.4	7.15	5.49	
Outdoor Plaza	Outdoor Plaza	FC	5.30	2.5	2.12	1.15	5.40
Parking Lot	Parking Lot	FC	4.77	2.0	2.39	0.5	4.25
Pool Area	Pool Area	FC	8.56	3.6	2.38	11.8	3.28
Ramp	Ramp	FC	6.27	4.6	1.38	0.4	1.83
Sidewalks	Sidewalks	FC	4.84	1.0	4.84	11.0	11.00

OTO DEVELOPMENT
LIGHTING/PHOTOMETRIC PLAN
SCALE 1" = 20'-0"

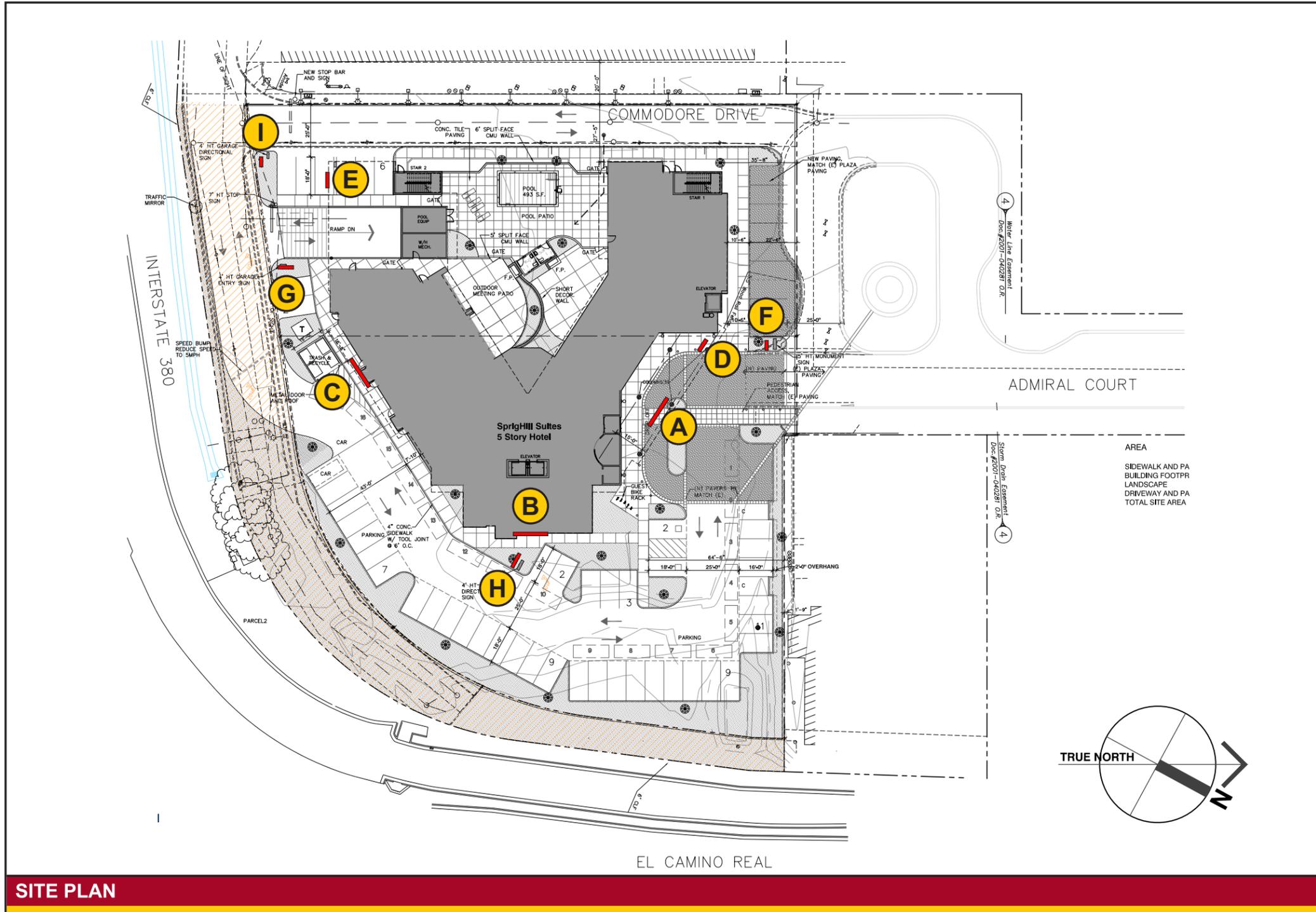
PROJECT INFO.

300 Frank H. Ogawa Plaza
Oakland, CA 94612
TEL: 510.463.8395 • FAX: 510.463.8395

ARCHITECTURAL
DIMENSIONS

SPRINGHILL SUITES
EL CAMINO REAL @ HWY 380
SAN BRUNO, CA 94066

JOB NO. C1040
DATE: 08.10.2015
DRAWING NO. PM-1



- SCOPE OF WORK**
- A** 36" Channel Letters - Remote (See pg. 5 for specifications)
 - B** 30" Channel Letters - Remote (See pg. 6 for specifications)
 - C** 36" Channel Letters - Remote (See pg. 7 for specifications)
 - D** 12" Building Letters (See pg. 8 for specifications)
 - E** 24" Parking Letters (See pg. 9 for specifications)
 - F** 5'-0" OAH D/F Monument Sign (See pg. 10 for specifications.)
 - G** 4'-0" OAH D/F Directional Sign (See pg. 11 for specifications.)
 - H** 4'-0" OAH D/F Directional Sign (See pg. 12 for specifications.)
 - I** 4'-0" OAH D/F Directional Sign (See pg. 11 for specifications.)

SITE PLAN



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: NTS	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno, CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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2			
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4	Hermes A.	06-01-15	Please refer to PATHFINDER request for more details.
5	Flor B.	08-03-15	Please refer to PATHFINDER request for more details.
6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 1 of 12

Request Number:
DEV-SHS1-00049-R6

File Location:
W:\Springhill Suites\Art\DEV-SHS1-00049



A D NORTH ELEVATION @ ILLUMINATED CHANNEL LETTERS
 Scale: 1/16"=1'-0"

**NOTE: CONCEPTUAL IMAGE ONLY
 VERIFY ARTWORK AND DIMENSIONS
 PRIOR TO FABRICATION**



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno, CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 2 of 12
 Request Number:
DEV-SHS1-00049-R6
 File Location:
 W:\Springhill Suites\Art\DEV-SHS1-00049



B NORTH-EAST ELEVATION @ ILLUMINATED CHANNEL LETTERS
 Scale: 1/16"=1'-0"

**NOTE: CONCEPTUAL IMAGE ONLY
 VERIFY ARTWORK AND DIMENSIONS
 PRIOR TO FABRICATION**



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno , CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 3 of 12
 Request Number:
DEV-SHS1-00049-R6
 File Location:
 W:\Springhill Suites\Art\DEV-SHS1-00049



SOUTH ELEVATION @ ILLUMINATED CHANNEL LETTERS

Scale: 1/16"=1'-0"

**NOTE: CONCEPTUAL IMAGE ONLY
VERIFY ARTWORK AND DIMENSIONS
PRIOR TO FABRICATION**

**NOTE: ALL DIMENSIONS AND
CONDITIONS MUST BE FIELD
VERIFIED PRIOR TO MANUFACTURE**



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale:	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno , CA
Sales: Brenda H.	Client Approval: _____ Date: _____

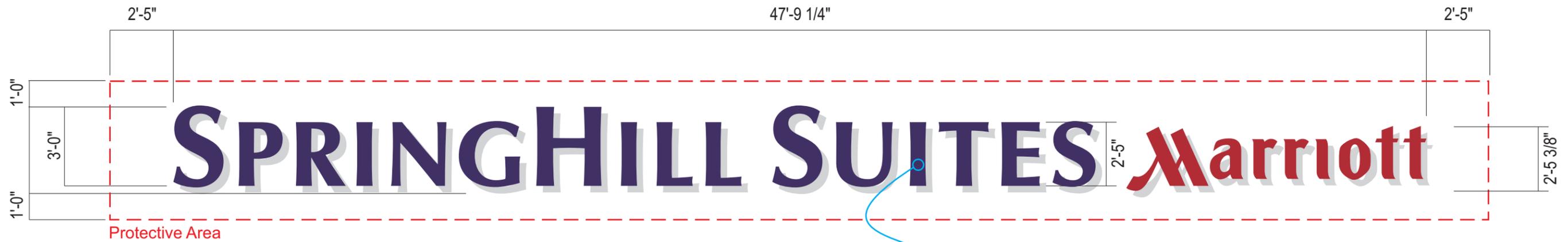
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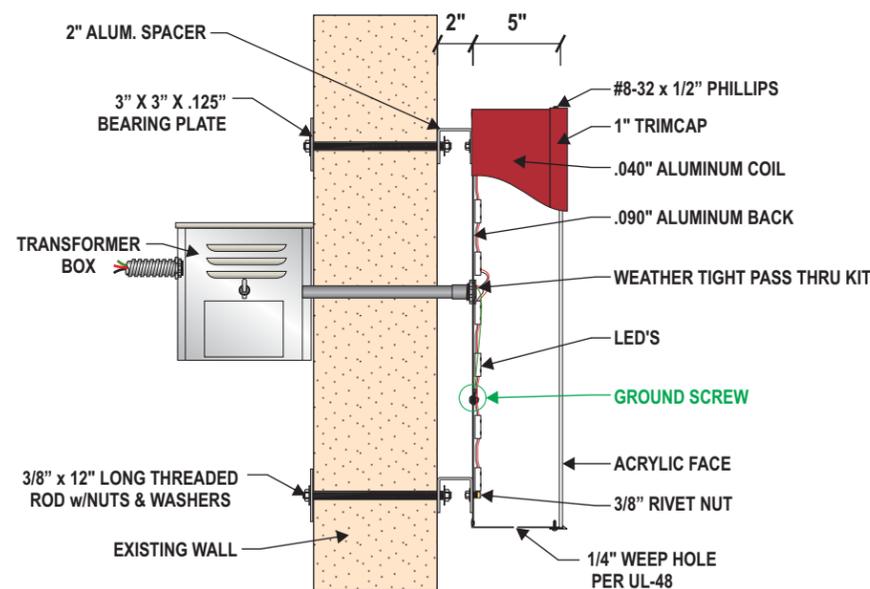
DESIGN DRAWING 4 of 12

Request Number:
DEV-SHS1-00049-R6

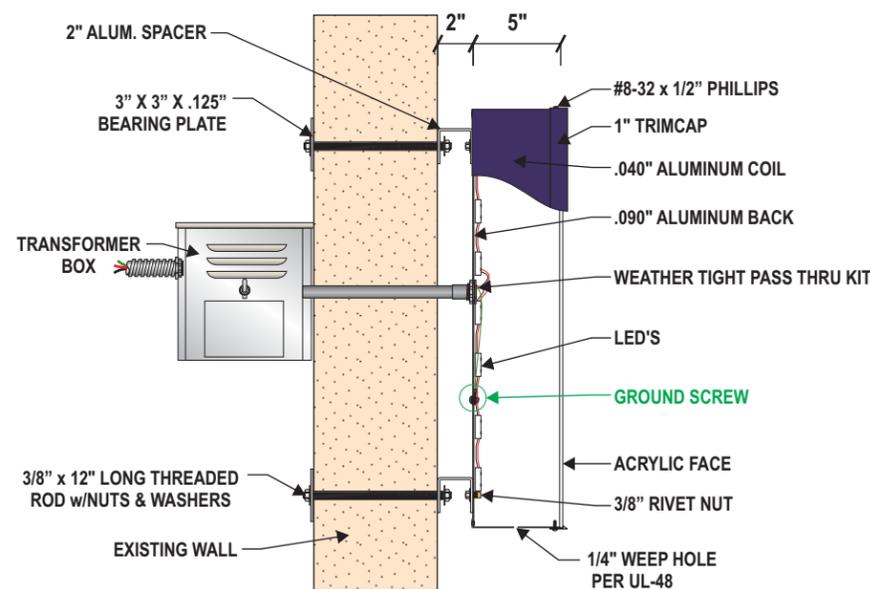
File Location:
W:\Springhill Suites\Art\DEV-SHS1-00049



A FRONT VIEW @ INTERNALLY ILLUMINATED LETTERS
Scale: 1/4" = 1'-0"



SECTION DETAIL @ MARRIOTT (LED) CHANNEL LETTERS:
SCALE: NTS



SECTION DETAIL @ SPRINGHILL (LED) CHANNEL LETTERS:
SCALE: NTS

REMOTE CHANNEL SPECIFICATIONS:

SPRINGHILL SUITES LETTERS:

FACES:..... .177" #2447 WHITE ACRYLIC WITH FIRST SURFACE DUAL COLOR
3M #DVN11873 PURPLE PERFORATED VINYL
TRIM CAP:..... 1" JEWELITE PAINTED Mp00341 SATIN FINISH
RETURNS:..... .040" X 5" DEEP PRE-PAINT ALUMINUM PAINTED Mp00341 SATIN FINISH
BACKS:063" PRE-PAINTED WHITE ALUMINUM
ILLUMINATION:.. 15MM DESIGNER 65 WHITE NEON

MARRIOTT:

FACES:..... .177" #2447 WHITE ACRYLIC WITH FIRST SURFACE
FIRST SURFACE 3M #VT11331 RED
TRIM CAP:..... 1" JEWELITE PAINTED Mp08937 SATIN
RETURNS:..... .040" X 5" DEEP PRE-PAINT ALUMINUM PAINTED Mp08937 SATIN FINISH
BACKS:063" PRE-PAINTED WHITE ALUMINUM
ILLUMINATION:.. RED GELCORE LEDS

COLOR SPECIFICATIONS

	Red Vinyl: 3M #VT-11331 Paint : Matthews #MP-08937 w/ Satin Finish
	Dual Color Vinyl: 3M #VDN-00093 Paint: Matthews #MP-00341 w/ Satin Finish
	Returns: Matthews #MP-00341 & #MP-08937

NOTE: FIELD VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE FABRICATION.



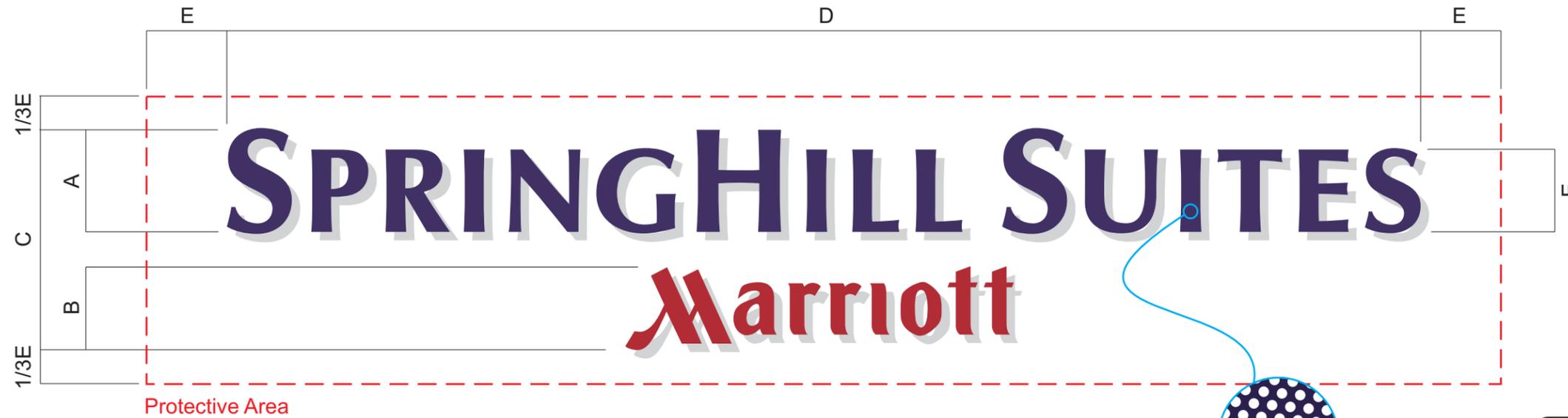
Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno , CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 5 of 12

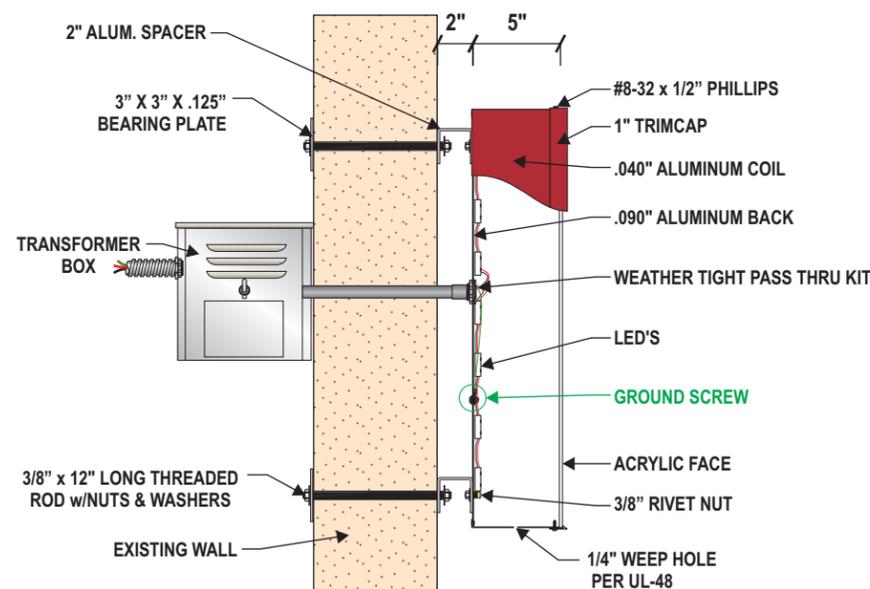
Request Number:
DEV-SHS1-00049-R6
File Location:
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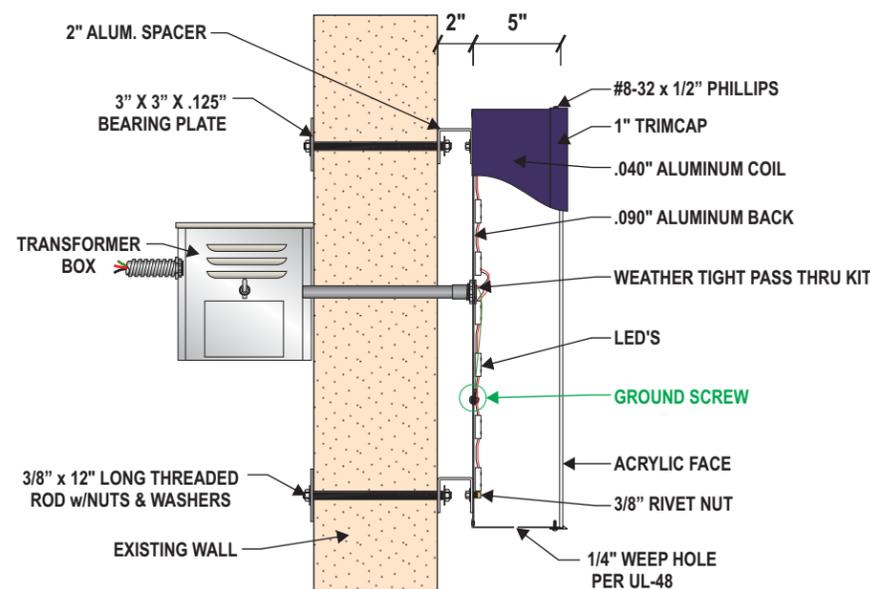
FRONT VIEW @ INTERNALLY ILLUMINATED LETTERS



NIGHT VIEW



SECTION DETAIL @ MARRIOTT (LED) CHANNEL LETTERS:
SCALE: NTS



SECTION DETAIL @ SPRINGHILL (LED) CHANNEL LETTERS:
SCALE: NTS

REMOTE CHANNEL SPECIFICATIONS:

SPRINGHILL SUITES LETTERS:

FACES:..... .177" #2447 WHITE ACRYLIC WITH FIRST SURFACE DUAL COLOR
 3M #DVN11873 PURPLE PERFORATED VINYL
 TRIM CAP:..... 1" JEWELITE PAINTED Mp00341 SATIN FINISH
 RETURNS:..... .040" X 5" DEEP PRE-PAINT ALUMINUM PAINTED Mp00341 SATIN FINISH
 BACKS:063" PRE-PAINTED WHITE ALUMINUM
 ILLUMINATION: . 15MM DESIGNER 65 WHITE NEON

MARRIOTT:

FACES:..... .177" #2447 WHITE ACRYLIC WITH FIRST SURFACE
 FIRST SURFACE 3M #VT11331 RED
 TRIM CAP:..... 1" JEWELITE PAINTED Mp08937 SATIN
 RETURNS:..... .040" X 5" DEEP PRE-PAINT ALUMINUM PAINTED Mp08937 SATIN FINISH
 BACKS:063" PRE-PAINTED WHITE ALUMINUM
 ILLUMINATION: . RED GELCORE LEDS

ILLUM. LTR. DIMENSION LEGEND

	A	B	C	D	E
	24"	20"	4'-4"	23'-5"	19"
B	30"	25"	5'-5"	29'-3"	24"
	36"	30"	6'-6"	35'-0"	29"
	48"	40"	8'-8"	46'-10"	39"

COLOR SPECIFICATIONS

	Red Vinyl: 3M #VT-11331 Paint : Matthews #MP-08937 w/ Satin Finish
	Dual Color Vinyl: 3M #VDN-00093 Paint: Matthews #MP-00341 w/ Satin Finish
	Returns: Matthews #MP-00341 & #MP-08937

B ILLUM. CHANNEL LTRS. - REMOTE

NOTE: FIELD VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE FABRICATION.

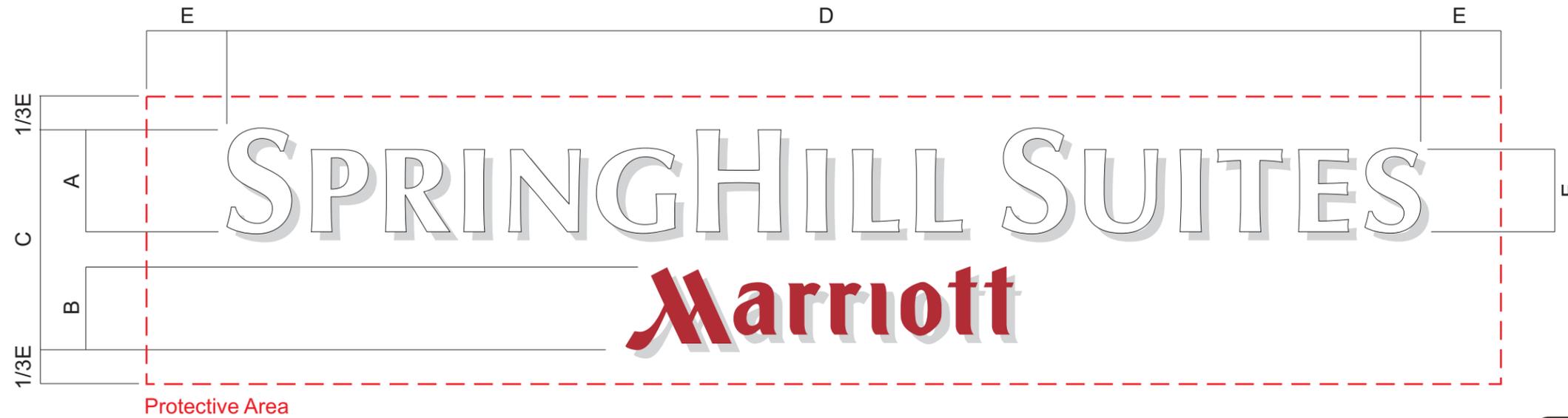


Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno, CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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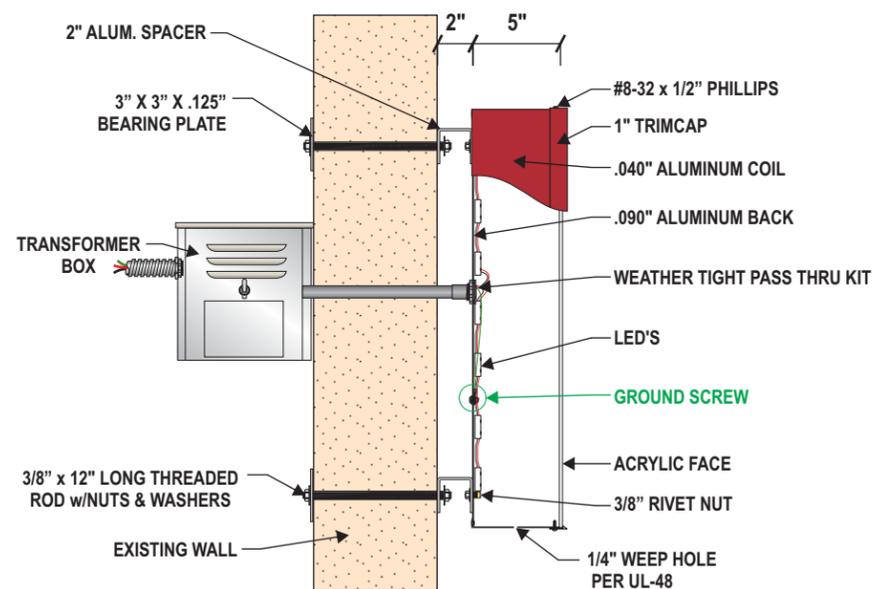
DESIGN DRAWING 6 of 12
Request Number: DEV-SHS1-00049-R6
File Location: W:\Springhill Suites\Art\DEV-SHS1-00049



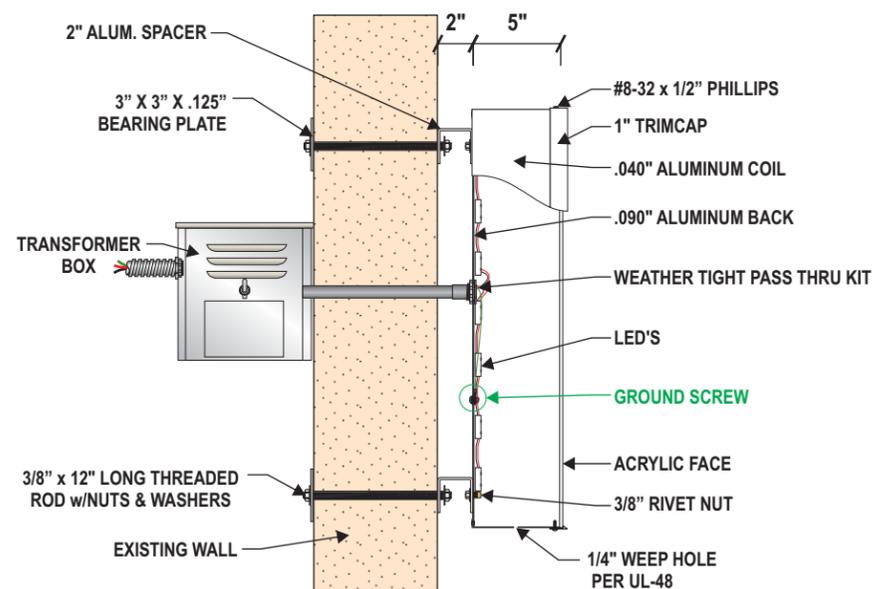
FRONT VIEW @ INTERNALLY ILLUMINATED LETTERS



NIGHT VIEW



SECTION DETAIL @ MARRIOTT (LED) CHANNEL LETTERS:
SCALE: NTS



SECTION DETAIL @ SPRINGHILL (LED) CHANNEL LETTERS:
SCALE: NTS

REMOTE CHANNEL SPECIFICATIONS:

SPRINGHILL SUITES LETTERS:

FACES:..... .177" #2447 WHITE ACRYLIC FACE
 TRIM CAP:..... 1" JEWELITE PAINTED Mp00341 SATIN FINISH
 RETURNS:..... .040" X 5" DEEP PRE-PAINT ALUMINUM PAINTED Mp00341 SATIN FINISH
 BACKS:063" PRE-PAINTED WHITE ALUMINUM
 ILLUMINATION:.. 15MM DESIGNER 65 WHITE NEON

MARRIOTT:

FACES:..... .177" #2447 WHITE ACRYLIC WITH FIRST SURFACE
 FIRST SURFACE 3M #VT11331 RED
 TRIM CAP:..... 1" JEWELITE PAINTED Mp08937 SATIN
 RETURNS:..... .040" X 5" DEEP PRE-PAINT ALUMINUM PAINTED Mp08937 SATIN FINISH
 BACKS:063" PRE-PAINTED WHITE ALUMINUM
 ILLUMINATION:.. RED GELCORE LEDS

ILLUM. LTR. DIMENSION LEGEND

A	B	C	D	E
24"	20"	4'-4"	23'-5"	19"
30"	25"	5'-5"	29'-3"	24"
36"	30"	6'-6"	35'-0"	29"
48"	40"	8'-8"	46'-10"	39"

COLOR SPECIFICATIONS

	Red Vinyl: 3M #VT-11331 Paint : Matthews #MP-08937 w/ Satin Finish
	Returns: Matthews #MP-00341 & #MP-08937

C ILLUM. CHANNEL LTRS. - REMOTE

NOTE: FIELD VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE FABRICATION.

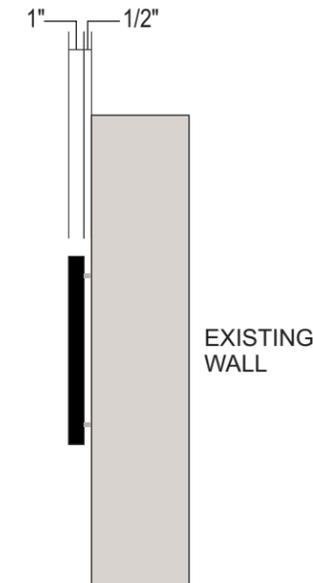
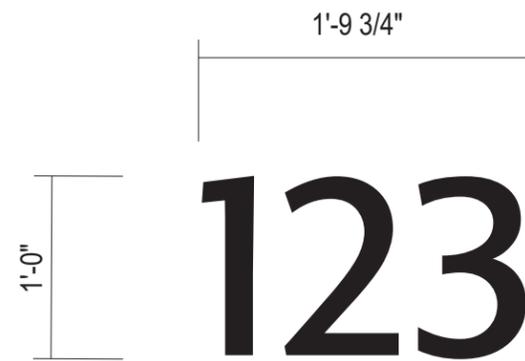


Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno, CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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DESIGN DRAWING 7 of 12
Request Number: DEV-SHS1-00049-R6
File Location: W:\Springhill Suites\Art\DEV-SHS1-00049



D FRONT VIEW @ BUILDING NUMERAL LETTERS
Scale: 1"=1'-0"

GENERAL SPECIFICATIONS:

1" THICK PLASTIC LETTERS PAINTED BLACK
PEGGED OFF 1/2" FROM WALL

**NOTE: CONCEPTUAL IMAGE ONLY
PERMIT DRAWING REQUIRED PRIOR
TO SUBMITTING TO ENGINEERING**



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale:	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno , CA
Sales: Brenda H.	Client Approval: _____ Date: _____

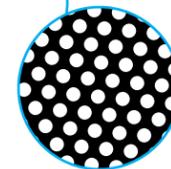
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DESIGN DRAWING 8 of 12
Request Number: DEV-SHS1-00049-R6
File Location: W:\Springhill Suites\Art\DEV-SHS1-00049



E FRONT VIEW @ CHANNEL LETTERS
Scale: 1/2"=1'-0"



GENERAL SPECIFICATIONS:
ILLUMINATED CHANNEL LETTERS

LETTERS:
FACES: .177 TRANSLUCENT WHITE ACRYLIC WITH FIRST SURFACE BLACK PERFORATED VINYL
TRIMCAP: 1" JEWELITE PAINTED BLACK
RETURNS: .040" X 5.3" PAINTED BLACK
BACKS: PRE-PAINTED WHITE .063" ALUMINUM
ILLUMINATION: WHITE L.E.D ILLUMINATED

NOTE: ALL DIMENSIONS AND CONDITIONS MUST BE FIELD VERIFIED PRIOR TO MANUFACTURE

NOTE: CONCEPTUAL IMAGE ONLY PERMIT DRAWING REQUIRED PRIOR TO SUBMITTING TO ENGINEERING

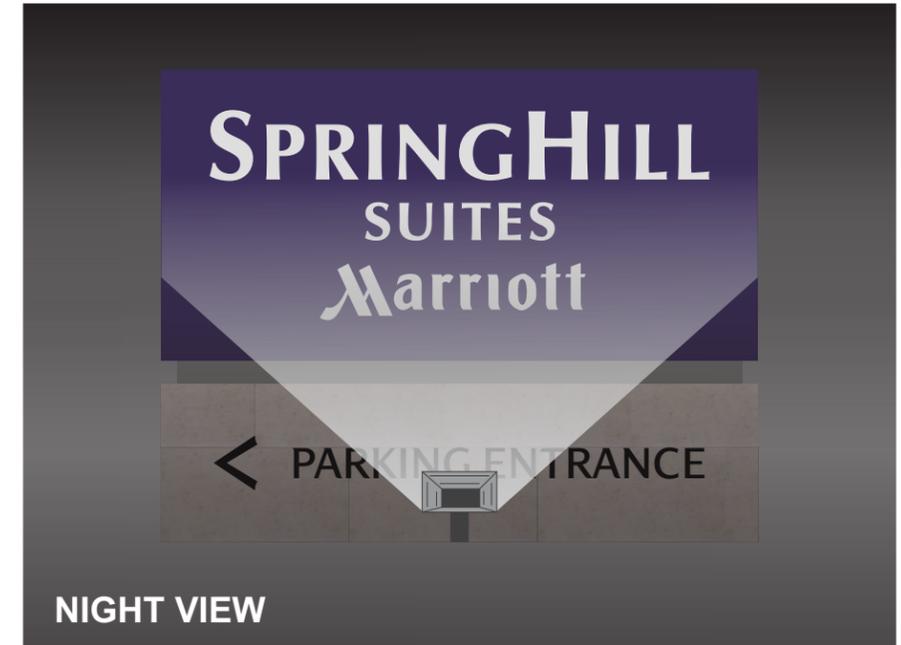
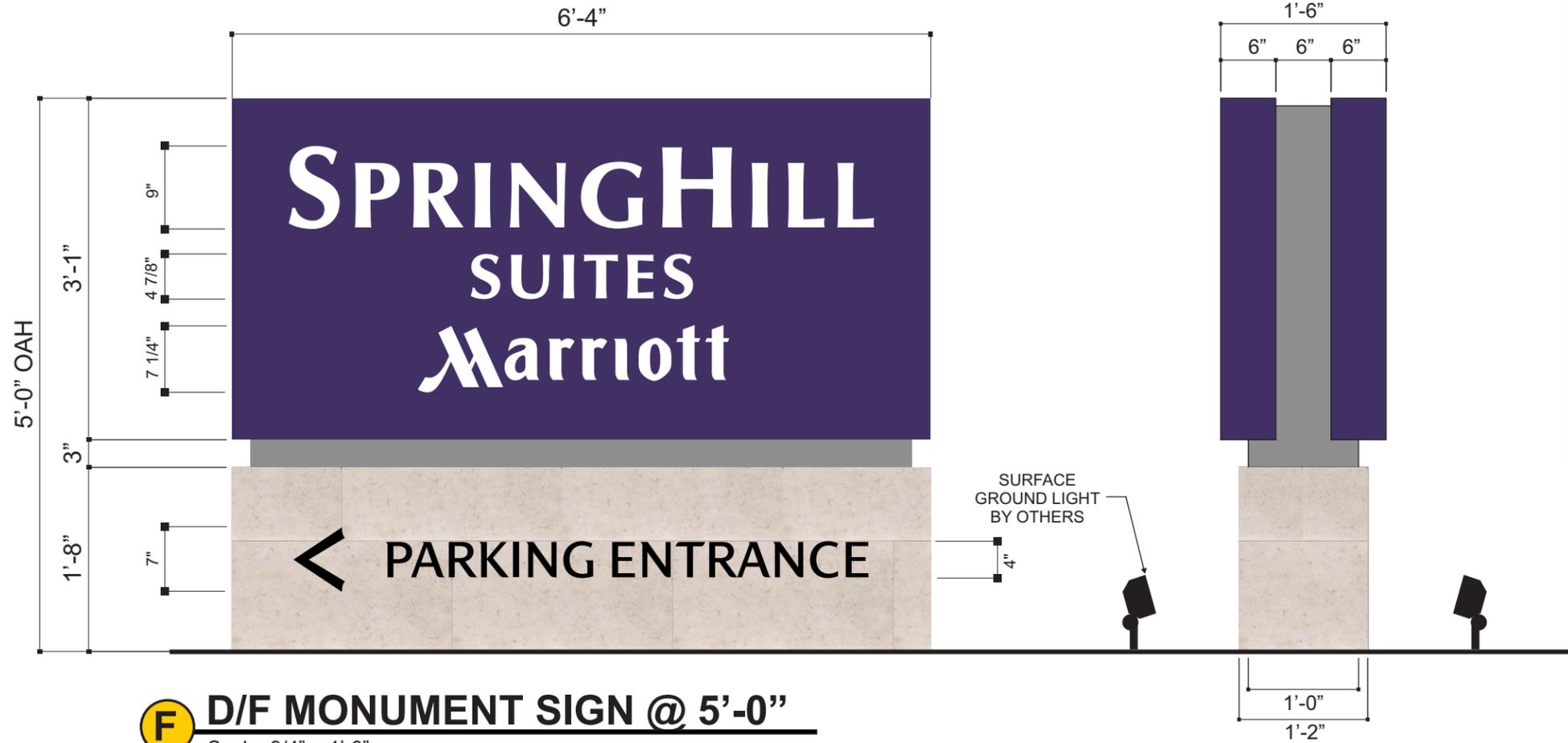


Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale:	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno , CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 9 of 12
Request Number:
DEV-SHS1-00049-R6
File Location:
W:\Springhill Suites\Art\DEV-SHS1-00049



F D/F MONUMENT SIGN @ 5'-0"
Scale: 3/4" = 1'-0"

GENERAL SPECIFICATIONS:

CABINET:

FACES: ALUMINUM ROUTED FOR FLUSH IN LAY WITH WHITE PLEX PAINTED TO MATCH VIOLET

ILLUMINATION: WHITE LED's

MARRIOTT : CLEAR ACRYLIC WITH RED VINYL 2ND SURFACE & WHITE PERFORATED FIRST SURFACE.

PARKING ENTRANCE : BLACK SCOTCHLITE VINYL 1ST SURFACE

BASE CLADDING:

12" x 24" FRENCH LIMESTONE (FRENCH WHITE COLOR) WITH 1/4" FULL SMOOTH TOOLED GROUT JOINT

GROUND LIT : (BY OTHERS)

NOTE: FIELD VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE FABRICATION.

COLOR SPECIFICATIONS

	Violet Vinyl: #VT-11291 1st. Surf. Raspberry #3630-133 2nd. Surf. Paint #MP-00341 Satin Finish
	Lexan: White
	Paint (Base): Matthews #MP-20140 w/ Gloss Finish.



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno, CA
Sales: Brenda H.	Client Approval: _____ Date: _____

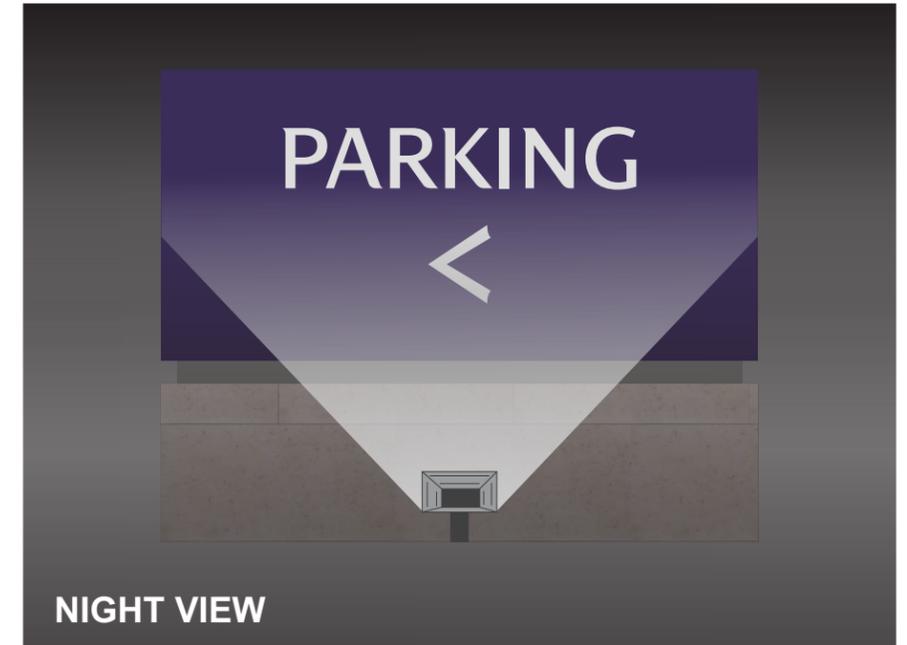
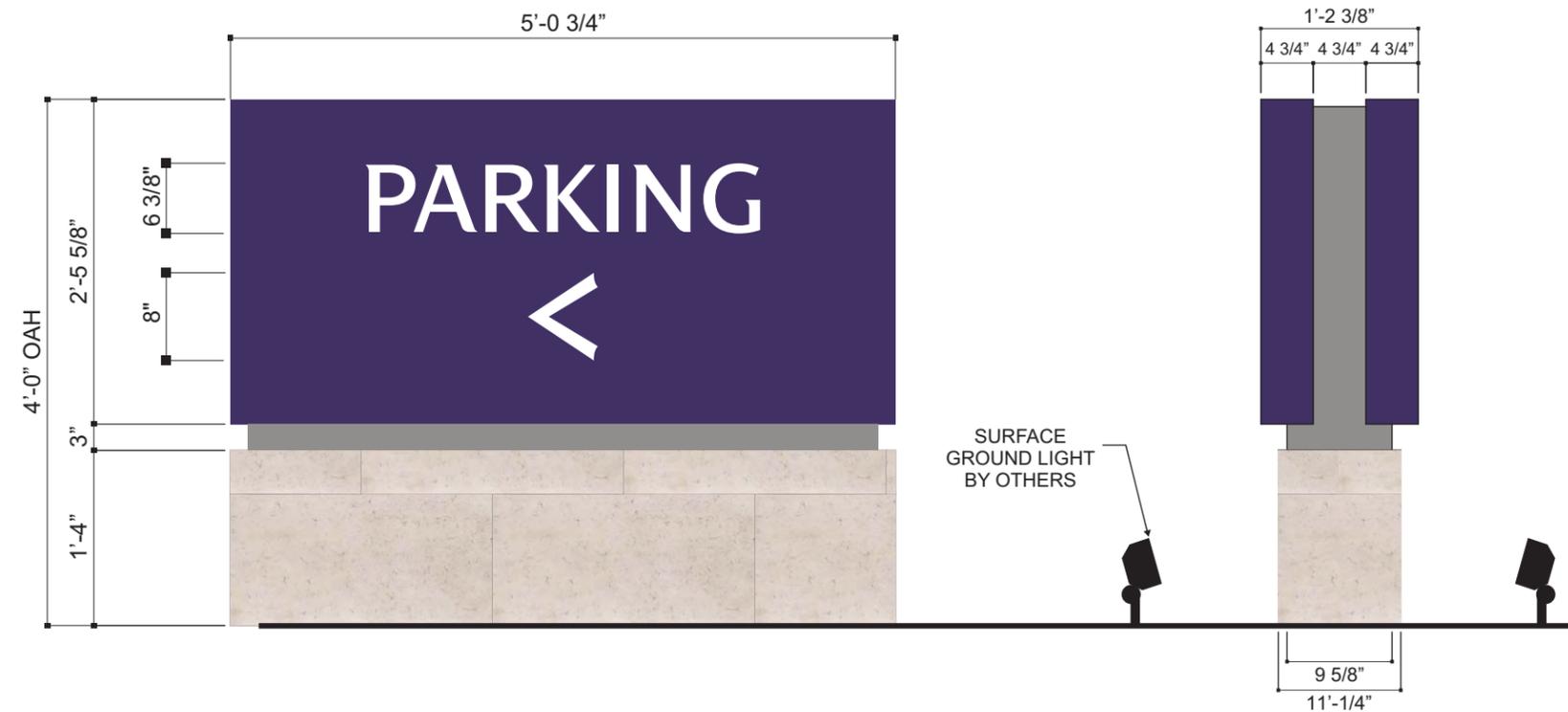
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No.	Designer	Date	Revision Notes
1	E. Tiedra	03-16-15	Please refer to PATHFINDER request for more details.
2			
3	Flor B.	05-29-15	Please refer to PATHFINDER request for more details.
4	Hermes A.	06-01-15	Please refer to PATHFINDER request for more details.
5	Flor B.	08-03-15	Please refer to PATHFINDER request for more details.
6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 10 of 12

Request Number:
DEV-SHS1-00049-R6

File Location:
W:\Springhill Suites\Art\DEV-SHS1-00049



G I **D/F PARKING SIGN @ 4'-0"**

Scale: 3/4" = 1'-0"

GENERAL SPECIFICATIONS:

CABINET:

FACES: ALUMINUM FACE PAINTED TO MATCH VIOLET WITH WHITE SCOTCHLITE VINYL 1ST SURFACE

BASE CLADDING:

12" x 24" FRENCH LIMESTONE (FRENCH WHITE COLOR) WITH 1/4" FULL SMOOTH TOOLED GROUT JOINT

GROUND LIT : (BY OTHERS)

NOTE: FIELD VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE FABRICATION.

COLOR SPECIFICATIONS

	Violet Vinyl: #VT-11291 1st. Surf. Raspberry #3630-133 2nd. Surf. Paint #MP-00341 Satin Finish
	Lexan: White
	Paint (Base): Matthews #MP-20140 w/ Gloss Finish.



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno, CA
Sales: Brenda H.	Client Approval: _____ Date: _____

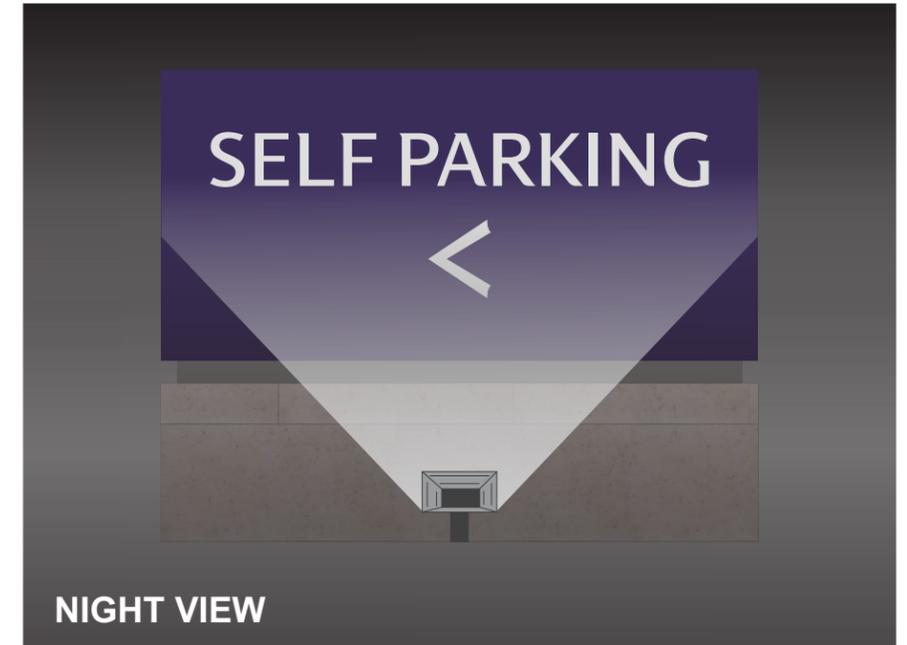
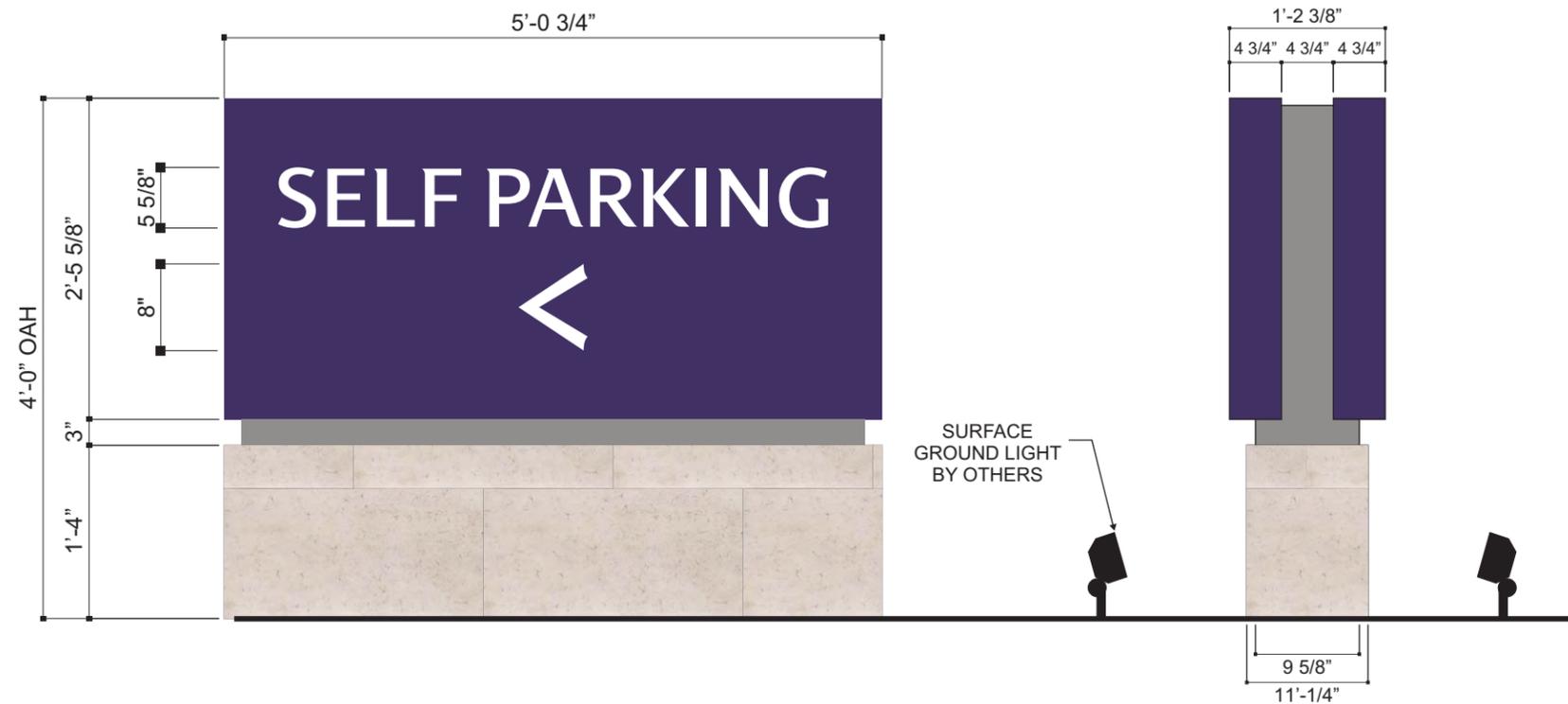
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DESIGN DRAWING 11 of 12

Request Number:
DEV-SHS1-00049-R6

File Location:
W:\Springhill Suites\Art\DEV-SHS1-00049



H D/F PARKING SIGN @ 4'-0"

Scale: 3/4" = 1'-0"

GENERAL SPECIFICATIONS:

CABINET:

FACES: ALUMINUM FACE PAINTED TO MATCH VIOLET WITH WHITE SCOTCHLITE VINYL 1ST SURFACE

BASE CLADDING:

12" x 24" FRENCH LIMESTONE (FRENCH WHITE COLOR) WITH 1/4" FULL SMOOTH TOOLED GROUT JOINT

GROUND LIT : (BY OTHERS)

COLOR SPECIFICATIONS

	Violet Vinyl: #VT-11291 1st. Surf. Raspberry #3630-133 2nd. Surf. Paint #MP-00341 Satin Finish
	Lexan: White
	Paint (Base): Matthews #MP-20140 w/ Gloss Finish.

NOTE: FIELD VERIFY ALL DIMENSIONS AND CONDITIONS BEFORE FABRICATION.



Date: 03-12-15	Project Name: SPRINGHILL SUITES
Scale: Noted	Address: 124 Johnson Court
Drawn: E. Tiedra	City / State: San Bruno , CA
Sales: Brenda H.	Client Approval: _____ Date: _____

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6	E. Tiedra	08-11-15	Please refer to PATHFINDER request for more details.

DESIGN DRAWING 12 of 12
Request Number: DEV-SHS1-00049-R6
File Location: W:\Springhill Suites\Art\DEV-SHS1-00049